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**Ganges Harbour Walk Project Steering Committee**  
Notice of Meeting on **Friday, December 7, 2018 at 10:00 AM**  
Creekside Meeting Room, 108 121 McPhillips Ave, Salt Spring Island, BC

Gary Holman  
Matt Steffich

Peter Grove  
Chris Acheson

Gregg Dow

Robin Williams

(R) = Regrets

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**AGENDA**

1. **Approval of Agenda**
2. **Adoption of Minutes of February 20, 2018** 2-4
3. **Director and Chair Reports**
4. **Outstanding Business**
5. **New Business**
  - 5.1 **Ganges Harbourwalk Feasibility Report** 5-155
  - 5.2 **Islands Trust Memorandum-Ganges Harbourwalk-Next Steps-October 1, 2018** 156-261
  - 5.2 **Ganges Harbour Walk Budget Update (November 2018)** 262
6. **Next Meeting**
7. **Adjournment**

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*To ensure quorum, advise Tracey Shaver 250 537 4448 if you cannot attend.*



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**Minutes of the Regular Meeting of the  
GANGES HARBOUR WALK PROJECT STEERING COMMITTEE  
Held February 20, 2018, Creekside Meeting Rm, 108 121 McPhillips Ave, Salt Spring  
Island, BC**

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**DRAFT**

**Present:**

**Committee Members:** Matt Steffich, Peter Grove, Wayne McIntyre, Robin Williams, Chris Acheson, Gregg Dow.

**Staff:** Karla Campbell, Senior Manager, Salt Spring Island Electoral Area; Dan Ovington, Parks and Recreation Manager; Tracey Shaver, Recording Secretary

**Absent:** Wayne Lee P.Eng, Project Manager

Chair Steffich called the meeting to order at 10:05 am.

**1. Approval of Agenda**

**MOVED** by P. Grove, **SECONDED** by G.Dow,  
That the Ganges Harbour Walk Project Steering Committee agenda of February 20, 2018 be approved.

**CARRIED**

**2. Adoption of Minutes of June 6, 2017**

**MOVED** by P. Grove, **SECONDED** by W. McIntyre,  
That the Ganges Harbour Walk Project Steering Committee minutes of June 6, 2017 be approved.

**CARRIED**

**3. Director and Chair Reports**

- 3.1** Chair Matt Steffich outlined his objectives for today's meeting:
1. "Reset" starting point of project by determining what progress has been made.
  2. Define a unified communications plan.
  3. Obtain direction from the Provincial Government on foreshore lease issues.

**4. Outstanding Business**

**4.1 Phase 1 Engineering and Structural Assessments – Status Update**

Senior Manager Karla Campbell provided a status update on the assessment process that the Capital Regional District is undertaking on the existing boardwalk structure.

- Agreements for the right to enter private foreshore properties and conduct investigative work have been secured.
- Four contracts have been awarded to assess the geotechnical, structural, archeological, and marine ecological aspects of the existing boardwalk.
- Project Manager will summarize reports once contracted work is complete and it is anticipated that the information will be presented to the Committee in June if not before.

- The \$150,000 Community Works funding is to be used for assessment purposes only, there is no funding in place for the construction phases of this project.

## **5. New Business**

### **5.1 Statutory Right of Way Tenure and Rezoning- Update**

Trustee Peter Grove provided an outline of the process Islands Trust has been working on regarding rezoning and investigation of foreshore lease renewals.

The Committee discussed:

- Islands Trust public information meeting in November regarding proposed C7 zoning.
- Islands Trust staff provided mock up designs of potential shoreline development for discussion purposes only.
- Strong community showing and input received; resulted in support for community boardwalk without development.
- Islands Trust staff reviewing status of development permits already in place under current C1 zoning.
- A member of the public suggested that the foreshore riparian rights are over extended and should be further investigated.
- Islands Trust staff have requested that the province consult with Islands Trust regarding the renewal or replacement of tenure areas adjacent to properties in line with the proposed boardwalk.
- Considering the potential to negotiate an altered or replaced foreshore lease in the interest of the community, based on the OCP.
- The province is unlikely to comment on foreshore lease options without an application to obtain the foreshore lease being submitted.
- Islands Trust staff are preparing a report for the March Local Trust Committee meeting to outline options for next steps.

The Ganges Harbour Walk Project Steering Committee discussed potential ideas to move the boardwalk project forward should negotiations with the upland property owners fail.

The following next steps were established:

- Chair Steffich to prepare a project status summary on behalf of the Committee which the CRD will release to the public.
- Draft minutes from this meeting will be placed on CRD Website for public distribution as soon as possible.
- Improve internal communications with project status updates.

### **5.2 Consultation with First Nations**

The Committee established that consultation with First Nations is a vital component of the harbour walk project and that both the CRD and Islands Trust have professional staff to assist with the communication and negotiating process.

**5.3 North Ganges Transportation Management Plan Update (NGTMP)**

Committee member Robin Williams reported on a Transportation Commission project which has a potential impact on the Harbour Walk project. The NGTMP is being completed in phases. The current phase is the upland portion of Lower Ganges Road from the intersection of Upper and Lower Ganges to Rainbow Road and around the corner to Windsor Plywood. Anticipated to be completed this year.

Preliminary designs for the phase on the harbour side of the road are completed, but have been on hold pending the outcome of revitalizing the boardwalk. The Transportation Commission anticipates asking the community through referendum which phase of the NGTMP should be completed next. The choice will be between a harbour side pathway/bike lane and improvements to Ganges Hill.

The Transportation Commission would like to integrate their roadside design with whatever comes out of the harbour walk project.

**6. Motion to Close the meeting**

**MOVED** by P. Grove, **SECONDED** by G. Dow,

That the Ganges Harbour Walk Project Steering Committee move into a Closed meeting in accordance with the Community Charter Part 4, Division 3, Section 90 (1): (e) the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality;

**CARRIED**

The Committee closed the meeting at 11:18 am.

The Committee returned from the closed meeting without report at 11:40 am.

**7. Rise and Report-** no report

**8. Next Meeting will be at the call of the Chair**

**9. Adjournment**

**MOVED** by P. Grove, **SECONDED** by R. Williams,

That the meeting be adjourned at 11:42 pm.

**CARRIED**

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**CHAIR**

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**SENIOR MANAGER**



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## REPORT TO GANGES HARBOUR WALK STEERING COMMITTEE MEETING OF FRIDAY, DECEMBER 07, 2018

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**SUBJECT**     Ganges Harbour Walk Feasibility Report

### **ISSUE**

To review and consider the next steps in the Ganges Harbour Walk project.

### **BACKGROUND**

The current harbour walk infrastructure was installed in the 1980s as a collaborative initiative involving public agencies and community volunteers. The legal instruments required to guarantee public access (known as statutory right of ways or SROWs) were unfortunately not secured at the time of original construction. Consequently, there are gaps in the harbour walk, and some segments have fallen into disrepair. In 2017 the Capital Regional District (CRD) engaged a Project Manager to undertake the following engineering and environmental assessments of the current harbour walk infrastructure to inform the design process:

1. Structural assessment of existing works including remaining service life.
2. Geotechnical review of the existing foundation and shoreline.
3. Ecological and environmental marine assessment.
4. Archeological overview assessment.
5. Site survey to record elevation and dimensional parameters of the proposed site.

The results of these studies are attached to this report as Appendix A: Ganges Harbour Walk Summary Report Feasibility Constraints dated September 2018 by Aqion Water Technologies Ltd.

The harbour walk infrastructure is over 30 years old, installed without adherence to any specific design, or any recorded construction drawings, and has deteriorated significantly due to a lack of maintenance and repairs. Much of the information gathered on the structural integrity of the piles, indicates that they will require replacement within 10+ years. The Stantec report states that, “the estimated useable life” of the repurposed structure will be less than that of a new boardwalk structure; and that a cost benefit analysis may find that it is not cost-effective to repair/re-use the existing structure”.

The challenges moving the project forward to a design phase are (1) securing agreement for a statutory right of way (SROW) over the foreshore; and (2) an overall harbour area plan with design guidelines and policies to inform upland development and integrate a harbour walk design within the existing marine infrastructure. (Appendix B June 5, 2018, Correspondence)

1. The Islands Trust (IT) submitted a crown tenure application to secure a SROW over the foreshore for a future boardwalk.

The Ministry of Forest Lands Natural Resource Operations (FLNRO) is reviewing the application and have advised both the CRD and IT that the project may impact the upland property owner riparian rights which requires ministry staff to consider the existing rights,

contractual, and other rights of the crown tenure holder. Riparian rights and the right of access has been specifically defined with respect to the waterfront property. Ingress and egress must be possible from every point along the water frontage over every part of the foreshore (Ministry, 2008, p. 9) these rights are generally summarized as follows:

- An owner of land abutting water is entitled to access to and from the water
- Applies to non-tidal river, tidal river, lake, or the sea where there is contact with the property frontage
- Riparian owner must be physically able to access the water from their property and cannot be prevented from doing so by an obstruction erected in the foreshore
- Construction of a boardwalk in the foreshore requires ‘consent’ from the owner

FLNRO has informed both Islands Trust and CRD to “address any potential impacts” the harbour walk may have on the “upland property owner’s rights”; and with the “objective of reaching a mutually acceptable arrangement”. (Appendix C July 25, 2018 Correspondence)

The responsibilities for this project are split between the Island’s Trust being responsible for land use planning; and the CRD’s role is design and construction. The difficulty in addressing the harbour walk impact on the upland owners property is (a) there are no designs for the harbour walk; and (b) without upland owner consent or SROW it is difficult to inform criteria for detailed site designs, dimensions, elements of mixed uses, construction materials, alignment, and how to incorporate the harbour walk within the existing upland owner’s infrastructure and commercial business operating along and within the foreshore.

As with any project, prior to acquiring any land or property rights staff must consider the liabilities and risks prior to acquisition. The risks may be: environmental, financial, access limitation, encumbrances, entitlements or other risks of ownership. The current challenges associated with the foreshore SROW include several risks such as: financial, environmental, access and derelict infrastructure, public health and safety, etc. Staff recommend not taking responsibility for an SROW at this time as (a) it would not be fiscally prudent to inherit liability for the existing boardwalk infrastructure until a viable project is determined which includes detailed designs, costing, and secured funding (i.e. electoral assent for incurring debt); and (b) without secured project funding the acquisition places the derelict foreshore infrastructure responsibility and liability upon of the parks and recreation budgets at the cost of other priorities.

2. It is most likely that without solid commitments on land use changes and planning approvals required to attract an investment in developing the uplands and a reasonable boardwalk amenity contribution, the upland property owners may not agree to the required rights of way along the foreshore of their properties.

To facilitate this the Islands Trust may need to continue with an engagement process to confirm and/or develop the vision and community planning goals, objectives, and policies for the harbour area and consider the following:

- Continue working with the upland owners to refine a proposal until it reasonably meets planning objectives and services needed to support the development. Once the use, density, and form of the project is established conduct a land economics analysis to determine the value of the development site and determine the increase in land value that is available to fund boardwalk amenity.

- Address the community objections to proposed densities; and define a community vision for an acceptable form and character harbour area plan that is reasonable and flexible enough to address development constraints and realize the opportunities of a sustainable development and add economic value through a harbour revitalization.

## **ALTERNATIVES**

That the Ganges Harbour Walk Steering Committee recommends to the Salt Spring Island Parks and Recreation Commission that:

### **Alternative 1**

The Ganges harbour walk project be held in abeyance until the foreshore right of way acquisition is determined; recommend to the Islands Trust to pursue continued engagement with the upland foreshore owners and community to develop the vision and community planning goals, objectives, and policies for the harbour area.

### **Alternative 2**

Cost estimates be obtained for the removal and disposal of the derelict sections of the boardwalk.

### **Alternative 3**

The report be referred back to staff.

## **CONCLUSION**

The route for the harbour walk cuts across a part of Ganges working waterfront and an infrastructure supporting significant marine traffic and commercial enterprise. To plan and design a harbour walk in a working waterfront it should incorporate the various uses and needs of recreation, tourism, commercial and marine activities. A harbour area plan and policies will help inform the implementation of the Ganges Harbour walk designs and development strategy.

## **RECOMMENDATION(S)**

That the Ganges Harbour Walk Steering Committee recommends to the Salt Spring Island Parks and Recreation Commission that the Ganges harbour walk project be held in abeyance until the foreshore right of way acquisition is determined; recommend to the Islands Trust to pursue continued engagement with the upland foreshore owners and community to develop the vision and community planning goals, objectives, and policies for the harbour area.

Submitted by:	Karla Campbell, BPA, Senior Manager, Salt Spring Island Electoral Area
Concurrence:	Dan Ovington, BA, Manager of Parks and Recreation

KC/do:ts

Attachments:

Appendix A: Ganges Harbour Walk Summary Report Feasibility Constraints dated September 2018 by Aqion Water Technologies Ltd.

Appendix B June 5, 2018, Correspondence CRD Director to Honourable Doug Donaldson, Minister of Forests, Lands, Natural Resources Operations and Rural Development)

Appendix C July 25, 2018, Correspondence, Craig Sutherland, Assistant Deputy Minister, of Forests, Lands, Natural Resources Operations and Rural Development to Director McIntyre)

**GANGES HARBOUR WALK  
SUMMARY REPORT.  
FEASIBILITY CONSTRAINTS**



Prepared for the Capital Regional District

Salt Spring Island BC

September 2018

## REPORT INDEX

Executive Summary and Recommendations

Project Background

Feasibility Assessment Program

Project Synopsis

Summary Appendix

## APPENDICES

- A. Horizontal Control Survey Mapping (Polaris)
- B. Archaeological Constraint Report (McLay)
- C. Marine Ecology Summary Report (Whitehead)
- D. Geotechnical Constraint Report (Ryzuk)
- E. Structural Constraint Report (Stantec)

## EXECUTIVE SUMMARY & RECOMMENDATIONS

“Technical Feasibility” studies, as described herein, have indicated that no substantial archeological, marine ecological, or geotechnical constraints are currently evident within the narrowly defined, and presently visualized, alignment for the proposed Ganges “Harbour Walk”

These study constraints, also encompassing structural parameters, were defined for this specific alignment, which generally follows the Ganges Harbour shoreline between the Rotary Park viewing platform, and the “Peck’s Cove “ harbour view location.

Additional focus was applied to the structural work scope for the previously built sections of the boardwalk, and preliminary engineering assessments, as noted in the related studies, indicate that, in spite of the significant time lapse since the original construction of these facilities, residual value is apparent within the existing “boardwalk” structure.

The following recommendations are considered appropriate, at this time, to facilitate further progress towards the necessary structural upgrades, and to potential near-term public access, to Sections 6 and 7, as described herein, for the Ganges “Harbour Walk”.

1. Proceed with the removal of the (upper) deck boards, posts, and guardrails for the existing structure in Sections 6 and 7 as defined in this report. This sector now has access points at 2 upper road-side locations, and additionally, at 2 beach stairway locations, to the harbor foreshore areas.
2. Complete a 3<sup>rd</sup> party, detailed, structural competency, dynamic loading, and residual life assessment of the existing foundation and “sub-structure” facility components for sections 6 and 7 as noted.
3. Continue negotiations for community access via the existing upper stairway through Lot 1 to the adjacent (lower) existing Boardwalk alignment.
4. Promote a locally focused, and consensual approach, to the successful completion of this valuable community asset.

Respectfully submitted,  
AQION Water Technologies Ltd.



per: W. Wayne Lee P. Eng.

WWL/kl



## PROJECT BACKGROUND

The Ganges Harbour “Boardwalk” was initially conceived in the 1980’s as a collaborative endeavor involving local organizations and community volunteers.

Construction of sections of this project were originally completed, however portions of the overall alignment have remained unfinished, and some areas have consequently fallen into disrepair.

The Boardwalk program has recently been re-activated to undertake an engineering evaluation, and related condition assessments, of the existing site facilities. Additionally, studies of salient and / or potential site constraints related to completion of the overall project have been undertaken.

A “Project Charter”, outlining a comprehensive, multi-step procedure has been established by the Capital Regional District (CRD) to effect completion of this viable, and much desired, improvement to the Ganges Village pedestrian circulation system.

## FEASIBILITY ASSESSMENT PROGRAM

An initial phase of the CRD work program was to complete technical studies concerning specific locations for the boardwalk along the proposed Harbour foreshore alignment.

This study area lies generally between properties adjacent to the Ganges Marina and “Peck’s Cove”, along the Western section of the Harbour.

The scope of work for this analysis was also to confirm the current suitability of the original structure, and to provide feasibility assessments for probable on-going engineering design, or facility upgrade circumstances.

Five specific “feasibility parameters” were selected for these studies.

Because suitable field survey information was not initially available for this program, a comprehensive horizontal control survey was completed to provide credible reference data for the required site investigations.

Subsequently, 4 feasibility parameters, encompassing the archaeological, marine ecological, geotechnical, and structural implications for this project were reviewed to complete these “feasibility” analyses.

Detailed constraint reports for each of these parameters are included in attached Appendices B to E, and a Summary Appendix provides condensed information.

## PROJECT SYNOPSIS

This section of the report provides further commentary regarding the four established “feasibility constraints”, and directly references the ”Summary Appendix” included with this documentation.

### Parameter Work Scopes

The Archaeology and Marine-Eco feasibility parameters are essentially “macro” in nature for this study, in that both topics generally encompass the entire specified BW alignment sector between the Rotary Park and Peck’s Cove terminus points.

The geotechnical study parameter is only partially “macro”, in that in addition to a general assessment of both foreshore and adjacent landform geologic conditions, etc. contingent to the proposed alignment within this specific study area, additional (geotech) references are necessarily made to foundation conditions for the existing built structures, pile bearing unknowns, etc.

The structural constraint analysis however has necessarily been presented in a mostly “micro” context for this assignment, encompassing an assessment of existing original (or “near” original) facilities such as piles and boardwalk constructions within all of sections 2 to 7.

Consequently, the Archaeological and Marine Ecology issues related to sections 6 and 7 have been confirmed as minor constraints, whereas the (inter-related) geotechnical and structural constraints being attributed to sections 6 and 7 are seen to be quite significant.

### Ganges Marina Properties

Obviously a very consequential issue regarding the work scopes prepared for the structural and geotechnical proponents was the future implications of development scenarios for the extensive Marina holdings within this sector of the currently visualized alignment for the Ganges “Harbour Walk”.

Given the current circumstances regarding re-development options for the Marina land holdings, and to some extent the “Fishery” property, the structural and geotechnical constraint reporting completed at this time for sectors 2 to 5 is considered to be preliminary only.

Subsequent sections of this “Project Synopsis” overview briefly the individual study parameter conclusions, etc. which are included with our “Summary Appendix”.

SYNOPSIS cont'd. p. 2

## ARCHAEOLOGY

A standard regulatory procedure, encompassing AOA and PFR work scopes, was followed by Eric McLay for this archaeological feasibility assessment.

His report concludes that "...no recorded or unrecorded archaeological heritage sites are located specifically within the narrowly-defined Ganges Harbour Walk project corridor".

As a possible contingency plan, McLay has noted that the upslope areas adjacent to the Peck's Cove terminus, although not encompassed within the study terms of reference, have not been previously investigated.

## MARINE ECOLOGY

The report conclusions note that "The overall sensitivity of the marine ecosystem along the Boardwalk alignment is relatively low due to a combination of factors....with localized exceptions".

The "localized exceptions" as further described, encompass known or possible forage-fish spawning habitat within sections 1 & 2, seaward of section 6, and North of section 7. An intertidal area in section 4 was also noted for beach organisms.

The sensitivity of adjacent terrestrial ecosystems was considered to be "relatively low", although "substantial infestations" of invasive plant material was observed in sections 4, 6, and 7. (ref. report photos 41 & 42)

## GEOTECHNICAL

Common sense, credible anecdotal information, and practical interpretations of vegetated shoreline features, sub-terrain (pile) foundation conditions, and submerged pile bearing capacities are features of the Ryzuk Geotechnical feasibility study for this project

The report concludes as follows:

"We consider that the existing Boardwalk is generally located within a relatively safe area with respect to potential geological hazards. There are some unknowns with respect to localized conditions, however, we believe that any foreseeable mobilization occurring within the slopes above or below the Boardwalk are expected not to result in catastrophic failure of the structure, though local repairs may be required.

SYNOPSIS cont'd. p.3

## STRUCTURAL

The Stantec structural feasibility report outlines a detailed, and methodical, on-site investigation program completed for the existing Boardwalk facilities, including preliminary capacity assessments of originally installed piles within the assigned stand-alone and infill area locations, i.e. generally sections 3 and 4, and sections 2 and 5, respectively.

A detailed tabulation is provided showing the current suitability, and related technical information, for the essentially modular, existing Boardwalk structural components within each of the 7 defined study sections. (ref. report pages 13-16)

Arguably, this “feasibility” constraint is the most difficult to address for this project.

Currently unknown, or possibly unavailable, construction records from the original construction phases, the nearly 30 year tenure of largely neglected structural components within an unforgiving marine environment, and current building code variances are realistic complications for a preliminary structural evaluations of the current facilities.

Some existing components of the Boardwalk will realistically still have useful value, particularly sub-structural members. Other facility components may be adequate with upgrades or modifications. And obviously, the “top-side” deck boards are now well beyond their useful life.

## Summary Appendix

1. Archaeological “Overview Assessment”  
Eric McLay / August 10, 2018
2. Marine Ecology Summary Report / 26 July 2018  
Alan Whitehead / pp. 9-10
3. Geotechnical Constraint Report / August 27, 2018  
Andrew Jackson / pp.7-8
4. “Ganges Harbour Walk Structural Assessment”  
Stantec / September 10, 2018  
Executive Summary / pp. i, ii

August 10th, 2018

Karla Campbell, Manager  
Salt Spring Island Parks and Recreation  
Capital Regional District

**RE: ARCHAEOLOGICAL OVERVIEW ASSESSMENT AND PRELIMINARY ARCHAEOLOGICAL FIELD RECONNAISSANCE OF CRD's GANGES HARBOUR WALK, SALT SPRING ISLAND, B.C.**

Dear Ms. Karla Campbell,

On behalf of the Capital Regional District, I have prepared the attached 22-page letter report describing an archaeological overview assessment (AOA) and preliminary field reconnaissance (PFR) study of the proposed *Ganges Harbour Walk* project area from CRD's Rotary Park to Peck's Cove Park, Salt Spring Island, B.C.

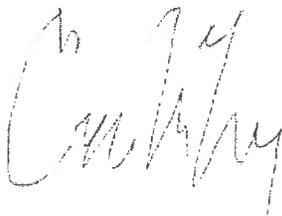
**SUMMARY OF RESULTS**

Based on a comprehensive review of background archaeological site information and the results of the preliminary field reconnaissance, I identify no recorded or unrecorded archaeological heritage sites are located specifically within the narrowly-defined *Ganges Harbour Walk* project corridor. However, I caution that the northern end of the DfRu-009 archaeological site directly overlaps four private waterfront properties that immediately borders the *Ganges Harbour Walk* project area, where any project-related proposals to rezone or expand development require regulation under provincial permits in respect of the *Heritage Conservation Act*, R.S.B.C 1996, Chapter 187.

**HERITAGE CONSERVATION RECOMMENDATIONS**

- In my professional opinion, I recommend that no further preliminary archaeological impact assessment studies or archaeological monitoring are required for the CRD to proceed with the present narrowly-defined scope of the *Ganges Harbour Walk* project corridor;
- If any potential unrecorded archaeological deposits, features or materials are unexpectedly discovered during construction of the *Ganges Harbour Walk*, I recommend that the CRD staff and contractors immediately halt any ground disturbing operations and a qualified archaeologist inspect the location to provide further advice and recommendations for heritage conservation in consultation with the Archaeology Branch and local Coast Salish First Nations.
- As a precautionary measure, if the CRD wishes greater certainty concerning the identification of potential unrecorded archaeological heritage sites on the upland portion of Peck's Cove Park, I recommend that the CRD consider completing a more comprehensive archaeological impact assessment (AIA) study specific to the proposed boardwalk and related parkland improvements.

Respectfully,



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Eric McLay, PhD Candidate, Archaeologist

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It was also noted that the space under the boardwalk's higher north end is currently used as a shelter by homeless persons; tarps having been placed on the decking above and hanging down the sides to create a dry living space.

Segment 7 is entirely terrestrial and does not impinge on the adjacent marine ecosystem. The adjacent marine environment is comprised of bedrock and gravels, and the biota (Table 2) is similar to that found at the higher intertidal elevations of Segment 6; the foreshore here is the widest in the entire study area. It is noted, however, that and-lance spawning habitat has been mapped by the Islands Trust approximately 50 m from Segment 7 at the north end (Figure 5).

The upland environment is comprised of a seaward fringe of shrubs between the boardwalk and the riprap; the area under the boardwalk which is unvegetated due to shade; and a low escarpment area on the landward side that densely vegetated. The back-shore fringe is dominated by shrubs and small trees including oceanspray, gorse, arbutus, bigleaf maple, as well as Himalayan blackberry, grasses and other species. Vegetation on the escarpment area includes variably dense ground cover of alumroot, stonecrop and other forbs; a variety of shrubs such as Indian plum, salmonberry, Himalayan blackberry; and a relatively open canopy of bigleaf maple, Douglas-fir and arbutus. No bird nests or bats were observed on the underside of the boardwalk.

The marine ecological sensitivity in Segment 7 is considered low because of its situation on land above the high tide level. Reactivation of the existing boardwalk will entail considerable renovation including replacement of the decking and railing and may require replacement of one or more pilings. While risk of adverse environmental impacts due to reactivation is considered low, careful planning of any demolition and reconstruction will be necessary to ensure adequate protection of the intertidal habitats.

Terrestrial sensitivity is also considered low. No at-risk species were observed. The density of invasive species such as gorse and Himalayan blackberry is lower than in Segment 6; as at the latter segment, reactivation of the boardwalk may also facilitate control efforts beside Segment 7.

#### 4. Conclusions and Recommendations

Based on the findings presented above, it is concluded that:

1. The overall sensitivity of the marine ecosystem along the boardwalk alignment is relatively low due to a combination of factors including the high intertidal or terrestrial setting, and intensive human use of the adjacent marine waters, with localized exceptions.
2. No critical terrestrial wildlife habitats or at-risk species were found to occur along the alignment.
3. Segments 1 and possibly 3 (Figure 2) are considered to have high ecological sensitivity due to the presence of known or possible forage-fish spawning habitat, which is considered at-risk, on the soft seabed crossed by the alignment in areas where no boardwalk exists at present.
4. Also, forage-fish (Pacific sand-lance, *Ammodytes hexapterus*) spawning habitat, mapped by the Islands Trust (Figure 5), is present on the foreshore seaward of Segment 6 and north

of Segment 7. The former habitat, in particular, may be at risk during reactivation of the existing boardwalk if adequate mitigation is not in place during construction; while the latter is not considered at risk from this project.

5. Segment 4 (Figure 3) is considered to have higher ecological sensitivity due to the presence of a diverse community of intertidal organisms on the rocky or gravel substrate under the existing boardwalk, which would have to be repaired due to its current derelict condition.
6. The sensitivity of the adjacent terrestrial ecosystem is considered relatively low due to either (a) the developed existing condition in Segments 2, 3, 5, 6 and 7; or (b) the low risk of significant disturbance during any future repair or reconstruction of the existing boardwalk in Segments 3, 4, 5, 6 and 7.
7. There are substantial infestations of gorse and, to a lesser extent, Himalayan blackberry and English ivy along the landward side of the alignment, most notably beside Segments 4, 6 and 7, the control of which is desirable.

It is recommended that planning of the boardwalk project's reactivation and completion include the development of an Environmental Management Plan (EMP) to address the need to:

- a. comply with all local, provincial and federal statutes, bylaws and regulations;
- b. protect nearshore water quality in the marine environment;
- c. protect at-risk forage-fish spawning habitat in Segments 1 and 3 and near Segment 6;
- d. protect mid-low intertidal rock and gravel habitat in Segment 4;
- e. protect high intertidal rock and gravel habitat in Segment 6 during any demolition and reconstruction or new construction; and
- f. control invasive terrestrial species, particularly gorse and Himalayan blackberry.

The EMP should be developed through consultations with the CRD, project engineers and designers, experienced marine contractors (ideally including persons involved in the construction of the existing boardwalk sections), the regulatory agencies having jurisdiction, qualified environmental professionals, and land owners and/or lessees. The EMP should also consider water- and land-based accesses, appropriate disposal of demolition debris, and timing of construction to coincide with periods of lowest risk to marine and terrestrial species.

## 5. References

Druehl, Louis D. 2000. **Pacific Seaweeds – A Guide to Common Seaweeds of the West Coast.** Harbour House Publishing, Madeira Park, B.C. 190p

Harbo, Rick M. 1997. **Shells & Shellfish of the Pacific Northwest – A Field Guide.** Harbour Publishing, Madeira Park. 270p

exception was noted within a portion to the immediate north of the Marina jetty within Section 5 where a limited number of the pile footings were observed to have been cast atop the preexisting rocky beach materials (shown in the lower right photograph of page 3). These soils may or may not be fills and the coarse angular rock materials appear to have scoured below the footing locally. The long term support of the piles within this location are therefore not optimum and deformation of such may potentially occur if scour were to continue, or if the soils undergo long term consolidation.

The location at the head of Ganges Harbour may experience minor tsunami inundation as a result of a significant event influencing the Gulf Islands Area. The risk of such to cause damage to the structure is however deemed low due to the location and the anticipated inundation wave amplitude being generally less than 3 m.

#### GEOTECHNICAL RECOMMENDATIONS

Based on our review and the above detailed anecdotal information, we consider the existing piles to be generally serviceable from a geotechnical perspective provided that the members are deemed to be competent by the structural consultant, and in this case it would be reasonable to rely of such for the proposed usage. A noted exception would be for the piles within Section 2 and 3 where the lateral loading differential due to the surrounding boulder fill generally exceeds 45 degrees (0.3 m vertical difference from one side of the pile to the other). We consider that the rocks leaning against the piles could be removed to negate this lateral loading condition, or it may be an option to cast concrete around these locations to solidify the rock mass and reduce the potential for strain should the pile begin to yield under the lateral stress imparted by the boulders. Additionally, piles with foundations bearing atop the rocky beach where scour has occurred should have the support conditions improved with addition of concrete to infill the scoured areas. Notwithstanding the preceding, we expect that should the support conditions of isolated piles be marginal and minor settlement occur in the long term, the timber structure would likely be capable of accommodating minor differential settlement if acceptable to the structural consultant.

If additional piles are proposed for the Boardwalk reconstruction we recommend that such be constructed either directly atop the bedrock or driven to refusal by drop hammer under direct geotechnical supervision. For the structural design considerations, the piles can be considered to be end bearing and the bedrock can be assumed to provide bearing resistance of 1MPa for the 0.3 m diameter pile tip or the 0.9 m diameter spread footing area.

#### CLOSURE

In summary, from a geotechnical perspective we believe that the existing piles would be generally serviceable provided that the remedial works noted above are implemented. We consider that there would not be a specific requirement for further investigation work at this time unless new piles are proposed, which would likely be within the marine areas. In this case, we

would suggest that the new locations could be probed from a barge to review the mud thickness and the depth to bedrock. We would be pleased to assist with the project moving forward.

We consider that the existing Boardwalk is generally located within a relatively safe area with respect to potential geological hazards. There are some unknowns with respect to localized conditions, however, we believe that any foreseeable mobilization occurring within the slopes above or below the boardwalk are expected not to result in catastrophic failure of the structure, though local repairs may be required.

We trust that the preceding is suitable for your purposes at present. Please don't hesitate to contact our office if we can be of further assistance.

Yours very truly,  
Ryzuk Geotechnical

Andrew Jackson, P.Geo. Eng.L.  
Project Geoscientist



Attachment – Terms of Engagement and Site Plan

cc: Wayne Lee, P.Eng. (aqionwater@gmail.com)

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

September 10, 2018

### Executive Summary

Stantec Consulting Ltd. conducted a structural assessment of the existing Ganges Harbour Walk on Salt Spring Island on April 30, 2018. The Ganges Harbour Walk was originally constructed approximately 30 years ago. It appears that the boardwalk was built to appropriate standards and codes at the time of construction. A load rating for the typical configuration of boardwalk was completed and it was found that the sections of the boardwalk in good condition would satisfy the loading requirements of the British Columbia Building Code 2012 (BCBC 2012) for gravity loading. A load rating of the driven piles was not completed as records regarding the depth and capacity of the piles were not available at the time of the assessment. The portions of the boardwalk supported on concrete piers were assessed for current seismic loads and were found to be inadequate. Apart from the in-use sections of boardwalk, which appear to be regularly maintained, the rest of the boardwalk sections have not been properly maintained, which has resulted in significant deterioration. The load rating does not apply to the out-of-use sections of the boardwalk which are currently considered to be unsafe for public use.

There are two sections of boardwalk reviewed that are currently in use. Section 1 refers to the lookout portion of the boardwalk at Rotary Park. This section was found to be in fair to good condition and has been regularly repaired and maintained. A cursory review of the lookout portion of boardwalk was conducted as it may be a possible tie-in location for future boardwalk construction. Section 2 refers to the Fishery boardwalk. This section is in fair to good condition and also appears to undergo regular maintenance and repairs. Stantec has recommended that several repairs are completed for the Fishery boardwalk as part of ongoing maintenance to keep this structure in good condition.

Section 3 and 4 refers to the stand-alone piles located north of the Fishery property. Only piles above the water line in section 3 were accessible during the assessment, these piles were found to be in poor condition. It is assumed that the inaccessible piles in section 4 are also in poor condition based on a visual assessment conducted from the shore. It is Stantec's opinion that these piles, in their current condition, are not feasible for use in any future construction.

There are three sections of boardwalk that were completed during the original construction but are currently not open to the public. Section 5 and 6 are completed portions of the board walk within the high tide region, supported off of the exposed rock by piles with concrete piers. Section 5 was not accessible during the review, but it is assumed to be in similar condition to section 6. The substructure of section 6 appears to be in fair condition aside from the concrete piers which are in poor condition. Replacement of a percentage of substructure components and extensive repair of the concrete piers, would be required in order to restore the existing boardwalk. Section 7 refers to the portion of boardwalk and lookout platform located nearest to Peck's Cove Park. This section of boardwalk is above the high tide line and is supported on driven piles. The substructure appears to be in fair condition and would likely only require minor repairs and replacements. In order to restore the piles, further testing would be required to determine the depth and capacity. The pile-caps and stringers of the out-of-use sections were found to be in fair condition and it is anticipated that only minor repairs and replacements would be required to these components. The deck and railings of all three out-of-use sections are in poor condition and would likely require full replacement.

The expected lifespan of regularly maintained creosote-treated timber piles is approximately 50 years. The Harbour walk is approximately 30 years old and several sections have not been properly maintained during this time. If the existing piles were to be successfully re-purposed, they would likely only have 20 years of useful life, prior to requiring extensive maintenance and repairs. Once a conceptual alignment/ design of the Harbour Walk has been completed,



## **GANGES HARBOUR WALK STRUCTURAL ASSESSMENT**

Introduction  
September 10, 2018

a cost-benefit analysis would be recommended to determine whether it is feasible to restore the boardwalk existing boardwalk in sections 5-7 versus a full replacement.

There are two stone/ concrete stairs that access the beach from the boardwalk and one timber stair that accesses the boardwalk from Lower Ganges Road. All three stairs were rated in fair condition; however, it was noted that the stone/ concrete stair do not meet all of the requirements of the BCBC 2012. Minor maintenance and repairs have been recommended for all three stairs.

## Appendix A

### Horizontal Control Survey Mapping (Polaris)

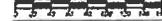
SITE PLAN ALONG GANGES HARBOUR AND LOWER GANGES ROAD, SALT SPRING ISLAND.

SHEET 1 OF 3

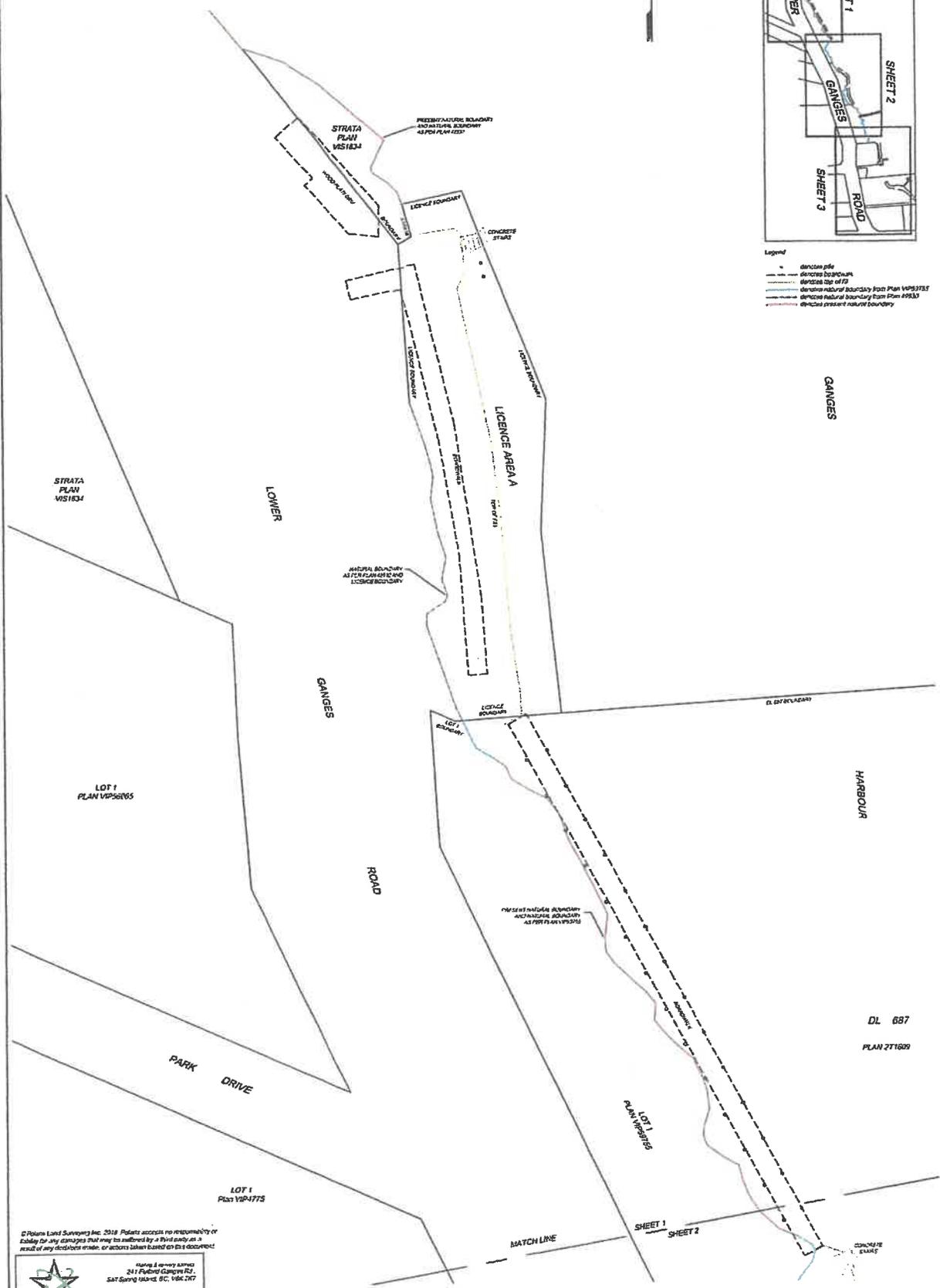


All dimensions are in metres and do not include ground surface elevations unless noted.  
The horizontal plan of this plan is 550mm in size by 660mm in height (D-Scale) when plotted at a scale of 1:200.

Key Plan  
Scale 1:2500



- Legend
- - - - - concrete pile
  - - - - - concrete boundary
  - - - - - concrete top of FD
  - - - - - concrete natural boundary from Plan VPS3787
  - - - - - concrete natural boundary
  - - - - - concrete present natural boundary



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Date: 2018-04-23  
Time: 09:09  
Drawing: 2176-004-LOCAL-030  
Layout: Site Plan

SITE PLAN ALONG GANGES HARBOUR AND LOWER GANGES ROAD, SALT SPRING ISLAND.

SHEET 2 OF 3

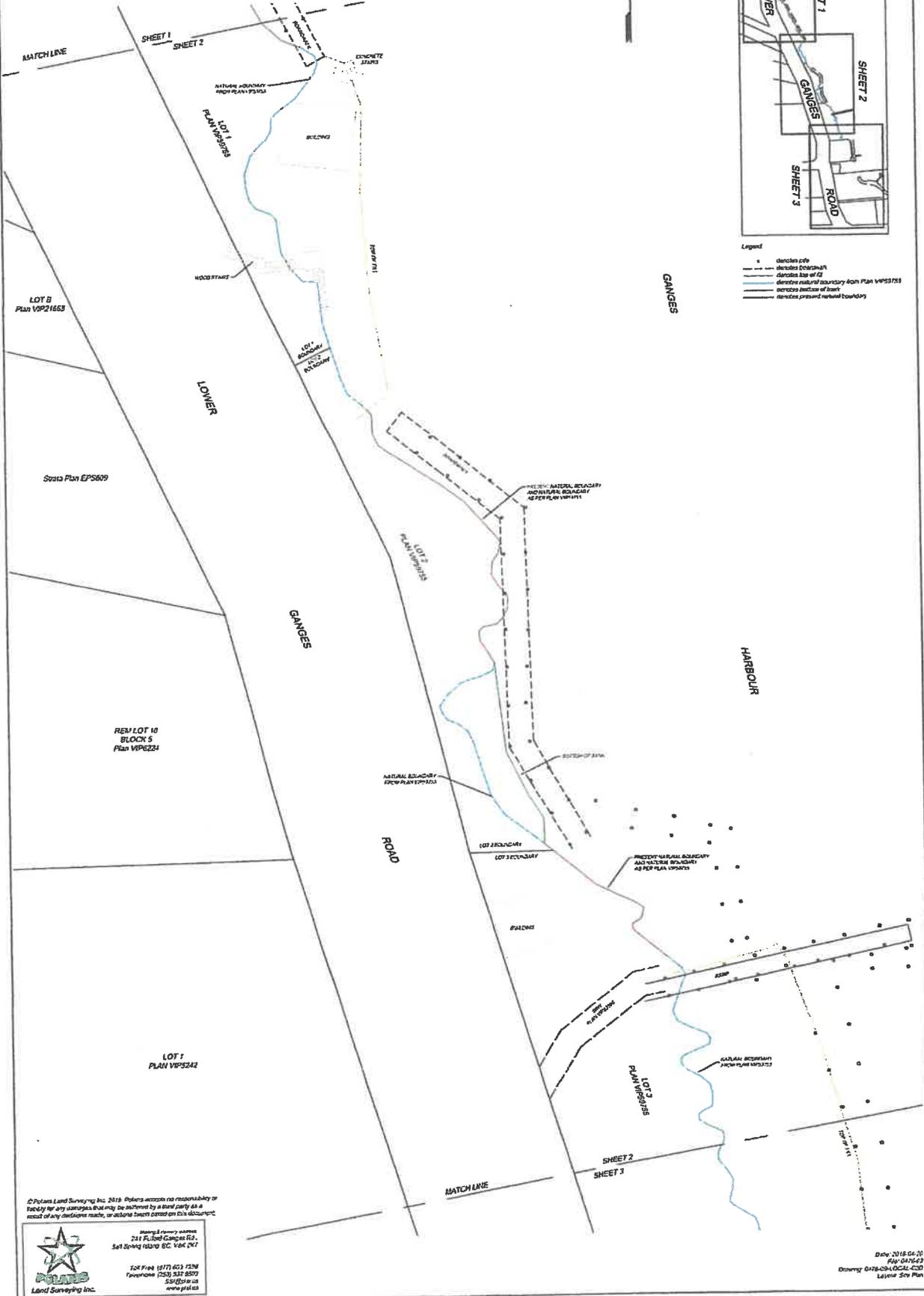


All distances are in metres and decimals thereof, unless otherwise noted.  
The intended plot size of this plan is 552mm or 567mm in length (A0-Size) when plotted at a scale of 1:2000.

Key Plan  
Scale 1:2500



- Legend
- denotes site
  - - - denotes O/S boundary
  - denotes lot of O/S
  - - - denotes natural boundary from Plan VPS3751
  - denotes section of boundary
  - denotes proposed natural boundary



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Date: 2018-05-21  
File: 18164-3  
Drawing: 0478-024-00-01-420  
Landscape Site Plan

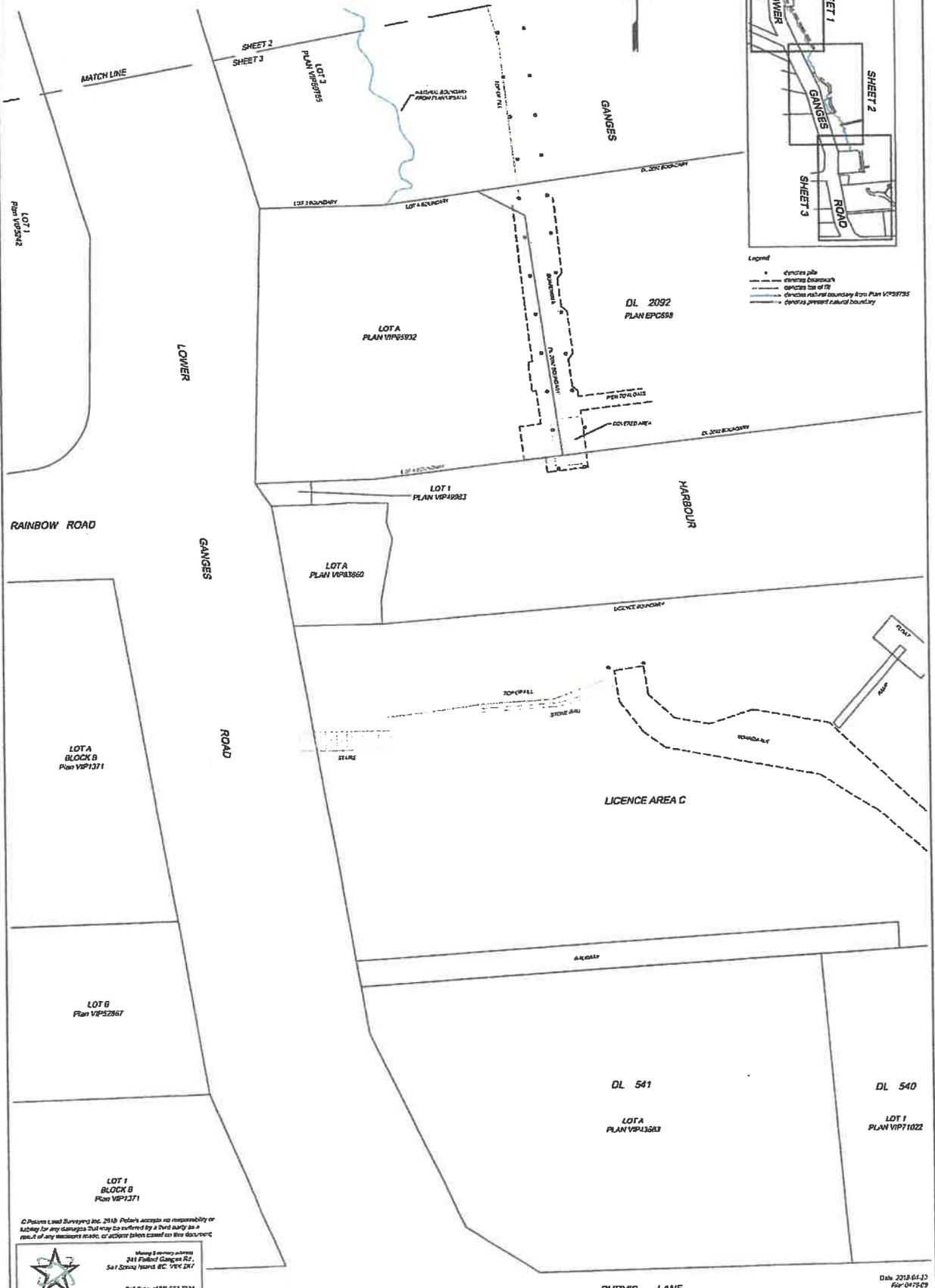


All distances are in metres and decimal's thereof, unless otherwise noted.  
 The intended plot size of this plan is 850mm on a width by 560mm on a height (A1-C1) when plotted at a scale of 1:200.

Key Plan  
 Scale 1:2500



Legend  
 - - - - - existing path  
 - - - - - existing easement  
 - - - - - proposed lot of 76  
 - - - - - Ganges natural boundary from Plan VP58733  
 - - - - - depicts present natural boundary



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HEREFORD AVENUE

PURVIS LANE

Date: 2018-04-13  
 File: 047520  
 Drawing: 0476404-001-C-03  
 Layout: Site Plan

## Appendix B

### Archaeological Constraint Report (McLay)

## ARCHAEOLOGICAL OVERVIEW ASSESSMENT AND PRELIMINARY FIELD RECONNAISSANCE STUDY OF CAPITAL REGIONAL DISTRICT'S GANGES HARBOUR WALK, SALT SPRING ISLAND, BRITISH COLUMBIA

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On behalf of the Capital Regional District (CRD), I describe in this letter report an archaeological overview assessment (AOA) study and preliminary field reconnaissance (PFR) study of the *Ganges Harbour Walk* project, Salt Spring Island, B.C. (Figure 1 and 2).

### 1.0 Ganges Harbour Walk Project, Salt Spring Island

As a collaborative project between the CRD, Salt Spring Islands Trust Committee (SSITC) and its partners, the *Ganges Harbour Walk* project plans to create a continuous harbour-side boardwalk or 'seawalk' between the CRD's Rotary Park and Pecks Cove Park in Ganges Harbour (CRD 2018; Salt Spring Islands Trust Committee 2018).

In the 1980s, the planning, design and implementation of a harbourside boardwalk was partially completed on the north end of Ganges Village through the cooperation of community volunteers, public agencies and upland property owners. Securing public access across private waterfront lands could not be established at that time, however, and the boardwalk was never completed (CRD 2018). The existing wooden structures have not been maintained and public access is currently prohibited. In 2014, the SSITC and CRD began a new planning and community engagement process to revive the harbour walk project as part of the broader *Ganges Village Plan* (SSITC 2014). Under the project charter, the CRD is responsible for project planning and implementation, including undertaking preliminary assessment studies, such as structural engineering reports, geotechnical, environmental, and archaeological studies (CRD 2018).



**Figure 1** View of existing wooden boardwalk built in the 1980s on the foreshore underneath shoreline cliffs east of Lower Ganges Road, Salt Spring Island, March 16, 2018. Looking North.



**Figure 2** Ganges Harbour Walk Project Area, Salt Spring Island, B.C. (CRD Regional Map 2018).  
<https://maps.crd.bc.ca/>

Beginning at CRD's Rotary Park, the proposed harbourside route spans a short 350m distance north to CRD's Peck's Cove Park (Table 1, Figure 2). In between these pathway destinations, the harbour walk route on the foreshore crosses through a complex of private upland waterfront properties with mixed residential, commercial and industrial zoning, as well as several Crown-leased commercial water lots, where public access must be negotiated and secured by the establishment of a statutory right-of-way (SSITC 2015, Table 1). In 2016, a preliminary Bylaw No.491 was drafted by the Salt Spring Islands Trust Committee to amend zoning to allow increased development potential for four private upland properties as an incentive for negotiations (SSITC 2016). While the community engagement process demonstrated broad public support for the concept of the *Ganges Harbour Walk* project by Salt Spring Island residents, external government agencies and First Nations, several concerns were raised related to potential impacts to the character of Ganges Village, including potential impacts to recorded and unrecorded archaeological heritage sites (SSITC 2016; Sjuberg 2017).

**Table 1** Ganges Harbour Walk - Adjacent Upland Properties and Water Lots

<b>PID and PIN</b>	<b>Property Description</b>	<b>Area (ha)</b>	<b>Current Zoning</b>	<b>DISTANCE (m)</b>
-	Rotary Community Park (CRD)	0.201	Park (PR5)	0
012-724-794 90062109	145 Lower Ganges Rd. (Cudmore Gap) Lot A, Section 1, Range 3 and 4 East, North Salt Spring Island, and District Lot 2066, Cowichan District, VIP83860	0.167	Residential (R6)	14
017-088-364	Lot 1 (Farmer's Institute), Section 2, Range 3 and 4 East, North Salt Spring Island, Cowichan District, VIP49983	0.015	Residential (R6)	2
012-882-921 14316870	151 Lower Ganges Rd. (The Fishery) Lot A, Section 2, Range 4 East, North Salt Spring Island, and District Lot 689, Cowichan District, VIP65932	1.027	Industrial (In1)	35
90122010	District Lot 2092 (bordering Lot A above)	1.820	Shoreline (S6)	--
018-951-775	161 Lower Ganges Rd. (Ganges Marina) Lot 3 Section 2, Range 4 East, North Salt Spring Island, Cowichan District, VIP59755	0.950	Commercial (CA1d)	70
018-951-767	Lot 2 (Ganges Marina) Section 2, Range 4 East, North Salt Spring Island, Cowichan District, VIP59755	0.413	Residential (R6)	70
018-951-759	Lot 1 (Ganges Marina), Section 2, Range 3 and 4 East, North Salt Spring Island, Cowichan District, VIP59755	1.097	Residential (R6)	100
14487370	District Lot 687 (bordering Lot 1, 2, 3 above)	3.830	Shoreline (S1)	-
-	Peck's Cove Park (CRD)	0.080	Residential (R6)	60
	<b>Total</b>			<b>±350</b>

In the CRD's preliminary planning and assessment studies phase, no detailed conceptual design or engineering plans are presently available to review for the *Ganges Harbour Walk* project; however, the general plan of the *Ganges Harbour Walk* project is to follow the existing harbourside route, establish statutory right-of-ways through upland properties for public access, upgrade and improve the existing wooden boardwalk where structurally sound, complete existing gaps in the boardwalk, and install new pathway features, such as metal railings, lighting and signage (CRD 2018, SSITC 2018).

Land alterations in the course of the *Ganges Harbour Walk* project that may cause ground disturbance potentially involve the removal and replacement of old creosote posts and concrete piers on the foreshore; the installation of new concrete piers to complete existing gaps in the boardwalk; the upgrading of sections of the existing wooden boardwalk, including the viewing platform and wooden entrance ramp at the CRD's Pecks Cove Park, and the potential construction of a bridge at CRD's Rotary Park. The schedule for the CRD to complete preliminary assessment studies for the *Ganges Harbour Walk* project is the 2018/2019 fiscal year.

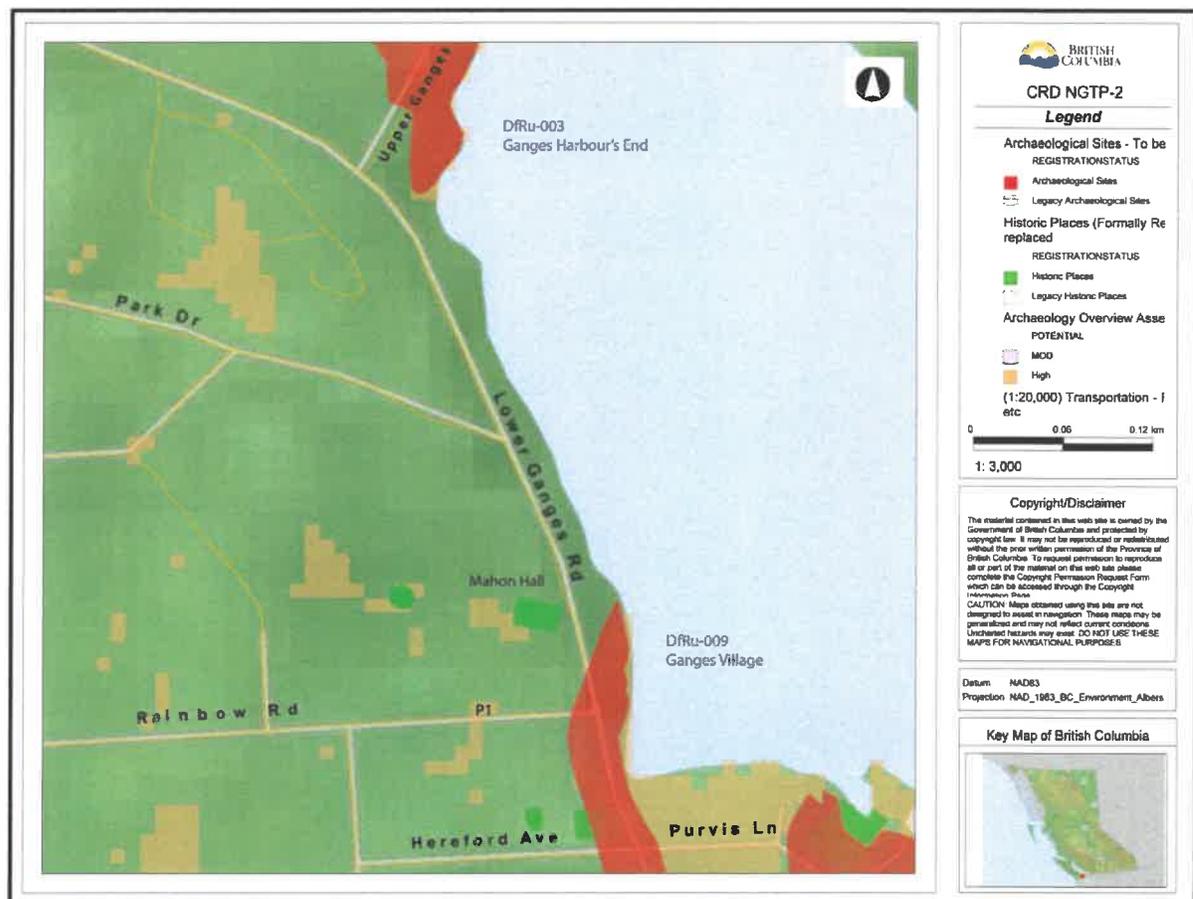
The Capital Regional District Salt Spring Island has requested this archaeological overview assessment (AOA) study and preliminary field reconnaissance (PFR) to help address whether there may be a need for additional heritage inspection or mitigation under provincial heritage permits prior to any proposed land-altering improvements for construction of the boardwalk, as regulated by the Archaeology Branch, Ministry of Forests, Lands, Natural Resource Operations and Rural Development (FLNRORD), in respect of the provincial *Heritage Conservation Act*, RSBC 1996, Chapter 187.

## 2.0 Provincial Heritage Site Registry – DfRu-003 and DfRu-009, Ganges Harbour

In examination of the provincial Archaeology Branch's *Remote Access to Archaeological Data* (RAAD) website, I identify that two archaeological sites (DfRu-003 and DfRu-009) are located in the general vicinity of the *Ganges Harbour Walk* project area (Figure 3). These two large ancient village sites in Ganges Harbour are known by the Coast Salish place-name *Shiya'hwt*, or SYOWT, translated as "Make it Cautious", in the Sencoten and Hul'q'umi'num' languages (Elliott and Poth 1990; HTG 2005; Rozen 1985).

### 2.1 DfRu-003 Ganges Harbour's End, or Shiya'hwt/SYOWT Village

The DfRu-003 site is recorded as a large, deep coastal archaeological settlement and burial ground located at the head of Ganges Harbour (Arcas 1993, 1994, 2006; Cassidy and Seymour 1974; Cassidy et al. 1974; Golder 2009; Hill 1971; McLay 2017; Pratt 2012, Simonsen 1983). In 1974, the DfRu-003 site along Upper Ganges Road was estimated to measure approximately 300m in length by 40m in width and up to 2.0m in depth (Cassidy and Seymour 1974, Figure 4). At that time, this village site was evaluated to have been negatively impacted by past road construction, commercial development and shoreline erosion. In 1992, an archaeological impact assessment (AIA) study was directed by the Ministry of Transportation to assess the proposed upgrading and widening of Upper Ganges Road (I.R. Wilson 1992). Large-scale archaeological excavations at DfRu-003 were subsequently undertaken to mitigate proposed road developments on the upland side of Upper Ganges Road, which revealed significant archaeological settlement deposits dating to the past 2200 years (Arcas 1993).



**Figure 3** Provincial Archaeological Overview Assessment Map of *Ganges Harbour Walk* project area, Salt Spring Island, B.C. Remote Access to Archaeological Data Website, Archaeology Branch, MFLNRORD, Accessed March 15th, 2018.

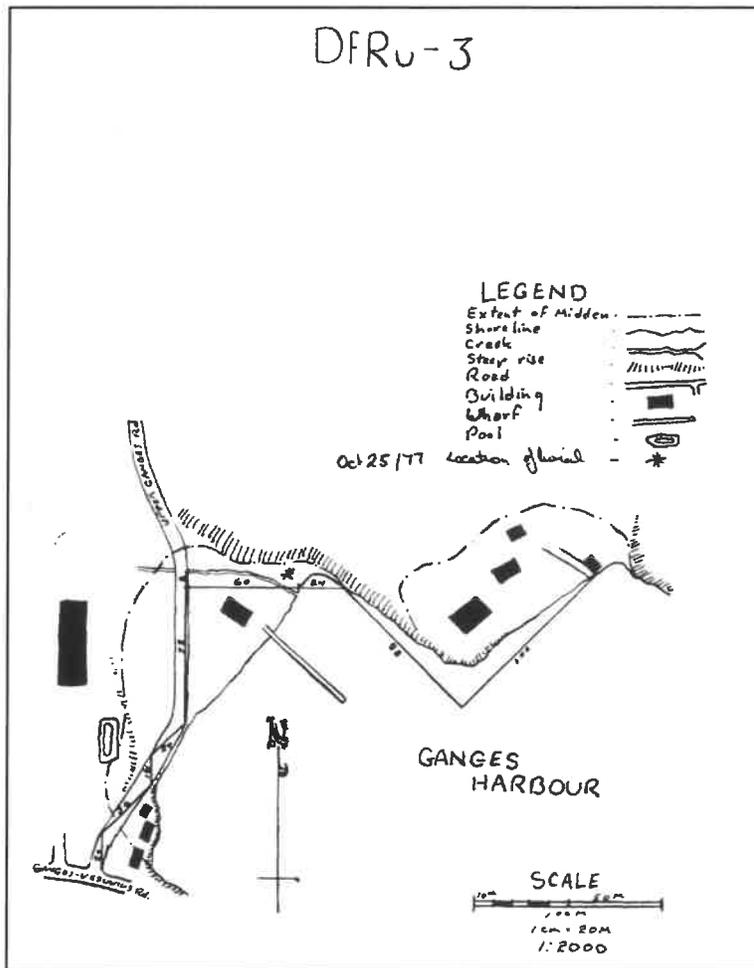


Figure 4 1974 Provincial Site Form Map, DfRu-003, Ganges Harbour's End, Salt Spring Island, B.C.

In 1994, an archaeological impact assessment study of the proposed subdivision of the Harbour House Hotel property at the corner of Lower and Upper Ganges Road further refined the northern and western extent of the DfRu-003 archaeological site boundary (Arcas 1994). In 2006, archaeological monitoring later identified a small area of previously unrecorded archaeological shell deposits on this same property during construction of the Bayside Condominium project (Arcas 2006). In 2014, archaeological monitoring and small-scale test evaluations for the CRD's new walkway and public information kiosk on the waterfront side of Upper Ganges Road further identified deep, intact archaeological deposits preserved along the road shoulder, which had previously been considered damaged or destroyed by past road construction (McLay 2017).

In review, I identify that *Ganges Harbour Walk* project area is located outside of the recorded boundary of the DfRu-003 site located at the head of Ganges Harbour, as defined by provincial site form maps and the Archaeology Branch RAAD website (Figure 3 and 4). No potential archaeological impacts are anticipated to the DfRu-003 site within the current scope of the project.

## 2.2 DfRu-009 Ganges Village

The DfRu-009 site is recorded as a very large coastal archaeological settlement and burial site, measuring approximately 640m x 40m, that underlies downtown Ganges village and its surrounding shoreline, including Grace Islet (Cassidy and Seymour 1974; Hill 1971; Madrone 2013; Mathews, McLay and Smith 2016; McLay 2018, Millennia 2014; Simonsen and Somogyi 2010; Theissen 2013).

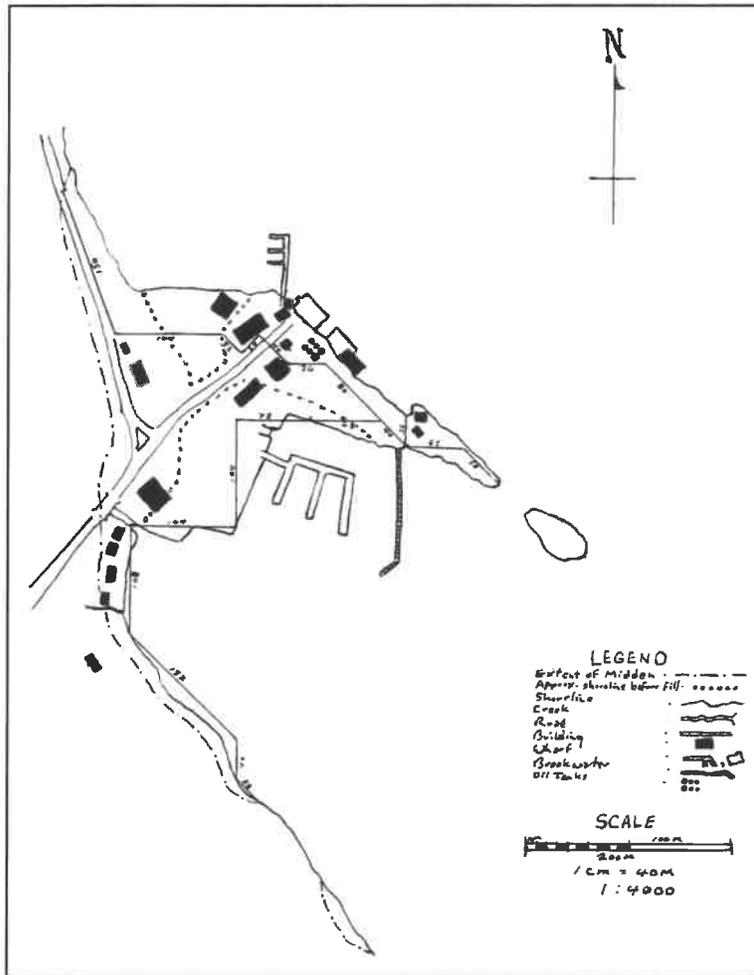
Over the past 50 years, the DfRu-009 site in Ganges Village has been encroached upon by expanding urban and commercial development. As amateur archaeologist and local Salt Spring island resident Beth Hill (1971) described, “the midden has been completely buried under fills for the Centennial Park, the Fire Hall, and the new Mouat Store. As shown in early photographs of Ganges, a narrow clamshell spit joined the Ganges dock to the main area of the village.” (Figure 4).



**Figure 5** “Ganges Inn, Salt Spring Island.” Unknown photographer and date (circa 1904-1911). BC Archives, Victoria [Acc. HP075213; 193501-001]. The shoreline in the foreground indicates this photograph was taken from the scenic viewpoint on what is now “The Fishery” property overlooking the CRD’s Rotary Park.

Hill (1971, Part 2) documented several artifacts held in private collections from this large, ancient village settlement on Salt Spring Island, including a nephrite celt, a polished ground slate bowl recovered near Mouat’s store, and a very large ground slate point described to have been collected on the north end of Ganges Village “at the present location of Mahon’s Hall, in front of the school, Ganges”.

On February 24<sup>th</sup>, 1974, provincial archaeologists Steven Cassidy and Brian Seymour first systematically mapped the DfRu-009 site at Ganges Village as part of a regional survey of archaeological sites in the Southern Gulf Islands (Cassidy and Seymour 1974; Cassidy *et al.* 1974). During the 1974 survey, they acknowledged the site had been largely buried under fill by roads, buildings and other modern land development. As described, “the midden has been built on, turned over, filled etc. to the point that only the extremities are still available to protect; or even see! (Stephen Cassidy Field Notes, RBCM, February 24, 1974). Nevertheless, the extent of the DfRu-009 site was broadly interpreted by the provincial archaeologists to encompass the majority of the Ganges Village and its shoreline, including its north end following Lower Ganges Road toward Mahon Hall (Figure 6).



**Figure 6** 1974 Provincial Site Form Map, DfRu-009, Ganges, Salt Spring Island, B.C.

The depth of archaeological deposit, the provincial archaeologists suggested, “at one time at least 1 metre, though the remaining deposits – except perhaps those on the spit immediately north of the breakwater – are shallower than this (DfRu-009 1974 Site Form). Importantly, it must be noted the 1974 provincial survey methods comprised a visual inspection of the site, where no subsurface test excavations were completed to help evaluate the site’s depth, stratigraphy, and integrity, or precisely determine the extent of the DfRu-009’s site boundaries. These methodological constraints were acknowledged by the provincial archaeologists, “Traces of midden can be found as one continues through Ganges but in most cases it is impossible to tell what is fill and what is intact. An approximation of the shoreline prior to filling has been placed on the map with the site form for DfRu 9. Whether or not portions of the midden under the fill are still intact or not is not known.” (Steven Cassidy, Field Notes, RBCM, February 24, 1974).

Since 1974, major road infrastructure and commercial expansion projects have continued in Ganges Village. The only documentation of the DfRu-009 site during this period is a hand-written note by provincial archaeologist Steven Acheson, who in 1998 briefly commented on the old site form that, “Pockets of intact shell midden have been exposed during various construction activities alongside Ganges Road at the main junction and appear to extend under the road.”. This comment provides some evidence that intact archaeological deposits at DfRu-009 may remain preserved underneath the roads and buildings in Ganges, particularly along the former shoreline following Lower Ganges Road and Fulford-Ganges Road.

In 2011, for example, archaeological monitoring of the demolition for an old store foundation at 145/150 Fulford Ganges Road revealed several areas of intact archaeological shell deposits and burial features at DfRu-009 that had been preserved under the building buried deeply in underlying soils (Thiesson 2013).

Most recently, the DfRu-009 site boundary has been revised to correct administrative mapping errors introduced into provincial records (McLay 2018). Revisions included reconstructing the historical shoreline of Ganges site prior to large-scale waterfront land reclamation projects in the 1960s and slightly amending the southern end of the site boundary. The northern end of the DfRu-009 archaeological site boundary toward the corner of Lower Ganges Road and Rainbow Road, however, remains largely unchanged based on provincial survey data last recorded in 1974 (Figure 6).

In review, I evaluate that the narrowly-defined route of the *Ganges Harbour Walk* project area is located outside of the presently recorded DfRu-009 site boundary, as defined in provincial site form maps and Archaeology Branch's RAAD website (Figure 3 and 5). However, I confirm that the CRD's Rotary Park and four upland private properties immediately adjacent to the project area overlap with the DfRu-009 recorded archaeological site, including 141 Lower Ganges Road (the "Cudmore Gap"); Lot 1, Plan 44984 ("Famer's Institute"); 151 Lower Ganges Rd ("The Fishery"), and 161 Ganges Road, Lot 3 ("Ganges Marina") (Table 1, see Figure 19 and 20). As a qualification, however, it must be acknowledged that current archaeological site information this specific area of the DfRu-009 site is based on provincial survey data collected in 1974. Over the past four decades, it is evident that there have been extensive land alterations along Lower Ganges Road and adjacent waterfront private lands located within the DfRu-009 site boundary. The preservation and extent of archaeological deposits on the upland properties that overlap the north end of the DfRu-009 site is presently unknown.

### 2.3 Potential for Unrecorded Archaeological Sites

In review of the Archaeology Branch's RAAD *Regional Archaeological Overview Assessment* model, no areas of high statistical potential for unrecorded archaeological site locations are identified along the shoreline within the *Ganges Harbour Walk* project area, including Peck's Cove Park (Figure 3). In general, the shoreline of this northwest section of Ganges Harbour represents steep, inaccessible cliffs above a rocky foreshore. Nevertheless, the shoreline located in between the two large ancient villages, DfRu-003 and DfRu-009, has remained largely undeveloped over the past century. Prior to the construction of Lower Ganges Road, it is likely an indigenous trail traversed the shoreline to connect peoples between these two villages. Importantly, the Archaeology Branch's RAAD AOA website does not specifically model for intertidal archaeological sites, such as fish traps, weirs, perishable wet-sites, or submerged settlements etc. In a comprehensive regional survey of the Southern Gulf Islands between 2006 to 2015, however, I did not identify any intertidal clam garden features within the current project area in Ganges Harbour. Overall, I interpret that there exists moderate potential for small, low-visibility intertidal and upland archaeological sites in the project area.

When accessing archaeological information on the Archaeology Branch's RAAD website, it is expressly cautioned, "The Province makes no warranties or representations regarding the accuracy, timeliness, completeness or fitness for use of any or all data, data products or services provided by the Archaeology Branch.". Site location information in the provincial heritage site registry may not always be accurate and may contain errors. Importantly, the provincial heritage site registry is incomplete and there exists unrecorded site locations that are not presently identified in provincial records.

In the following section, I describe the results of a preliminary field reconnaissance (PFR) study of the *Ganges Harbour Walk* project area to visually inspect and further evaluate whether additional archaeological inspection or monitoring work under provincial heritage permit may be required by the CRD prior to future project planning and implementation.

### 3.0 RESULTS OF A PRELIMINARY FIELD RECONNAISSANCE

On March 16th, 2018, I directed a preliminary field reconnaissance (PFR) study of the *Ganges Harbour Walk* project area accompanied by Coast Salish hereditary grave workers and experienced field assistants, Mr. Adrian Underwood, Tsawout First Nation, and Mr. Harold Joe, Cowichan Tribes.

#### 3.1 CRD's Rotary Park to The Fishery

We began our reconnaissance at the CRD's Rotary Park on the north end of Ganges Village (Figure 6). In 1995, Rotary Park was one of the last major land reclamation projects to expand Ganges Village. As viewed on historical air photos, the park is built on artificial fill atop what was formerly a shallow mud flat north of the tombolo sand spit in Ganges (see Figure 5 and 19).



**Figure 7** General view of the beginning of the proposed Ganges Harbour Walk route at CRD's Rotary Park. The upland properties to the left are known as "Cudmore's Gap" (145 Lower Ganges Rd.), the "Farmer's Institute" (Lot 1, Plan 44893) and the property at centre is "The Fishery" (151 Lower Ganges Rd). Looking North.

The western edge of the CRD's Rotary Park follows the historical shoreline, which formerly presented a flat terrace atop of a steep, low sedimentary cliffs (Figure 19). The north end of the DfRu-009 archaeological site boundary is recorded parallel to Lower Ganges Road as far north as the Ganges Marina across from Mahon Hall (Figure 3 and 20).

The property immediately to the northwest of the CRD's Rotary Park is locally known as "Cudmore's Gap" (145 Lower Ganges Road), a small, undeveloped parcel of residentially-zoned land (0.167 ha) on the edge of the historical shoreline (Figure 7). The waterfront of this property been partially filled in with rock rip rap on the foreshore (DL 2099) (SSITC 2015: 3). Adjacent to this property is a narrow corridor of land (Lot 1, Plan 44983), measuring approximately 6.0m x 2.5m in size, which provides waterfront access for the Salt Spring Island Farmer's Institute (SSITC 2015: 4). Rock rip rap has also been previously deposited on the edge of the shoreline, where a gated wooden staircase provides entrance to a floating dock (Figure 7). The shallow mud flat in front these two private upland properties preserve the last remainder of the natural sandy foreshore on the north end of Ganges Village.



**Figure 8** Entrance to 'The Fishery' property (151 Lower Ganges Rd) with Ganges Marina (Lot 3) property in background on right and Mahon Hall on left. Looking North.



**Figure 9** View of 'The Fishery' property (151 Lower Ganges Rd) overlooking reclaimed foreshore lands (DL 689), where a statutory right-of-way has been previously established for the boardwalk. Looking East.

The property to the north is known as "The Fishery" (151 Lower Ganges Road), a small industrially-zoned parcel of land that formerly was used as a historical log dump and booming ground in Ganges Harbour (Figure 8, 9 and 10). In 1993, this private upland property was commercially expanded by in-filling the foreshore and construction of an concrete upper retaining wall (DL 689) (Figure 9). At that time, a statutory public right-of-way was established for the boardwalk (SSITC 2015: 5).

To span the existing gap in the *Ganges Harbour Walk* project corridor between the CRD's Rotary Park and The Fishery, a number of options have been proposed in staff reports and correspondence over the years, including the installation of a new elevated pier and walkway, the potential in-filling of the foreshore in front of Cudmore Gap, or possibly the construction of a new foot bridge.



**Figure 10** 1980s photograph of the historical shoreline at The Fishery and Ganges Marina properties, which were used at that time, respectively, as a log dump and a bulk oil/gasoline storage facility and marina (Photograph by Barbara Decker, Salt Spring Island Archives. [www.saltspringarchives.com/dekker/pages/2007027152.htm](http://www.saltspringarchives.com/dekker/pages/2007027152.htm))

### 3.2 Ganges Marina Lot 1, 2 and 3 /District Lot 687

Ganges Marina Ltd. (161 Lower Ganges Rd.) owns three upland waterfront properties (Lot 1, 2 and 3) and leases a large Crown water lot (DL 687) located in the centre of the proposed *Ganges Harbour Walk* project area (SSITC 2015: 6-7, Figure 2). Lot 3 (0.9ha) represents the commercially-zoned (CA1d) property where Ganges Marina operates its business, while Lot 1 and 2 are presently undeveloped, residentially-zoned (R6) waterfront parcels located on the east side of Lower Ganges Road. In the 1960s, the marina started operations as part of the Gulf Oil Plant and Marina, an outgrowth of the mid-century use of the property for bulk oil and gasoline storage and as a marine refueling station in Ganges Harbour (Driftwood 1967, Figure 10).

Lot 3 and DL 687 currently present a large gap in the existing *Ganges Harbour Walk* route. In 2006, the foreshore in front of Lot 3 was partially in-filled to expand the property's potential for future commercial development (SSITC 2015: 6-7). The proposed route of the *Ganges Harbour Walk* will presumably follow the outer perimeter of these recently reclaimed lands in DL 687 (see Figure 20).

The shoreline of Lot 2 represents steep, approximately 10m-high sedimentary cliffs. To access the project area, we walked north along Lower Ganges Road to Lot 1, where a wooden staircase provides access to the existing boardwalk within DL 687. At the base of the staircase, a narrow strip of reclaimed land built of rock rip-rap extends over an approximate 40m distance along the foreshore. A small aluminum-sided boathouse has been previously constructed at this location (Figure 11).

To the south, a 50m-long section of existing wooden boardwalk leads beneath the steep shoreline cliffs to the boundary of Lot 3. During our preliminary field reconnaissance, the tide reached a daily low of 2.1m chart datum, which provided a sufficient opportunity to inspect the existing boardwalk structure within the high to mid intertidal zones throughout the project area. The upper intertidal zone of the foreshore adjacent to Lot 2 consists of a very narrow strip of rocky foreshore consisting of large boulders and cobbles atop of shale bedrock. The existing boardwalk bordering Lot 2 has not been maintained and this section has been physically blocked from public access.



**Figure 11** Shoreline staircase on south end of Lot 1 that descends from Lower Ganges Road to narrow strip of reclaimed land on foreshore within DL 687, where a boathouse has been previously constructed. Looking North.

To the north beyond the boathouse adjacent to Lot 1, the constructed terrace ends and an approximately 70m-long, straight section of the existing wooden boardwalk has been built on the natural foreshore beneath steep, 10m-high shoreline sedimentary cliffs (Figure 12, 13 and 14).

The foreshore beneath and surrounding the existing wooden boardwalk represents a relatively flat, rock platform composed of exposed shale bedrock with minor surficial amounts of glacial cobbles and boulders (Figure 13). The rocky foreshore presented little sedimentation that could potentially obscure any potential subsurface intertidal archaeological deposits, features or materials. The exposed shale bedrock on the foreshore continues underneath the western shoreline cliffs to the head of the bay at Ganges Harbour's End.

While we completed a careful inspection of the upper to mid intertidal zone along the length of the foreshore from Lot 2 to the CRD's Peck's Cove Park, we did not observe any unrecorded intertidal archaeological features or materials in the project corridor, such as may be suggested by the presence of fire-altered rocks, eroded artifacts, lithic debris, or petroglyph boulder rock art.



**Figure 12** Existing wooden boardwalk section within DL 687 adjacent to Lot 1 built atop of a rocky foreshore of exposed shale bedrock. Looking North.



**Figure 13** View of exposed shale bedrock rock platform environment underneath existing wooden boardwalk adjacent to Lot 1. Looking North.



**Figure 14** View of foreshore beneath existing wooden boardwalk adjacent to Lot 1. Harold Joe and Adrian Underwood in distance. Looking South.

During our preliminary field reconnaissance, we further did not observe any physical evidence of unrecorded archaeological shell deposits, subsurface features or materials visibly eroding from any natural soil exposures along the length of the upland shoreline cliffs. While the inspection of upland private properties for unrecorded archaeological sites was not a specific objective of this study, the scope of the preliminary field reconnaissance reasonably extended to making observations that were visible from the shoreline and its pathways within the proposed *Ganges Harbour Walk* project corridor.

### 3.3 CRD's Peck's Cove Park

Peck's Cove Park (0.08 ha) and Peck's Anchorage Boardwalk are described, respectively, as a linear park and a developed trail in the CRD's Salt Spring Island's Park System Masterplan (Landworks Consultants 2011). Peck's Cove Park was originally created as part of the subdivision process for Kingfisher Cove's townhouse complex in the 1990s (SSITC 2015). The Peck's Anchorage Boardwalk provides a wooden ramp that rises from the shoreline to connect with Peck's Cove Park (Figure 15 and 16). The existing wooden boardwalk is built on a narrow strip of reclaimed land constructed of rock rip-rap, which extends for approximately 70m along the foreshore (Figure 15 and 16). The wooden boardwalk and viewing platform at Peck's Cove Park have are presently closed off from public access (Figure 17). The remains of a temporary homeless shelter was observed underneath the wooden platform entering into Peck's Cove Park, where modern shell fragments were observed scattered on the ground surface, notably introduced species of Japanese oyster.

The upland portion of Peck's Cove Park appears as a road shoulder or off-ramp from Lower Ganges Road, where past road construction has excavated deeply into the sloping terrace above the shoreline cliffs (Figure 18). From review of historical air photos, however, the former road through Peck's Cove Park may represent the original course of Lower Ganges Road before it was straightened into its present configuration in the 1950-1960s.



**Figure 15** Beginning of existing wooden ramp at Peck's Anchorage Boardwalk that leads upland into Peck's Cove Park. The boardwalk is situated on reclaimed land built of rock rip-rap on the foreshore beneath steep, 6m to 8m-high shoreline cliffs. Looking North.



**Figure 16** View of wooden ramp at Peck's Anchorage Boardwalk leading upland into Peck's Cove Park. Looking Southwest.



**Figure 17** Wooden viewing platform and boardwalk access point at Peck's Cove Park. Looking Southeast.



**Figure 18** View of Peck's Cove Park from shoulder of Lower Ganges Road. Looking South.

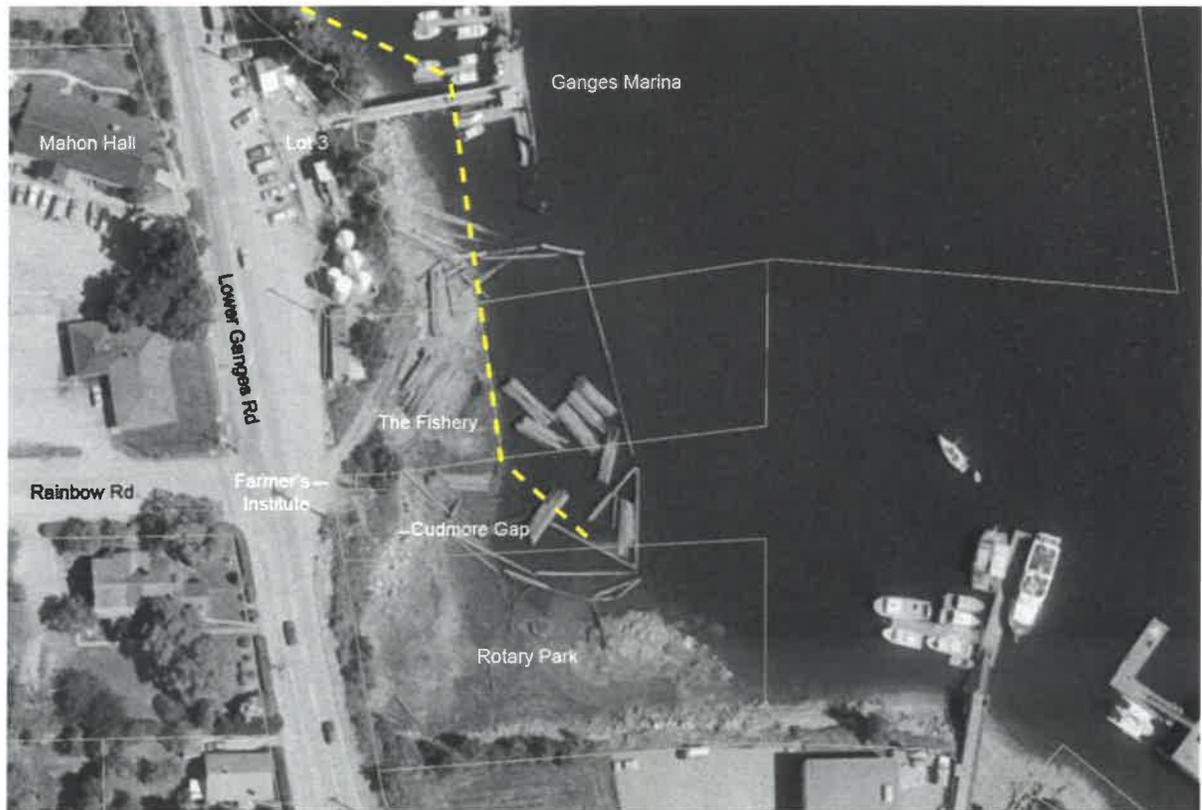
Peck's Cove Park has evidently been highly disturbed by past construction of Lower Ganges Road. In examination of shoreline exposures, the natural soil profile was observed to consist of very shallow light grayish-brown sands (10YR 3/2) overlying yellowish brown (10YR 3/4) glacial till atop of shale bedrock. In a close examination of the ground surface throughout the parkland, particularly along the shoreline edge in the vicinity of the existing boardwalk and viewing platform, we did not identify any physical evidence of any unrecorded archaeological deposits, features or materials at Peck's Cove Park.

### 3.2 Evaluation and Summary of Results

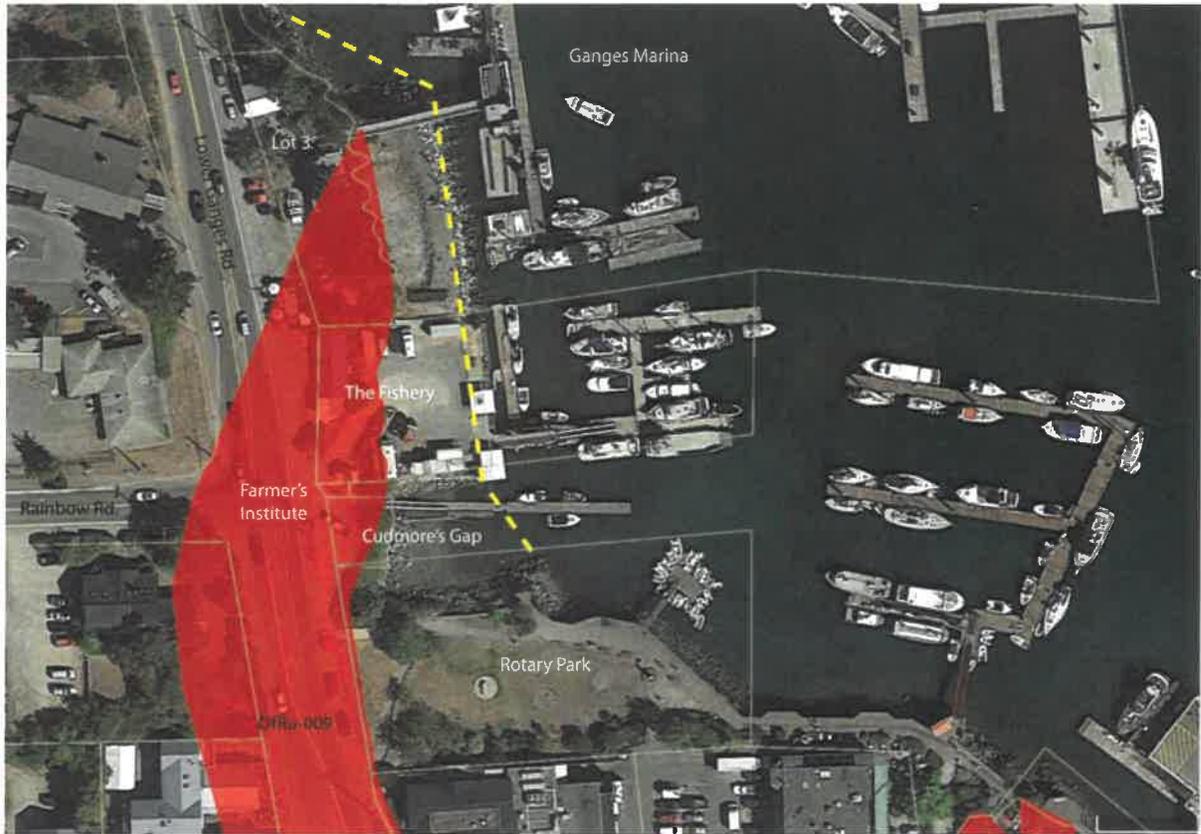
In summary, I consider the results of the Archaeological Overview Assessment (AOA) and Preliminary Field Reconnaissance (PFR) study of the *Ganges Harbour Walk project* sufficient to reasonably identify archaeological site potential in the project area, assess potential impacts by proposed land alterations, and offer practical recommendations for heritage conservation.

Based on my review, I identify that the narrowly-defined *Ganges Harbour Walk project* corridor is located outside of the boundaries of recorded archaeological village sites, DfRu-003 and DfRu-009. In addition, I did not identify any unrecorded archaeological deposits, features or material during our inspection of the foreshore north of Ganges Village from CRD's Rotary Park to Peck's Cove Park.

The majority of the proposed *Ganges Harbour Walk project* follows existing boardwalk infrastructure previously built on the foreshore in the 1980s or crosses over more recently reclaimed waterfront lands (Figure 19). Importantly, the proposed project route is located immediately adjacent to, but outside of, the recorded boundary of the DfRu-009 archaeological site, as defined by the 1974 provincial site form map and Archaeology Branch's RAAD website (Figure 20).



**Figure 19** 1985 aerial photograph of *Ganges Harbour Walk project* area prior to recent large-scale land reclamation efforts between 1993 to 2006 with modern cadastral data from Islands Trust [BC 85013 Frame 205, Scale 1: 15,000]. Proposed boardwalk route is represented by yellow dotted-line.



**Figure 20** Ganges Harbour Walk project area illustrating currently recorded DfRu-009 archaeological site boundary in relation to adjacent upland properties on the north end of Ganges Village. Google Earth Pro 2018.

In the vicinity of the DfRu-009 site, for instance, the proposed boardwalk route beginning at CRD's Rotary Park through to Ganges Marina will traverse off-shore of the historical shoreline and avoid foreshore and upland areas of potential archaeological concern (Figure 19 and 20). Any proposed physical disturbance of the foreshore or upland areas by new boardwalk developments for the *Ganges Harbour Walk* project are anticipated to be minimal.

As illustrated in Figure 20, I confirm that the northern end of the DfRu-009 archaeological site directly overlaps the CRD's Rotary Park and the four private waterfront properties that immediately borders the *Ganges Harbour Walk* project area (Table 1, Figure 19 and 20). As communicated in letter correspondence by the provincial Archaeology Branch to the Salt Spring Island Trust Committee in response to the draft rezoning Bylaw No.491 (SSITC 2016 Appendix 5), any project-related proposals to rezone or otherwise increase the development potential of these upland properties located within the DfRu-009 site boundary require regulation under provincial heritage permits in respect of the *Heritage Conservation Act*, R.S.B.C 1996, Chapter 187.

#### 4.0 HERITAGE CONSERVATION RECOMMENDATIONS

I provide the following recommendations to the CRD to address heritage conservation measures for the *Ganges Harbour Walk Project* in respect of the provincial *Heritage Conservation Act, R.S.B.C. 1996, Chapter 187*:

##### 4.1 No Additional Archaeological Inspection or Monitoring

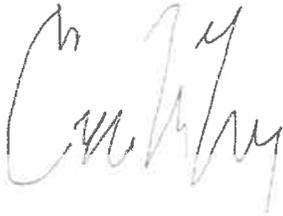
- In my professional opinion, based on the presently-available information, I recommend that no further preliminary archaeological impact assessment studies or archaeological monitoring are required for the CRD to proceed with the present narrowly-defined scope of the *Ganges Harbour Walk* project corridor from the CRD's Rotary Park to Peck's Cove Park.
- If any potential unrecorded archaeological deposits, features or materials are unexpectedly discovered during construction of the *Ganges Harbour Walk*, I recommend that the CRD staff and contractors immediately halt any ground disturbing operations and a qualified archaeologist inspect the location to provide further recommendations for heritage conservation in consultation with the Archaeology Branch and local Coast Salish First Nations.

##### 4.2 Archaeological Impact Assessment Study (s.14 permit) - Peck's Cove Park

- As a precautionary measure, if the CRD wishes greater certainty concerning the identification of potential unrecorded archaeological heritage sites on the upland portion of Peck's Cove Park, I recommend that the CRD may wish to consider completing a more comprehensive archaeological impact assessment (AIA) study specific to proposed boardwalk and related pathway developments in the park. A qualified archaeologist hired by the CRD would apply for a s.14 provincial heritage inspection permit from the Archaeology Branch (MFLNRORD) to direct the AIA study, which would allow subsurface testing to more accurately inspect and assess this upland area for buried or low-visibility archaeological evidence. A final permit report describing the AIA study results would provide baseline information to identify sites, assess potential impacts, and offer practical recommendations for the CRD how to proceed with proposed development in consultation with the Archaeology Branch and local Coast Salish First Nations.

I trust that this letter report and recommendations are satisfactory.

Respectfully,



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Eric McLay, PhD Candidate, Archaeologist

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## Appendix C

### Marine Ecology Summary Report (Whitehead)



**WHITEHEAD ENVIRONMENTAL  
CONSULTANTS LTD.**

P.O. Box 41, Bowen Island, B.C., Canada V0N 1G0  
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26 July 2018  
Project File: 090-4

Capital Regional District  
Parks and Recreation Service  
108 – 121 McPhillips Ave.  
Salt Spring Island, BC, V8K 2T6

Attention: Dan Ovington, Parks and Recreation Manager  
Via email: [dovington@crd.bc.ca](mailto:dovington@crd.bc.ca) (Tel: 250-537-4448)

Dear Mr. Ovington:

**RE: Ganges Harbour Boardwalk Project - Preliminary Marine Ecological  
Assessment, Salt Spring Island.**

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This letter provides our report on the preliminary marine ecological assessment of the Capital Regional District's (CRD) Ganges Harbour Boardwalk Project on Salt Spring Island. The report presents our understanding of the project and the CRD's related assessment needs, the findings of our field investigations and desk-based research, and preliminary recommendations.

### **1. Project Understanding**

The CRD's Parks and Recreation Service is seeking to complete the Ganges Harbour Boardwalk Project. The project was started in the 1990s by a number of public agencies, together with community volunteers, as part of the much-desired improvement to the Ganges Village pedestrian circulation system. However, the project was not completed; the portions that were built were never used and have fallen into disrepair. Approximately 200 m of elevated boardwalk were built as well as segments at grade on land.

The CRD has recently reactivated the project and initiated a feasibility assessment program that includes a technical review to confirm the current integrity, or suitability, of the now-aged existing installations, and to clarify potential future design or construction issues. One of the feasibility parameters is a marine ecological review and assessment to identify potential marine ecological constraints for the existing boardwalk facilities, and for specific, proposed shoreline development adjacent to possible future alignments. The study area for this marine ecological assessment extends along approximately 250 m from the western section of Ganges Harbour between properties adjacent to the Ganges Marina in the south and "Peck's Cove" in the north (Figure 1). The CRD's most current survey plan of the project is provided in Attachments 1 through 3.

You have retained our firm to complete the preliminary ecological assessment and review. As stated in the Terms of Reference, the Consultant Scope of Work is described below:



**Figure 1. Ganges Harbour Boardwalk Project Area, Salt Spring Island.**

(adapted from map created using <http://mapit.islandstrust.bc.ca>)

“The work scope for this preliminary marine ecology assessment will encompass the following tasks:

1. Preparation of a brief, written evaluation of any potential marine ecological concerns specifically related to the as-constructed boardwalk sections.
2. Preparation of a brief, written summary of the consultant’s site investigation, including references to relevant, specific, or at-risk species within the noted foreshore area.
3. Provide a preliminary overview of potential marine ecological impacts related to existing or currently proposed Ganges Harbour shoreline developments which may integrate the proposed boardwalk.”

Aspects related to potential impacts and environmental management needs during future construction (and associated demolition of existing abandoned boardwalk segments) were confirmed to be outside of the scope of this assessment.

## 2. Assessment Objectives and Methods

The assessment objectives were to:

1. Identify and evaluate any potential marine ecological concerns specifically related to the as-constructed boardwalk sections;
2. Document any relevant, invasive or at-risk species within the foreshore and boardwalk area; and
3. Identify any potential marine ecological impacts related to existing or currently proposed shoreline developments in Ganges Harbour which may integrate the proposed boardwalk.

The assessment methods included:

- compilation of background information including: survey plans obtained from the CRD, aerial photographs and other documentation from online sources;
- an overview of the project area with the Project Manager on behalf of the CRD (Wayne Lee, P.Eng. of Aquion Water Technologies Ltd.) on May 3<sup>rd</sup> 2018 to define the study area;
- reconnaissance-level field observations and photography of biophysical conditions during a variety of tide levels on May 2<sup>nd</sup> to 4<sup>th</sup> and during a zero tide on June 15<sup>th</sup> 2018; identification guides used are listed in Section 5;
- interviews with knowledgeable persons (Wayne Lee, P.Eng. and an unnamed homeless couple);
- reference to provincial at-risk species lists compiled by the Conservation Data Centre and described online at E-Flora BC (<http://ibis.geog.ubc.ca/biodiversity/eflora/>) and E-Fauna BC (<http://ibis.geog.ubc.ca/biodiversity/efauna/>); and
- reference to provincial invasive species lists described online by the Invasive Species Council of BC (<https://bcinvasives.ca/>) and the Salt Spring Island Conservancy (<https://saltspringconservancy.ca/>)

The field investigations on May 2<sup>nd</sup> through 4<sup>th</sup> entailed walking the entire length of the boardwalk project from the Rotary Park in Ganges north to Peck’s Cove during low tides when the tide level was between approx. 1 and 0.3 m above zero water Chart datum. During this work, biophysical conditions along the boardwalk corridor, including plant and animal species in the adjacent intertidal and back-shore habitats, were observed and photographed. (Please note that the species lists are preliminary and not exhaustive; not all species present were documented, and it was not

possible to identify all of the species observed.)

A follow-up inspection was completed on our behalf by Wayne Lee, P.Eng., on June 15<sup>th</sup> 2018 to obtain photographs in the southern portion of the boardwalk corridor during a zero tide. These photographs were subsequently reviewed by the author for additional information and insights.

### 3. Assessment Findings & Discussion

The project area is described below from south to north, divided into segments as shown in the following summary table and Figures 1 through 4. The lists of plants and animals observed in marine and terrestrial habitats are provided in Table 2 and Table 3, respectively.

**Table 1. Summary description of the Ganges Harbour Boardwalk Project study area.**

Segment No.	Length (m)*	Description**	Status
1	25	Rotary Park to Lot A ("Fishery" lot)	No boardwalk in place; abutment in place at south end for future connection from Rotary Marine Park sea walk
2	32	Lot A ("Fishery" lot)	Boardwalk in place, partly covered and in use
3	82	Lot 3	Pilings only
4	50	Lot 2	Boardwalk in place; in disrepair (unsafe); accessible from north end only by wooden stairway from Lower Ganges Road (access is blocked)
5	45	Lot 1 south	No boardwalk; accessible by wooden stairway from Lower Ganges Road
6	65	Lot 1 north	Boardwalk in place; accessible from south and north ends, respectively, by wooden stairway from Lower Ganges Road and from land at south end of Segment 7
7	75	Licence Area A	Boardwalk in place; in disrepair (unsafe); accessible from south and north ends (north end blocked)

\* Lengths are approximate, measured from Islands Trust online mapping (<http://mapit.islandstrust.bc.ca>)

\*\* See Figures 1 - 4

#### 3.1. Segment 1

Segment 1 is approximately 25 m long over open water and is entirely unbuilt (Figure 2; Photos 1-7). The starting point at the south end appears to have already been built as part of the sea walk in Rotary Marine Park. The north end would connect to the existing boardwalk within the so-called "Fishery" lot to the north (see Section 3.2).

The main biophysical features include steep riprap armoured banks at the south and north ends, and a very gently sloping lower intertidal foreshore consisting of silt-covered sand and gravel that are only exposed at the lowest tides (Photo 2). The segment contains a private moorage float that is accessed by gated wooden stairs and a ramp from Lower Ganges Road.

Conspicuous marine biota observed in this area included barnacles, algae (rockweed and green strap) and mussels on the riprap, and green algae and clams on the native substrate (Table 2). Several fish, tentatively identified as Smelt based on their size and shape, were observed engaging

in apparent spawning behavior in approx. 0.6 m of water in this area on May 3<sup>rd</sup> when the tide was at approx. 0.3 m.

Terrestrial vegetation is limited to approximately three small arbutus trees, salal, feral English ivy (invasive), Himalayan blackberry, grasses and other native and introduced plants at the top of bank beside the abutment in Rotary Marine Park (Table 3).

Based on our observations, the key ecological sensitivities identified in Segment 1 include the presence of low intertidal habitat that is submerged during most tides (and is therefore biologically more productive than the high intertidal), and the likely presence of forage-fish spawning habitat in the area crossed by the alignment. Construction of an elevated boardwalk across this gap will require careful planning, design and construction to prevent adverse impacts on the marine ecosystem.

### **3.2. Segment 2**

Segment 2 occupies the approximately 32 m long shoreline of Lot A Plan VIP69532, also known as “the Fishery” lot, which is entirely comprised of compacted fill with a riprap armoured bank to the water. The boardwalk is already in place, apparently in good condition, and currently in use (Figure 2; Photos 7 – 10). The existing boardwalk is covered in two places by tent-type structures used for storage; it is also connected to the gated access ramp to the adjacent marina. The wooden piles supporting the boardwalk are embedded within the riprap bank. Approximately 1 m of the native foreshore substrate, composed of silt-covered sand and gravel, is exposed beyond the toe of the riprap during a zero tide.

Conspicuous marine organisms observed in this area included barnacles, algae (rockweed and green strap) and mussels on the riprap and/or pilings, and green algae (sea lettuce and green strap) and clams on the native substrate (Table 2). A loose school of small unidentified fish was observed on May 3<sup>rd</sup> at the north end near the boundary with Segment 3, apparently feeding near the surface approx. 10 m from shore. There is no terrestrial vegetation of note in this segment. No at-risk or invasive species were observed.

The environmental sensitivity along Segment 2 is considered low, primarily because the boardwalk is already in place, and also because the adjacent marine environment is already used relatively intensively. Reactivation of the existing boardwalk here for future use is unlikely to affect the marine or terrestrial ecosystem.

### **3.3. Segment 3**

Segment 3 follows the waterfront of Lot 3 Plan VIP59755, is approximately 82 m long, and comprised mostly of compacted fill (Figure 2; Photos 11 – 17). Although there are no buildings on the fill, the area is used for storage of floats and used lumber. The north end of the fill area is crossed from west to east by an elevated pier that is in active use to access the Ganges Marina, which is separate from the smaller wharf accessed from Segment 2. The Ganges Marina floats extend southward in front of the fill area; however, most of the marina is located to the north of the access pier and extends eastward into Ganges Harbour by over 130 m from the edge of the fill.

A set of older pilings with no superstructure indicates the apparent intended route of the original boardwalk. The pilings along the southern 55 m of this segment are embedded in the riprap bank armouring the fill, in the same manner as in Segment 2, whereas the pilings along the remaining ~27 m to the north cross an area of open water where there is no fill and are presumed to be driven into the native seabed. The northernmost portion of the latter piles were observed to be above the zero tide level on June 15<sup>th</sup> 2018, whereas the southern portion were in the subtidal zone.

Marine biota along the riprap bank on the intertidal parts of the piles and on the seabed is similar to that observed in Segments 1 and 2 (Table 2). A school of small unidentified fish was observed in shallow water on May 3<sup>rd</sup> near the boundary with Segment 2, as described above.

Terrestrial vegetation is limited to sparse grasses and forbs as well as scattered seedlings of Douglas-fir and arbutus on top of the fill on the landward side of the boardwalk alignment (Table 3); the growth of tree seedlings is notably denser at the north end near the overhead pier. Vegetation is also growing on the top of some of the older piles along the boardwalk alignment, indicating the relatively advanced state of decay of these piles. No at-risk or invasive species were observed.

The ecological sensitivity in Segment 3 is limited to the north end where there is no fill and the alignment crosses open water including intertidal and shallow subtidal habitat. Since the substrate appeared similar to that observed in Segment 1, the lower intertidal and upper subtidal zone in this part of Segment 3 may also provide forage-fish spawning habitat.

To prevent adverse impacts on the marine ecosystem, construction of an elevated boardwalk across this gap will likely require careful replacement of the existing unused pilings, as well as planning to allow for development on the landward side of the fill, and careful design and construction of the new boardwalk. There is also the potential alternative of locating the southern part of the new boardwalk entirely on the fill; however, an elevated section would still be necessary in the northern part of this segment to connect with Segment 4.

### **3.4. Segment 4**

Segment 4 is located in Lot 2 Plan VIP59755 and is ~65 m long, approx. 50 m of which consists of an existing boardwalk that follows the foot of a cliff along the natural shoreline (Figure 3; Photos 17 – 24). The boardwalk, which is in disrepair and not in use (aside from apparent storage of miscellaneous materials near the south end, presumably accessed by boat during higher high tides), is built on tall wooden pilings either driven into the native substrate or affixed to cast-in-place concrete footings along the foot of the cliff, most of which are below the high tide level. The pilings are typically over 4 m long, reflecting the steeply sloping foreshore. The boardwalk is currently only accessible from land at the north end; however, the entrance is blocked due to safety concerns.

The biota along Segment 4 is abundant and diverse, reflecting the natural (undisturbed) condition of the foreshore and adjacent back-shore (i.e., not fill). Intertidal organisms observed in this segment included, on the bedrock, boulders and pilings: algae (rockweed, sea lettuce and others), barnacles, snails, Pacific oysters, ochre sea stars, sponges, and others (including unidentified invertebrate eggs on a concrete footing); and on the adjacent soft substrate: green, brown and red algae, crabs, clams, and others.

The adjacent terrestrial ecosystem supported a dense growth of native and introduced plant species (Table 3). Notable among these was the presence of significant amounts of gorse, a noxious invasive species, which was common through much of the overall study area, as well as Himalayan blackberry and English Ivy. No bird nests or roosting bats were observed on the underside of the boardwalk.

Environmental sensitivities in Segment 4 are relatively high in the intertidal habitat along the entire length of the existing boardwalk, mainly due to the abundance and diversity of organisms growing on the rocky substrate.

Reactivation of the existing boardwalk will likely entail renovation of the existing superstructure (decking and railing) and may also require replacement of one or more pilings. To prevent adverse impacts on the marine ecosystem, any demolition and reconstruction, careful planning, design and construction will be necessary.

### **3.5. Segment 5**

Segment 5 is approximately 45 m long, contains no boardwalk at present, and straddles the north and south ends of Lot 2 Plan VIP59755 and Lot 1 Plan 59755 (Figure 3; Photos 23-27). This segment lies on compacted fill entirely above the high tide level and has a riprap-armoured bank to the foreshore. The site is currently accessible by a robust, three-flight wooden stairway from Lower Ganges Road. At present, this stairway is used mainly to access the Salt Spring Adventure Co. Ltd.'s building that is situated within ~1 m of the top of bank at the north end of the segment. There is also a set of concrete stairs to the foreshore immediately north of the building. Any future boardwalk of comparable width to the existing sections would have to be located on the east side of the building, either elevated on piles over the riprap bank in a similar manner as in Segment 2 or by extending seaward the area of level fill.

Segment 5 in its present state is entirely terrestrial and does not impinge on the adjacent marine environment. The foreshore biota (Table 2) is similar to that found in Segment 4; however, the intertidal zone is much wider from this area northward due to the gentler slope to the zero tide level. The upland environment is largely influenced by the overhead canopy of mature trees including large bigleaf maples whose shade limits the growth of ground vegetation growth on the flat fill. At-risk or invasive species were not observed.

Environmental sensitivities in Segment 5 are considered low because of its location on fill above the foreshore. However, in the event that the boardwalk in the northern part of this segment were to be widened beside the existing building in the future, the presence of upper intertidal habitat on the riprap bank would have to be considered to avoid significant impacts. While risk of adverse environmental impacts due to reactivation of Segment 5 is low, careful planning and construction will be necessary to ensure protection of the adjacent marine ecosystem.

### **3.6. Segment 6**

Segment 6 is approx. 65 m long and located entirely in the northern part of Lot 2 Plan VIP59755 (Figure 4; Photos 27-33). There is a boardwalk in place which, although in disrepair, is currently

usable (with care due to missing planks in places) and can be accessed from land at either end. This boardwalk is supported on piles or posts set on cast-in-place concrete footings; the posts are much shorter than those in Segment 4, reflecting the gentle slope of the foreshore in this location.

The boardwalk in Segment 6 crosses the uppermost intertidal zone and is situated approx. 2 – 4 m from the foot of a steep escarpment along the back-shore. The substrate is comprised of gravel and areas of bedrock. Marine organisms (Table 2) are limited because the area is only wetted during the highest tides; barnacles, rockweed, and winkle snails were observed, all of which probably benefit from the shade of the boardwalk that is seldom higher than ~1.5 m overhead. Sand-lance spawning habitat has been mapped by the Islands Trust approximately 7.5 m on the seaward side of Segment 6 at the north end (Figure 5).

Terrestrial vegetation on the adjacent escarpment is dense and diverse, although tree cover tends to be sparse (Table 3). The most common species include Himalayan blackberry and gorse in the areas of soil, and alumroot, stonecrop and mosses on the areas dominated by bedrock. No bird nests were observed on the underside of the boardwalk.

Marine ecological sensitivities in Segment 6 are considered moderate because of its location over the uppermost intertidal zone, which although being the least biologically productive zone of the foreshore, is directly connected to the sea and in proximity to forage-fish spawning habitat. Reactivation of the existing boardwalk will entail some degree of renovation of the existing superstructure (decking and railing) and may also require replacement of one or more pilings. While risk of adverse environmental impacts due to reactivation is considered low, careful planning of any demolition and reconstruction will be necessary to ensure adequate protection of the intertidal habitats.

Any repairs to the existing boardwalk in Segment 6 are considered unlikely to affect the adjacent terrestrial ecosystem. However, it will be necessary to control the dense infestation along this segment by gorse, classified as noxious invasive species, which is already encroaching onto the boardwalk in places. Reactivation of the boardwalk may improve access and facilitate gorse control efforts.

### **3.7. Segment 7**

Segment 7 is approx. 75 m long, contains a derelict boardwalk, and is located entirely on compacted fill within Licence Area A (Figure 4; Photos 33-39). The seaward edge of the fill appears to have been loosely armoured with riprap. There is also a concrete stairway to the foreshore at the north end. Historically, when in use, the boardwalk could be accessed from land at either end, the north end originating on higher ground beside Lower Ganges Road. Access is possible from the south end, although unsafe due to rotting or missing planks; and access from the upper end is blocked with boards. The boardwalk is supported on posts that appear to have been driven into the ground; the height of the posts increases from less than 0.3 m in the south to over 2 m in the north such that the structure slopes upward by approx. 4-5 m over its entire length. There is also a separate low platform on higher ground immediately north of the upper end beside Lower Ganges Road. This deck-type structure is also in disrepair and access is blocked with boards.

It was also noted that the space under the boardwalk's higher north end is currently used as a shelter by homeless persons; tarps having been placed on the decking above and hanging down the sides to create a dry living space.

Segment 7 is entirely terrestrial and does not impinge on the adjacent marine ecosystem. The adjacent marine environment is comprised of bedrock and gravels, and the biota (Table 2) is similar to that found at the higher intertidal elevations of Segment 6; the foreshore here is the widest in the entire study area. It is noted, however, that and-lance spawning habitat has been mapped by the Islands Trust approximately 50 m from Segment 7 at the north end (Figure 5).

The upland environment is comprised of a seaward fringe of shrubs between the boardwalk and the riprap; the area under the boardwalk which is unvegetated due to shade; and a low escarpment area on the landward side that densely vegetated. The back-shore fringe is dominated by shrubs and small trees including oceanspray, gorse, arbutus, bigleaf maple, as well as Himalayan blackberry, grasses and other species. Vegetation on the escarpment area includes variably dense ground cover of alumroot, stonecrop and other forbs; a variety of shrubs such as Indian plum, salmonberry, Himalayan blackberry; and a relatively open canopy of bigleaf maple, Douglas-fir and arbutus. No bird nests or bats were observed on the underside of the boardwalk.

The marine ecological sensitivity in Segment 7 is considered low because of its situation on land above the high tide level. Reactivation of the existing boardwalk will entail considerable renovation including replacement of the decking and railing and may require replacement of one or more pilings. While risk of adverse environmental impacts due to reactivation is considered low, careful planning of any demolition and reconstruction will be necessary to ensure adequate protection of the intertidal habitats.

Terrestrial sensitivity is also considered low. No at-risk species were observed. The density of invasive species such as gorse and Himalayan blackberry is lower than in Segment 6; as at the latter segment, reactivation of the boardwalk may also facilitate control efforts beside Segment 7.

#### 4. Conclusions and Recommendations

Based on the findings presented above, it is concluded that:

1. The overall sensitivity of the marine ecosystem along the boardwalk alignment is relatively low due to a combination of factors including the high intertidal or terrestrial setting, and intensive human use of the adjacent marine waters, with localized exceptions.
2. No critical terrestrial wildlife habitats or at-risk species were found to occur along the alignment.
3. Segments 1 and possibly 3 (Figure 2) are considered to have high ecological sensitivity due to the presence of known or possible forage-fish spawning habitat, which is considered at-risk, on the soft seabed crossed by the alignment in areas where no boardwalk exists at present.
4. Also, forage-fish (Pacific sand-lance, *Ammodytes hexapterus*) spawning habitat, mapped by the Islands Trust (Figure 5), is present on the foreshore seaward of Segment 6 and north

of Segment 7. The former habitat, in particular, may be at risk during reactivation of the existing boardwalk if adequate mitigation is not in place during construction; while the latter is not considered at risk from this project.

5. Segment 4 (Figure 3) is considered to have higher ecological sensitivity due to the presence of a diverse community of intertidal organisms on the rocky or gravel substrate under the existing boardwalk, which would have to be repaired due to its current derelict condition.
6. The sensitivity of the adjacent terrestrial ecosystem is considered relatively low due to either (a) the developed existing condition in Segments 2, 3, 5, 6 and 7; or (b) the low risk of significant disturbance during any future repair or reconstruction of the existing boardwalk in Segments 3, 4, 5, 6 and 7.
7. There are substantial infestations of gorse and, to a lesser extent, Himalayan blackberry and English ivy along the landward side of the alignment, most notably beside Segments 4, 6 and 7, the control of which is desirable.

It is recommended that planning of the boardwalk project's reactivation and completion include the development of an Environmental Management Plan (EMP) to address the need to:

- a. comply with all local, provincial and federal statutes, bylaws and regulations;
- b. protect nearshore water quality in the marine environment;
- c. protect at-risk forage-fish spawning habitat in Segments 1 and 3 and near Segment 6;
- d. protect mid-low intertidal rock and gravel habitat in Segment 4;
- e. protect high intertidal rock and gravel habitat in Segment 6 during any demolition and reconstruction or new construction; and
- f. control invasive terrestrial species, particularly gorse and Himalayan blackberry.

The EMP should be developed through consultations with the CRD, project engineers and designers, experienced marine contractors (ideally including persons involved in the construction of the existing boardwalk sections), the regulatory agencies having jurisdiction, qualified environmental professionals, and land owners and/or lessees. The EMP should also consider water- and land-based accesses, appropriate disposal of demolition debris, and timing of construction to coincide with periods of lowest risk to marine and terrestrial species.

## 5. References

Druehl, Louis D. 2000. **Pacific Seaweeds – A Guide to Common Seaweeds of the West Coast.** Harbour House Publishing, Madeira Park, B.C. 190p

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Kozloff, Eugene N. 1983. **Seashore Life of the Northern Pacific Coast**. Douglas & McIntyre, Vancouver/Toronto. 370p

Pojar, Jim & Andy McKinnon (eds). 1994. **Plants of Coastal British Columbia including Washington, Oregon and Alaska**. BC Forest Service and Lone Pine Publishing, Vancouver. 526p

## 6. Statement of Limitations

This report was prepared exclusively for the Capital Regional District (the Client) by Whitehead Environmental Consultants Ltd. (WEC) for the Ganges Harbour Boardwalk Project, Salt Spring Island, B.C. The quality of information, estimates and conclusions contained herein are consistent with the level of effort expended and are based on: (i) information available at the time of preparation; (ii) data collected by WEC and/or supplied by outside sources; and (iii) the assumptions, conditions and qualifications set forth in this report. The report is intended for use by the Client and other regulatory agencies for this project only, subject to the terms and conditions of the Client's agreement with WEC concerning this assignment. Any other use or reliance on this report by any third party is made at that party's sole risk.

Thank you for the opportunity to assist you on this interesting project. Please contact us if you have any questions about this assessment.

Yours truly,

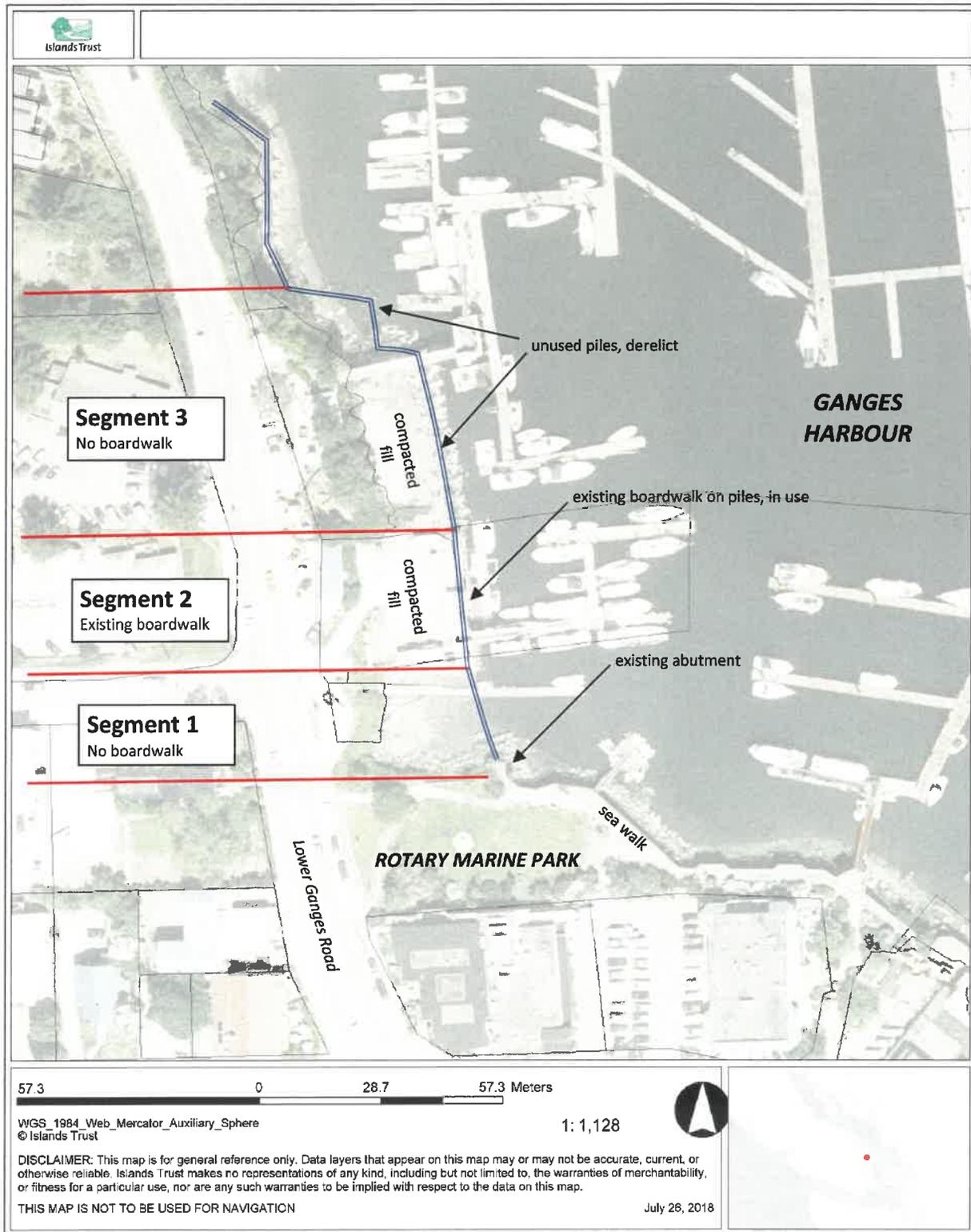
**WHITEHEAD ENVIRONMENTAL CONSULTANTS LTD.**



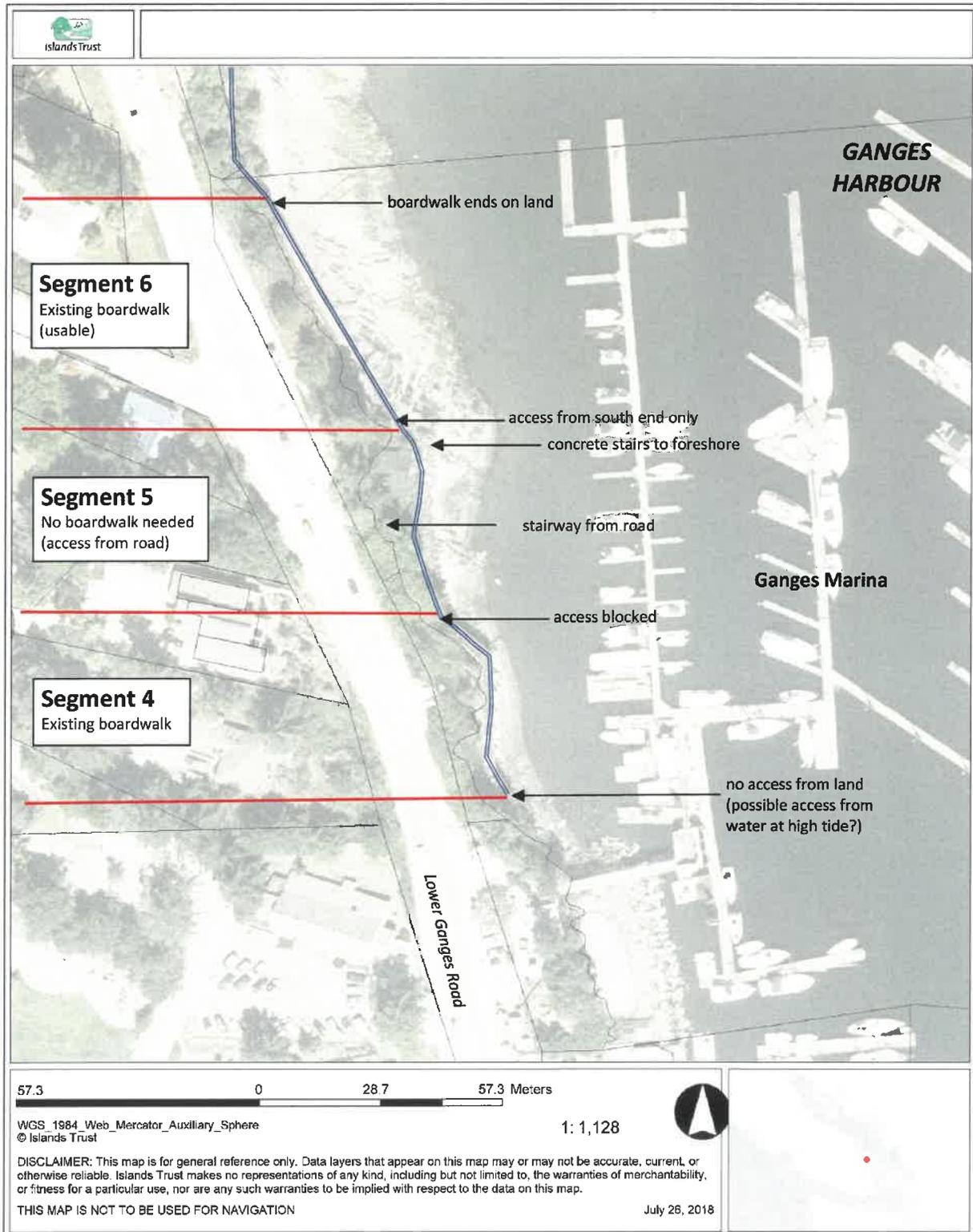
Alan J. Whitehead, M.Sc., R.P.Bio.  
President & Senior Environmental Specialist

Attachment (figures, tables, photos and survey plans)

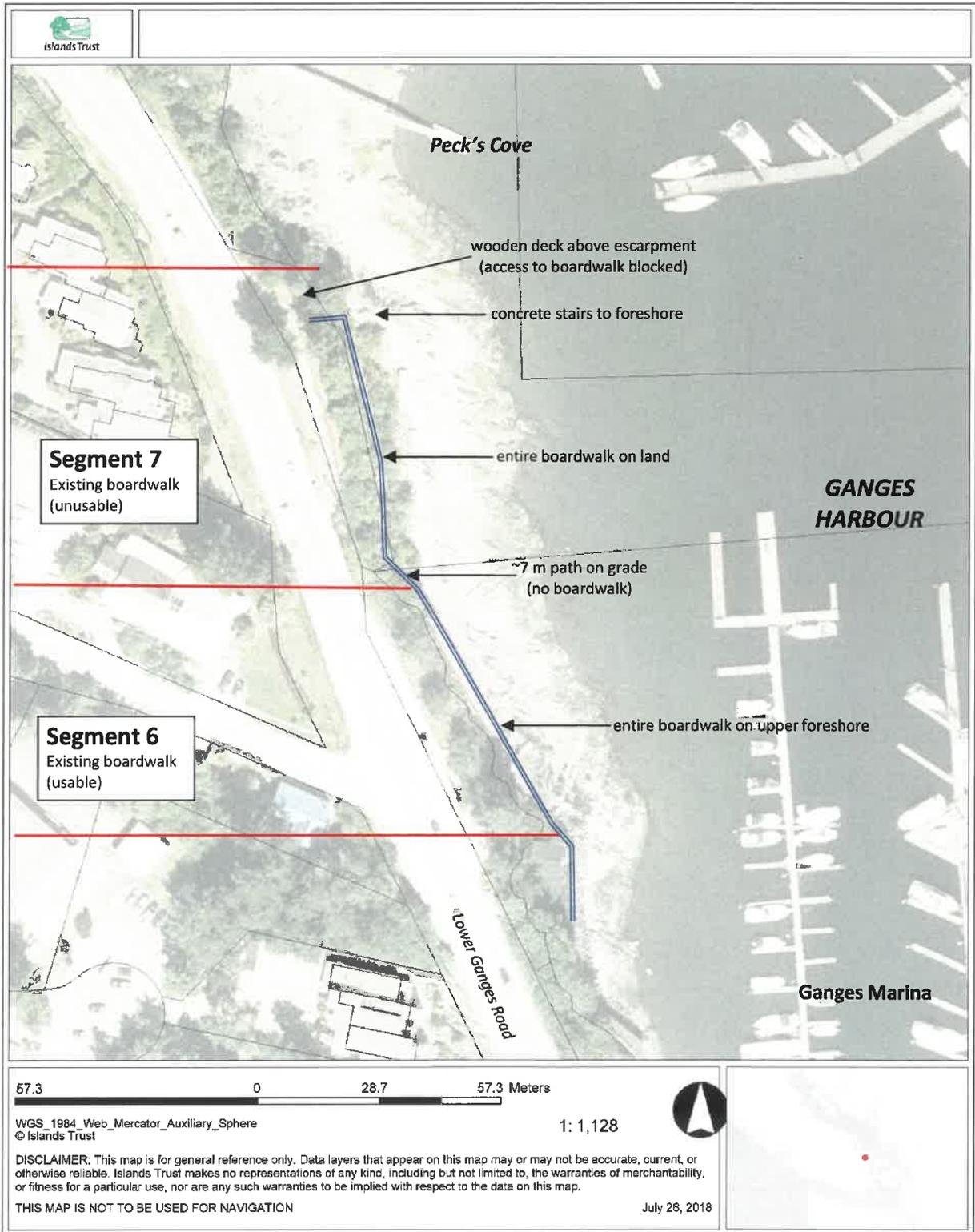
c.c. Wayne Lee, P.Eng., Aquion Water Technologies Ltd. (Project Manager)



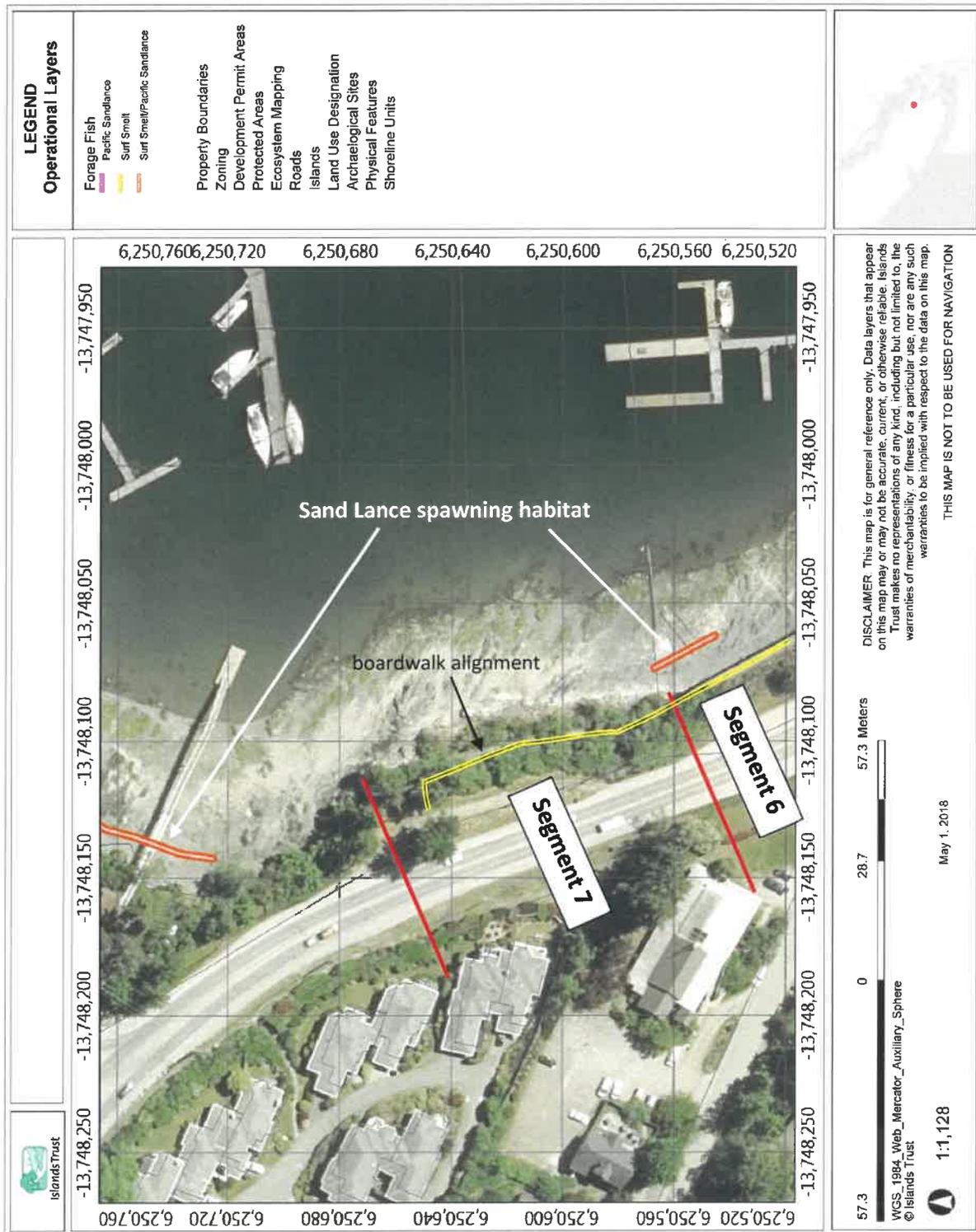
**Figure 2. Segments 1, 2 and 3 of the Ganges Harbour Boardwalk Project, showing lot boundaries.**  
(adapted from map created using <http://mapit.islandstrust.bc.ca>)



**Figure 3. Segments 4, 5 and 6 of the Ganges Harbour Boardwalk Project, showing lot boundaries.**  
 (adapted from map created using <http://mapit.islandstrust.bc.ca>)



**Figure 4. Segments 6 and 7 of the Ganges Harbour Boardwalk Project, showing lot boundaries.**  
(adapted from map created using <http://mapit.islandstrust.bc.ca>)



**Figure 5. Forage-fish spawning locations near the Ganges Harbour Boardwalk Project, as mapped by the Islands Trust.** (adapted from map created using <http://mapit.islandstrust.bc.ca>)

**Table 2. Preliminary List of Marine Biota Observed during a Reconnaissance of the Ganges Harbour Boardwalk Project Study Area, May 2-4 and June 15, 2018.**

Common Name	Scientific Name	Habitat/ Substrate	Observed in Segment No.	At-Risk/ Invasive	Notes
Sea lettuce	<i>Ulva</i> sp.	rock to gravel	1, 2, 3, 4		
Green strap	<i>Enteromorpha</i> sp.	rock to gravel	1, 2, 3, 4		
Rock weed	<i>Fucus</i> sp.	rock	1, 2, 3, 4		
Sea cauliflower	<i>Leathesia</i> sp.	rock	4,5		
Red algae	<i>Ceramium</i> sp.?	rock	4		unconfirmed identification
Sargassum	<i>Sargassum muticum</i>	rock to silty sand	4		
Acorn barnacle	<i>Balanus glandula</i>	rock, pilings	1,2,3,4		
Barnacle	<i>Chthamalus dalli</i> ?	rock, pilings	1,2,3,4		tentative ID
Mussel	<i>Mytilus edulis</i>	rock, pilings	1,2,3,4		
Tube worm	<i>Eudistylia</i> sp.	underside of marina floats	3		
Purple sponge	<i>Haliclona</i> sp.	rock	4		
Mud clam	<i>Mya arenaria</i>	gravel to silty gravel & sand	1,2,3,4 (6,7)		
Littleneck clam	<i>Protothaca staminea</i>	gravel to silty gravel & sand	1,2,3,4 (6,7)		
Clams	Possible additional species, unidentified	gravel to silty gravel & sand	1,2,3,4 (6,7)	unknown	
Pacific oyster	<i>Crassostrea gigas</i>	rock to coarse gravel	4		
Ochre seastar	<i>Pisaster ochraceus</i>	rock	4		
Periwinkle (snail)	<i>Littorina</i> sp.	rock	4,5		
Masked Limpet	<i>Tectura (Lottia) persona</i>	rock	4		
Plate Limpet	<i>Tectura (Lottia) scutum</i>	rock	4		
Dungeness or Red crab	<i>Cancer productus</i> or <i>C. magister</i>	rock to soft substrates	4		
Shire crab	<i>Hemigrapsus nudus</i>		4,7		
Smelt	<i>Hypomesus pretiosus</i>	pelagic, above gravel to silty sand	1	at-risk spawning habitat	unconfirmed identification
Fish 2	unidentified	pelagic, above gravel to silty sand	2,3	unknown (unlikely)	no invasive marine fish known on SSI

**Table 3. Preliminary List of Terrestrial Biota Observed during a Reconnaissance of the Ganges Harbour Boardwalk Project Study Area, May 2-4 and June 15, 2018.**

Common Name	Scientific Name	Habitat	Observed in Segment No.	At-Risk/ Invasive	Footnote No.
<b>Trees</b>					
Douglas-fir	<i>Pseudotsuga menziesii</i>		3,4,5,6,7		1
Western redcedar	<i>Thuja plicata</i>		3,4,5,6,7		
Bigleaf Maple	<i>Acer macrophyllum</i>		3,4,5,6,7		
Red alder	<i>Alnus rubra</i>		3,4,5,6,7		
Arbutus	<i>Arbutus menziesii</i>		1,3,4,5,6,7		2
<b>Shrubs</b>					
Mountain ash	<i>Sorbus sitchensis</i>	soil bank	6,7		
Saskatoon	<i>Amelanchier alnifolia</i>	soil bank	6,7		
Indian plum	<i>Oemleria cerasiformis</i>	soil bank	6,7		
Oceanspray	<i>Holodiscus discolor</i>	soil bank	6,7		
Nootka rose	<i>Rosa nootkana</i>	soil bank	6, 7		
Oregon grape	<i>Mahonia nervosa</i>	soil bank	6		
Snowberry	<i>Symphoricarpos albus</i>	soil bank	6		
Salal	<i>Gaultheria shalon</i>	soil bank	1		
Salmonberry	<i>Rubus spectabilis</i>	soil bank	6,7		
Himalayan blackberry	<i>Rubus discolor</i>	soil bank, disturbed areas	1,3,6,7	Invasive	3
Gorse	<i>Ulex europaeus</i>	soil bank	5,6	Invasive	4
Scotch broom	<i>Cytisus scoparius</i>		6	Invasive	
English ivy	<i>Hedera helix</i>		6	Invasive	
<b>Forbs</b>					
Broad-leaved Stonecrop	<i>Sedum spathulifolium</i>	rock faces	6,7		
Small-flowered Alumroot	<i>Heuchera micracantha</i>	rock faces, steep soil banks	6,7		
Miner's lettuce	<i>Claytonia perfoliata</i>	soil bank	6,7		
Cleavers	<i>Galium aparine</i>		6,7		
Dandelion	<i>Taraxacum officinale</i>		6,7	Invasive	
<b>Grasses</b>					
unidentified	several species		1,3,5,6,7		
<b>Ferns</b>					
Swordfern	<i>Polysticum munitum</i>		5		
Licorice fern	<i>Polypodium glycyrrhiza</i>	under boardwalk	6		
<b>Mosses</b>					
unidentified	several species	rock faces and soil banks	5,6,7		
unidentified	several species	on boardwalk	6,7		

Continued...

Completed...

Common Name	Scientific Name	Habitat	Observed in Segment No.	At-Risk/ Invasive	Footnote No.
<b>Lichens</b>					
Antlered perfume	<i>Evernia prunastri</i>	deciduous trees	6,7		
unidentified	several species	Rock, bark and lumber surfaces	4,6,7		

Notes:

1. Growing on landward side of boardwalk alignment.
2. Also observed growing on the seaward side of Segment 7.
3. Very abundant on the upper bank between Lower Ganges Road and the project area.
4. Very abundant along the bank above Segments 6 and 7. Classified as a noxious weed under the BC Weed Control Act. This is the most significant invasive plant in the project area.

## PHOTOGRAPHS



**Photo 1.** Segment 1 at south end of Ganges Harbour Boardwalk Project area, viewed to east. Abutment from sea walk in Rotary Marine Park is on right (arrow), denoting south end of study area. Note riprap banks on either side. 3-May-2018



**Photo 2.** Existing boardwalk abutment in Rotary marine Park viewed to north across Segment 1 to Fishery lot. The proposed new boardwalk is understood to begin here. 3-May 2018



**Photo 3.** Lower intertidal silty sand-gravel habitat in Segment 1 during a zero tide on 15-Jun-2018, viewed from south side. Dashed line indicates approximate area where forage-fish were observed in ~0.5 m deep water during a 0.3 m low tide on 3-May-2018.



**Photo 4.** Abutment at south terminus of proposed boardwalk alignment viewed to east from Rotary Marine Park during a higher tide. 2-May-2018



**Photo 5.** Private stairway, ramp and float in Segment 1 viewed to east during a zero tide. 15-Jun-2015



**Photo 6.** Foot of riprap bank and soft substrate on north side of Segment 1 viewed from float in Fishery lot during a zero tide. 15-Jun-2018



**Photo 7.** Riprap bank on north side of Segment 1 and south end of Segment 2 viewed toward covered existing boardwalk (arrows) in Fishery lot. 3-May-2018



**Photo 8.** Existing boardwalk and pilings in Segment 2 viewed from pier to marina floats. Note tent-like structure on boardwalk. Tide level was ~0.3m 3-May-2018



**Photo 9.** Uncovered boardwalk I n Segment 2 viewed to south from north property line toward storage tent. Level area on right is compacted fill. 3-May-2018



**Photo 10.** Soft substrate composed of silty sand and gravel at toe of riprap bank under boardwalk in Segment 2, viewed to south during a zero tide. 15-Jun-2018



**Photo 11.** Seaward edge of Lot 3 viewed to south. Unused pilings at top and bottom of bank denote originally intended alignment of boardwalk that was never built. Level area on right is compacted fill.  
3-May-2018



**Photo 12.** Segment 3 viewed to north toward elevated walkway to Ganges Marina. Note vegetation growing in tops of unused pilings. 15-Jun-2018



**Photo 13.** Arbutus seedlings growing out of decaying top of an unused piling in Segment 3. 3-May-2018



**Photo 14.** Old pilings in native seabed at north end of Segment 3 along alignment of boardwalk that was never built, viewed from top of riprap-armoured bank of fill, during a zero tide. Piles on right are in uppermost subtidal zone. 15-Jun-2018



**Photo 15.** North end of Segment 3 viewed toward existing boardwalk at south end of Segment 4 during a higher tide. 3-May-2018



**Photo 16.** Same site as previous photo, viewed during a zero tide. Note proximity and density of Gibsons Marina moorage on right. 15-Jun-2018



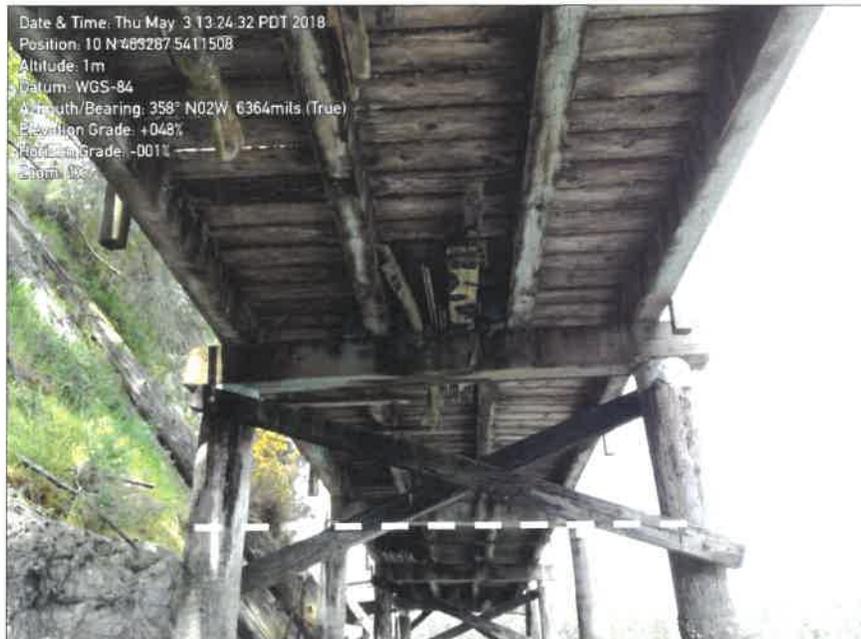
**Photo 17.** Steep native rock bank at north end of Segment 3 viewed north toward existing boardwalk in Segment 4 during a ~0.3 m tide. 3-May-2018



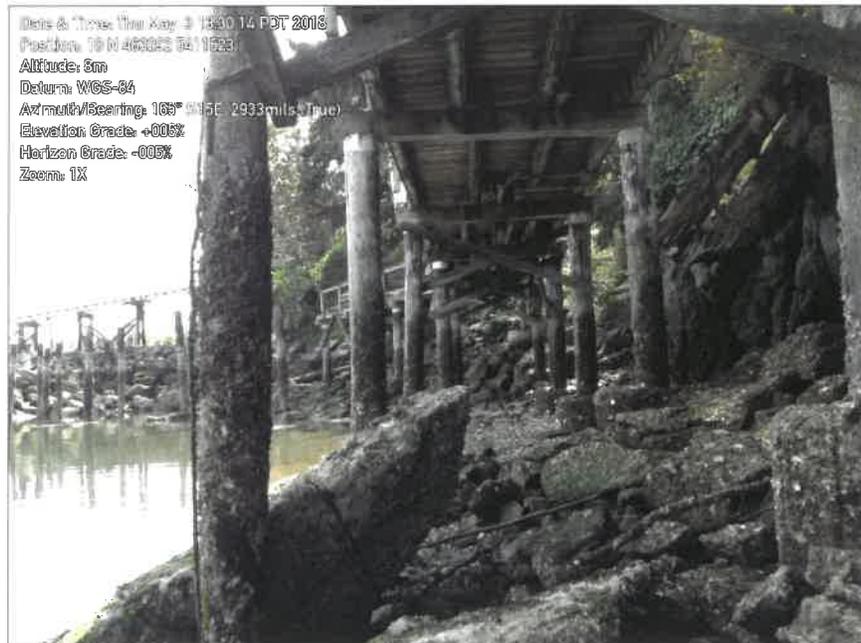
**Photo 18.** Abundant growth of algae and barnacles on native rock bank at south end of Segment 4, viewed to north. Tallest piles are 4 – 5 m high. 3-May-2018



**Photo 19.** Piles supporting existing boardwalk in Segment 4 are either set on concrete footings or (apparently) driven into native substrate. 3-May-2018



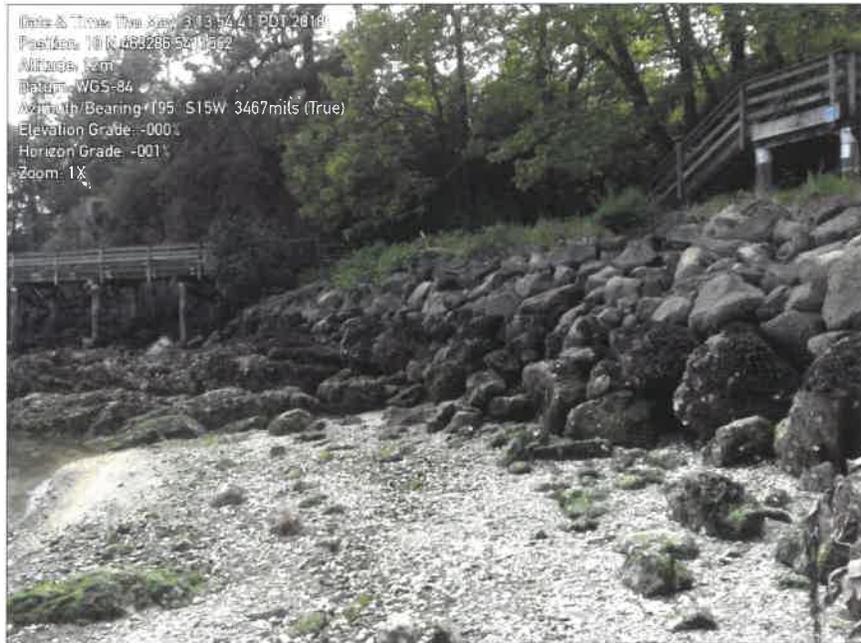
**Photo 20.** Underside of existing boardwalk in Segment 4. Dashed line indicates estimated water level at highest tides, approx. 1.5 m below decking. Note steep escarpment on left. 3-May-2018



**Photo 21.** Tall pilings and boardwalk in Segment 4 viewed to south toward Segment 3. 3-May-2018



**Photo 22.** Shorter pilings on bedrock under north end of Segment 4. Dashed line on cliff face on left indicates estimated water level during highest tides. 3-May-2018



**Photo 23.** Riprap-armoured fill bank above gravel beach beside Segment 5 viewed to south toward north end of boardwalk in Segment 4. Wooden structure on right is stairway from Lower Ganges Road. Note dense tree cover over Segment 5. 3-May-2018



**Photo 24.** Level fill area in Segment 5 viewed from foot of stairway toward north end of existing boardwalk in Segment 4. Note boards blocking entrance to unsafe boardwalk. 2-May-2018



**Photo 25.** Level fill area of Segment 5 viewed to north toward Salt Spring Adventure Co. Ltd.'s building. 2-May-2018



**Photo 26.** Concrete stairway to foreshore at north end of Segment 5. Existing lower boardwalk in Segment 6 is on right. 3-May-2018



**Photo 27.** Existing boardwalk (usable) in Segment 5 viewed from top of concrete stairway to beach in Segment 5. 2-May-2018



**Photo 28.** Boardwalk and short piles on concrete footings in Segment 6 crosses over upper intertidal habitat, viewed to north. Space under boardwalk is typically less than 1.2 m high. Boulders in left foreground are the north end of riprap beside Segment 5. 2-May-2018



**Photo 29.** Escarpment on landward side of existing boardwalk in Segment 6, viewed to north. 2-May-2018



**Photo 30.** Boardwalk in Segment 6 viewed to south from near north end. Note missing planks. Arrow in centre indicates gorse plant (noxious invasive species) encroaching onto boardwalk. 2-May-2018



**Photo 31.** Rockweed and barnacles growing in shaded upper intertidal habitat (bedrock and gravel) under boardwalk in Segment 6, viewed to north. Note cast-in-place concrete footings. 3-May-2018



**Photo 32.** North end of boardwalk in Segment 6 viewed from bedrock foreshore to east. Vegetated area on right is gap before start of Segment 7. Mapped forage-fish spawning habitat is sand and gravel on left approx. 7-8 m from boardwalk. 3-May-2018



**Photo 33.** Sand-gravel beach beside north end of boardwalk in Segment 6, viewed to north. Far right of area covered by jetsam is mapped as sand-lance spawning habitat. Note riprap bank and dense vegetation on right beside south end of Segment 7. 3-May-2018



**Photo 34.** South end of existing derelict (unusable) boardwalk in Segment 7, viewed to north. Note rising slope of boardwalk. 3-May-2018



**Photo 35.** Short piles support low boardwalk in south end of Segment 7. Habitat under and beside boardwalk is terrestrial. Vegetation on left is at foot of escarpment and foreshore is beyond vegetation on right. 3-May-2018



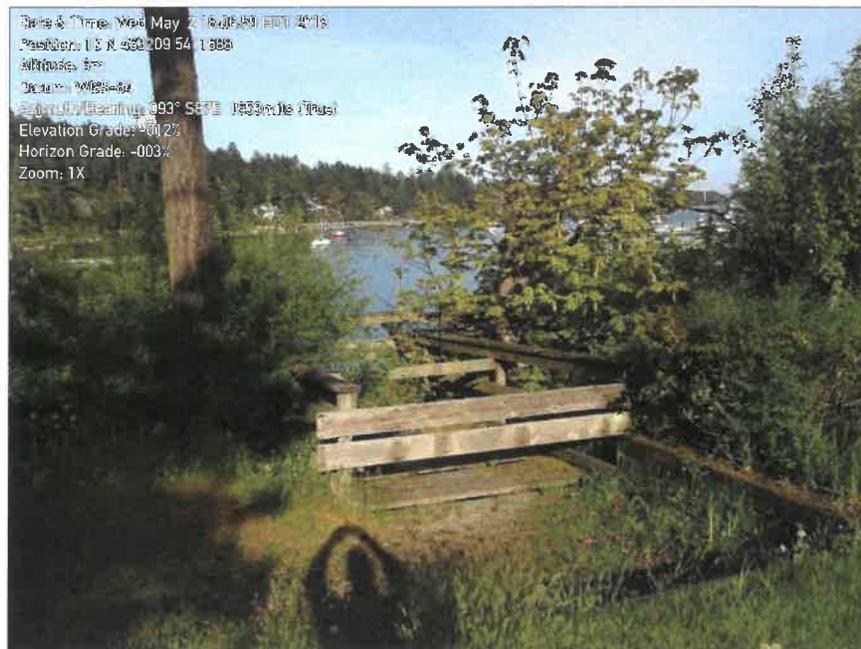
**Photo 36.** Tarps placed on decking at north end of boardwalk in Segment 7 to provide shelter for a homeless couple. Note dense terrestrial vegetation on either side of (otherwise unusable) boardwalk. 3-May-2018.



**Photo 37.** Homeless shelter under north end of boardwalk at higher north end of Segment 7. Note steep bank of escarpment and dense vegetation on right. 2-May-2018.



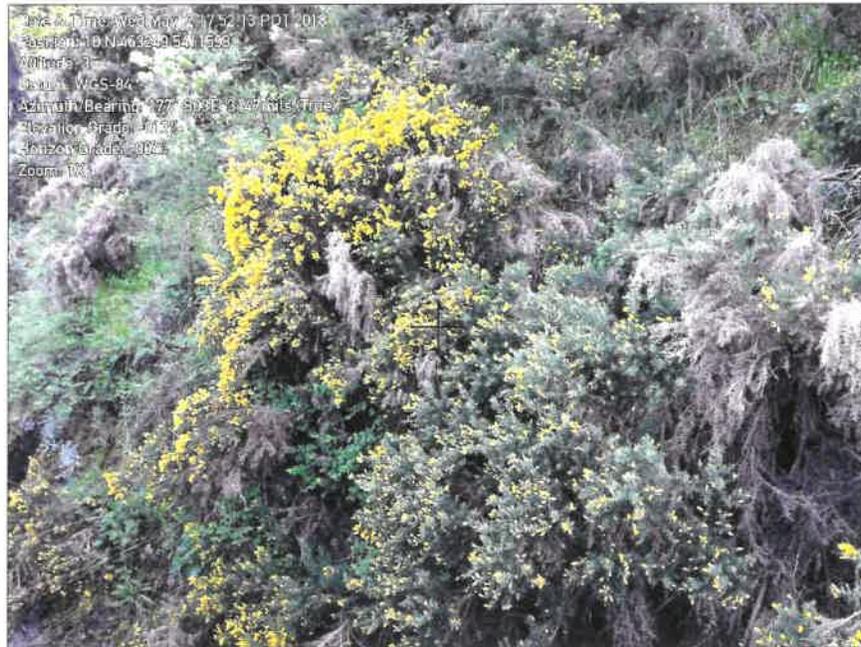
**Photo 38.** Concrete stairs to gravel-bedrock foreshore at north end of Segment 7 where existing boardwalk makes 90 degree bend to west. Mapped forage-fish spawning habitat is a further 50 m to the right. 2-May-2018



**Photo 39.** Blocked entrance to the north end of the existing derelict boardwalk in Segment 7, viewed west toward Ganges Harbour. 2-May-2018



**Photo 40.** Derelict platform beside blocked entrance to north end (arrow) of existing boardwalk in Segment 7 at the north end of the study area, viewed to southeast. This deck, the entrance to which is also blocked with boards, is on flat clearing at top of escarpment and accessible from Lower Ganges Road. 2-May-2018



**Photo 41.** Massive infestation of gorse on the bank above Segment 6. 2-May-2018



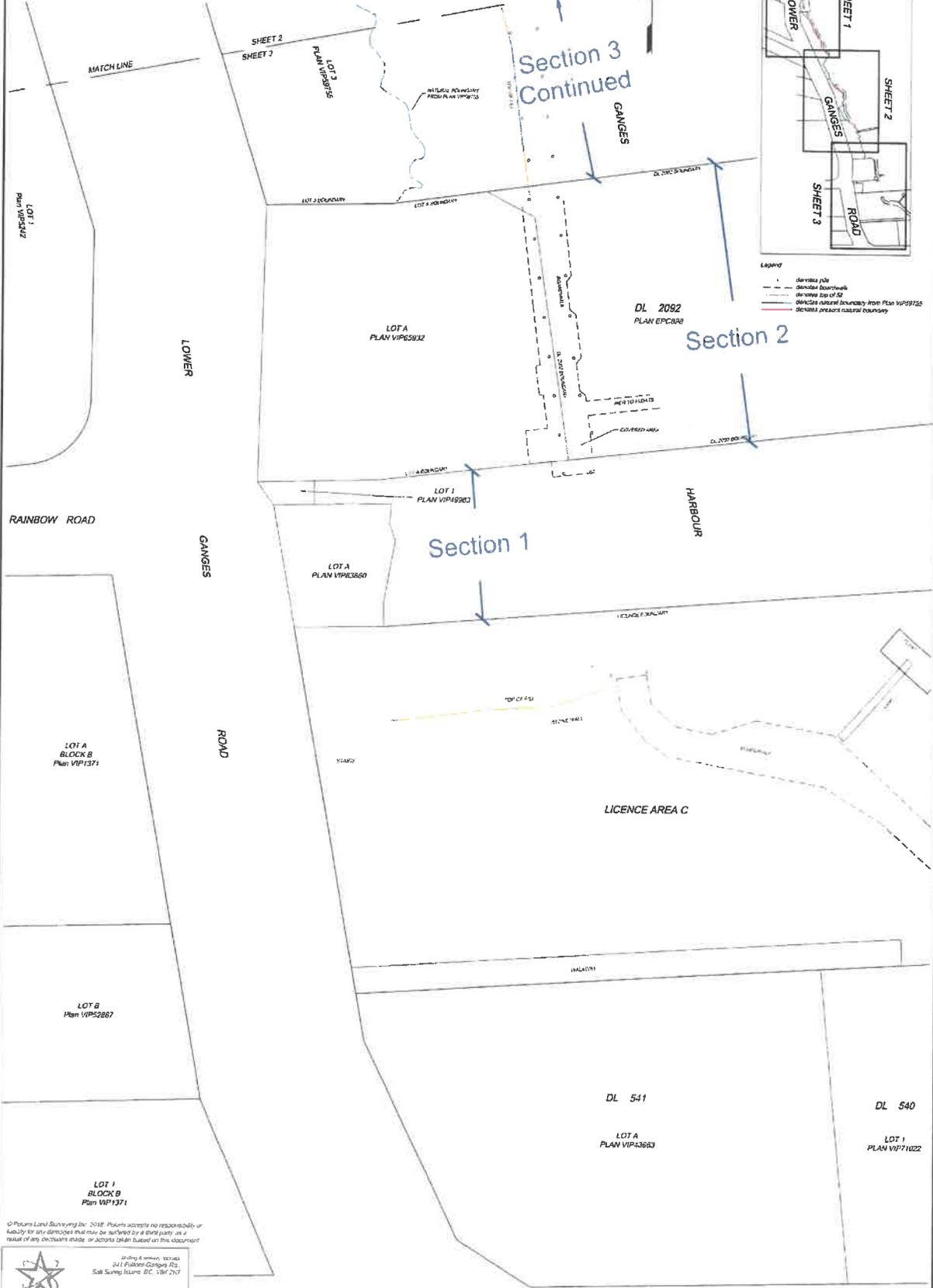
**Photo 42.** Gorse bush encroaching into boardwalk in Segment 6. 2-May-2018

0 2 4 6 8 10  
 All dimensions are in metres and bearings  
 measured clockwise unless otherwise noted.  
 The numbered sheet size of this plan is 559mm  
 in width by 863mm in height (D-Size) when  
 printed at a scale of 1:2500.

Key Plan  
 Scale 1:2500



Legend  
 - derived site  
 - boundary (topography)  
 - drainage top of SD  
 - detected natural boundary from Plan V/P29725  
 - detected proposed natural boundary



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HEREFORD AVENUE

PURVIS LANE

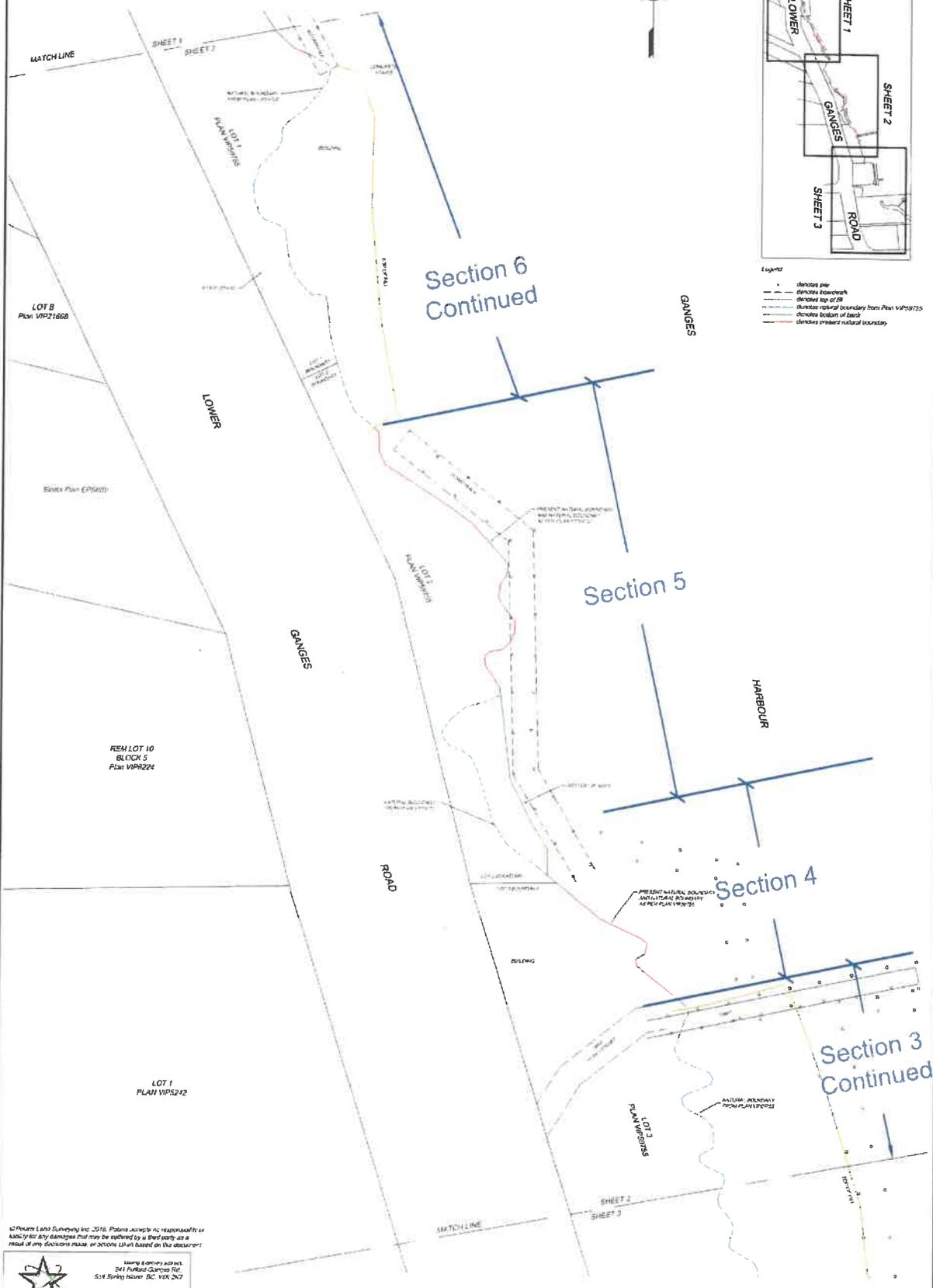
Date: 2018-04-20  
 File: 0478-05  
 Drawing: 0478-05 LOCAL C10  
 Layout: Site Plan



All distances are in metres and decimals thereof unless otherwise noted.  
The intended plot size of this plan is 250mm as shown by factors in range (D-Dim) when viewed at a scale of 1:200.



- Legend
- denotes pier
  - denotes Acropolis
  - denotes top of DB
  - denotes natural boundary from Plan V4P9725
  - denotes bottom of base
  - denotes present natural boundary



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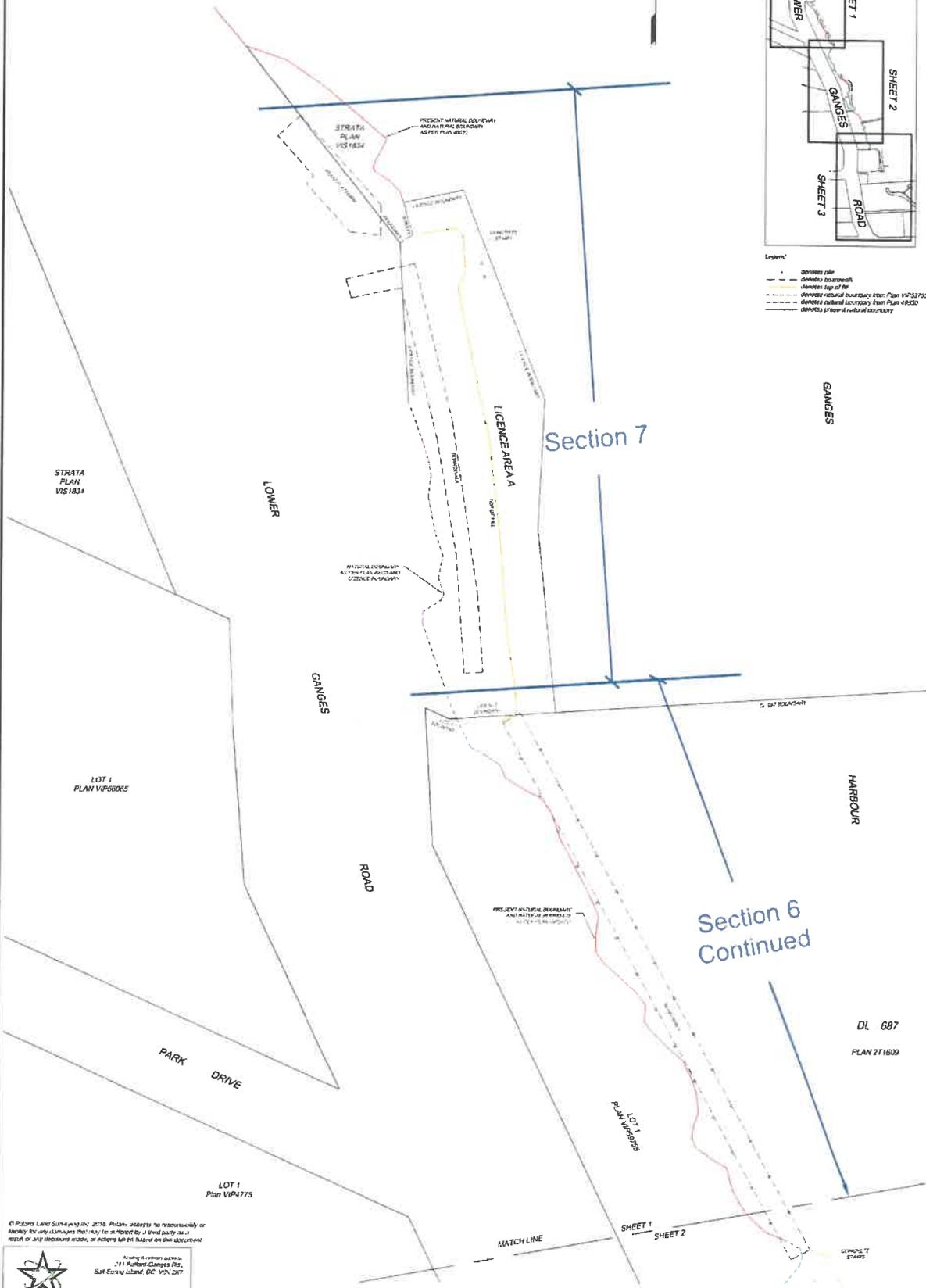
Date: 2019.04.20  
File: 0478-03  
Drawing: 0478-03-LOCAL-C30  
Layer: Site Plan

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 All distances are in metres and dimensions  
 shown, unless otherwise noted.  
 The minimum size of the sheet is 650mm  
 in width by 960mm in height (D-Size) when  
 printed at a scale of 1:200

Key Plan  
 Scale 1:2500  
 0 20 40 60 80 100 120 140 160



Legend  
 - - - - - denotes plan  
 - - - - - denotes boundary  
 - - - - - denotes top of RR  
 - - - - - denotes natural boundary from Plan VPS792  
 - - - - - denotes natural boundary from Plan VPS20  
 - - - - - denotes proposed natural boundary



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DL 687  
 PLAN 211609

Date: 2014-01-20  
 File: 0476-03  
 Drawing: 0476-03-DIGITAL-CSD  
 Layout: Site Plan

## Appendix D

### Geotechnical Constraint Report (Ryzuk)

**RYZUK GEOTECHNICAL**  
Engineering & Materials Testing

28 Crease Avenue, Victoria, BC, V8Z 1S3 Tel: 250-475-3131 Fax: 250-475-3611 www.ryzuk.com

August 27, 2018  
File No: 8-797-50

Capital Regional District  
Parks & Recreation Service  
145 Vesuvius Bay Road  
Salt Spring Island, BC  
V8K 1K3

Attn: Dan Ovington, BBA

Re: Ganges Harbour Boardwalk  
Ganges Harbour – Salt Spring Island, BC

As requested, we attended the referenced site recently to carry out a visual geotechnical review of the existing conditions within the area of the Ganges Harbour Boardwalk. The purpose of this investigation was generally to document the existing conditions, identify the potential geotechnical constraints, and provide preliminary recommendations with respect to the feasibility of the proposed Boardwalk reconstruction. Our associated observations, comments, and recommendations in this regard are contained herein. Our work has been carried out in accordance with, and is subject to, the attached Terms of Engagement.

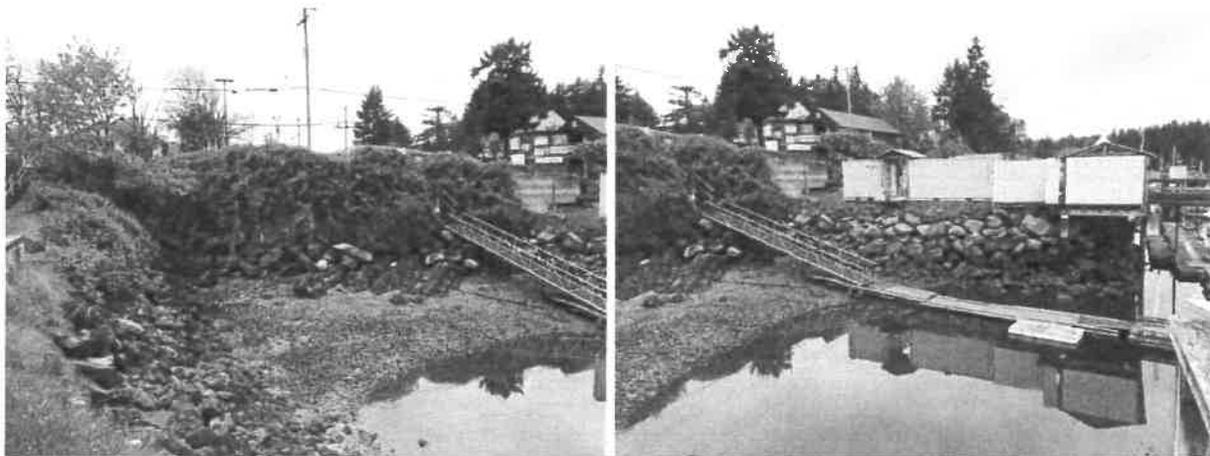
#### EXISTING CONDITIONS

The existing Boardwalk construction generally extends along the shoreline of Ganges Harbour from Rotary Park to Pecks Cove, being in the order of 350 m in length. The structure is discontinuous along the length and includes portions where timber piles and wood decking are in place, in various states of serviceability, as well as portions where only timber piles are present. We understand that the construction was carried out in the late 1990's by local volunteer efforts and that there are no specific records of the pile foundation installation. However, we acquired anecdotal information about the installation from a community member who was involved with the construction.

The ground conditions within the area were observed to consist of variable fill soils mostly comprised of large riprap/boulders, native very dense silty gravelly sand (glacial till) or glaciomarine clays, and shallow sedimentary bedrock. The bedrock was noted to include local thick sandstone beds intercalated with thinly bedded shale and mudstone. Bedding was steeply dipping to the southwest. Joint sets were generally observed to be variable and sub-orthogonal to the bedding. A veneer of topsoils and organics typically exists within the gently to moderately

sloped areas of the terrestrial terrain, while the steep slopes are predominantly bedrock. We expect that bedrock is typically shallow within the marine areas, however, some sand and mud deposits are also likely present overlying the bedrock. Available geological mapping denotes soils and bedrock to be generally consistent with our site observations. We also reviewed air photography circa 1956 from our archives. It is evident from the photographs that the shoreline within the harbour has been subject to significant fill placement, particularly within the southern portions of the Boardwalk alignment. The Boardwalk alignment is shown in plan on the attached survey drawings produced by Polaris Land Surveyors with seven sectors denoted for reference and conformance with the project area designations.

At the time of our assessment the tide was relatively low and therefore we were able to view the majority of the structure in place, as well as the existing shoreline conditions. We began our review at the southern portions (Section 1). No boardwalk installation extends through this area which is generally a small, shallow inlet with a shoreline comprised of bedrock and thin deposits of sand, gravel and cobbles. The slope adjacent to the foreshore appears to comprise large boulders inclined at approximately 45 degrees and covered with vegetation. An existing concrete bin-wall installation extends beyond the northern side of the slope. The photographs below show the conditions within Section 1.



The next sections to the north (Sections 2 and 3) includes a portion (Section 2) of the existing boardwalk which is actively used by ‘The Fishery’. The conditions within this area are typically characterized by a boulder riprap fill slope which was clearly placed around the timber piles subsequent to the installation, as shown in the photographs below taken from the dock pedestrian access ramp. Piles were spaced at approximately 3 to 4 m apart in pairs, with spacing of pairs along the alignment being between 4 and 5 m. We noted several of the piles were filled against with boulders such that there was a significant differential in height, and thus an uneven lateral loading condition exists. The slope of the boulder fill varies between approximately 35 and 45 degrees from horizontal and the material size was estimated to be 1000 kg – 2000 kg class riprap. The rock material appears to consist mostly of greywacke sandstone or granitic rock. A similar

condition extends within Section 3 but with no decking in place up to the Ganges Marina jetty with the overall length of Section 2 and 3 being in the order of 50 m.



The conditions change beyond this area (Section 4 and 5) where the next section of the Boardwalk infrastructure extends out into the bay for a short distance before returning landward to align alongside a variable, steep to very steep, bedrock slope in the order of 7 m high. Within this section, which extends some 60 m, the pile foundations were visible and were observed to consist of a cylindrical concrete footing in the order of 0.9 m diameter by 0.6 m thick. The foundations appear to have been cast directly atop the natural rock talus slope or bedrock of the in situ shoreline. We noted that rock material had been scoured from below the foundations locally. The photographs below show the general condition within this area as viewed looking to the north from below the northern marina jetty, and beneath the Boardwalk from the beach level. The third image shows the proximity of the structure to an overhanging bedrock bluff which includes mature trees located at, or near the crest of the slope. This section of the alignment terminates within a known filled area where a small steel building is situated (Section 6).





The Boardwalk structure continues north beyond the filled area and extends along the beach for another 60 m approximately (Section 7). The construction appears to be similar to the previous section whereby the piles have been set atop the bedrock and a concrete footing cast at the base. The beach conditions and the slope adjacent to the shoreline are shown below. The vegetated slope within the northern areas is generally inclined at between 40 and 60 degrees and is expected to be predominantly bedrock controlled, however, local very dense glacial till soils were noted to be present in pockets.



North of this area, there is a small separation and the alignment then extends within the terrestrial area above the beach. The Present Natural Boundary (PNB) is armored with a riprap slope adjacent to this location and again the pile foundations appear to have been set atop the existing grade with cylindrical concrete footings.

## GEOTECHNICAL CONSTRAINTS

The existing timber marine piles appear to be relatively intact and may be deemed by the structural consultant to be serviceable for reuse. We understand that the structural consultant will examine the competency of the piles (by coring and/or non-destructive methods) to determine whether the timbers have been subject to decay due to fungi/microbes or physical damage from boring organisms like Toredos. From a geotechnical perspective, where the pile base can be visually confirmed to be bearing directly on native soils or bedrock (most portions of Section 5, 6, and 7), the long term support conditions can be relied upon with negligible risk. Where the base support conditions are not directly viewable (Sections 2 through 4), the conditions can only be inferred and therefore a residual risk due to the unknown support conditions would be an accepted condition of the proposed structure usage.

Following our site assessment, we reviewed historical air imagery of the area as well as relevant geological mapping and the site survey produced by Polaris Land Surveyors. We also contacted a member of the local community Mr. Grant Wickland, who was involved with a significant portion of the Boardwalk construction.

We had a telephone discussion with Mr. Wickland who managed a group of volunteers over a number of years during the intermittent construction of the Boardwalk. Mr. Wickland is a skilled craftsman who also had previous experience working for the federal government on marine installations. Based on our discussions, we understand that the pile foundations within the majority of the alignment were constructed directly atop the bedrock. The concrete foundations included a 15M steel cage which was doweled into the bedrock by several centimeters and were typically cast at extreme low tide conditions. Exceptions were noted for the section of piles which were installed in conjunction with the north jetty installation of the marina ramp. This group was apparently driven to refusal through shallow bay deposits by conventional drop hammer on a barge mounted crane. Another exception was identified within the central section where the footings were cast locally (by a separate group of volunteers) atop the rocky beach materials (as shown on page 3 of the Part 1 report). Unfortunately, no specific geotechnical documentation exists for the installation that we are aware of.

The steep slopes adjacent to the boardwalk were reviewed for risk of instability and the associated rockfall/landslide hazard. We consider that the joint and bedding configuration are generally situated favorably with regards to risk of large scale instability throughout the alignment. However, the risk of local rockfall and landslide do exist due to the steep rock slope conditions. Notable areas for rock fall instability include the steep rock slope between the north side of the marina and the temporary building (Section 4 and 5). Instability of a portion of the

slope is likely to occur where a localized potentially unstable block was identified at the southern limit of this area, however, we consider that the mobilized rockfall would not likely impact the piles due to the offset distance and the span of water typically between the beach and piles. There are also several locations along the alignment where the vegetative cover within the upper portions of the slope is dense and the identification of potentially unstable blocks of rock or soil was not possible at the time of our review.

Notwithstanding the potential instability conditions, we consider that localized rock fall or landslip occurrence within these areas would likely not result in catastrophic impact damage causing failure of the structure, but rather the potential for local damage to isolated piles would be more likely in the event of such mobilization. It should also be noted that the presence of large trees along the slope crest can increase the risk of rockfall as the root ingress of joints can separate blocks, and windthrow occurrence could potentially cause impact of wood debris to the Boardwalk decking/railings.

An area of note within Section 5 was reviewed where the Boardwalk alignment extends parallel to an overhanging section of sandstone (shown in the upper photograph of Page 4). We carried out stability calculations to review the risk of rock toppling failure of the sandstone rock slab inclined at approximately 8 degrees over vertical. Our analysis indicated that the slab of rock is not at excessive risk of toppling during static conditions, and we expect that the mass would not likely mobilize during seismic ground motions associated with a design occurrence with a 2% probability of exceedance in 50 years. Indeed, the rock slope in this general configuration has likely experienced several large earthquake events since the glacial recession exposed the location over 10,000 years ago, with the slab having remained in place. However, as there are unknowns in this regard, the risk of such seismically induced mobilization may be considered as a residual risk. Also, the relatively slow erosion of the rock along the toe of the slope due to wave impact at high water levels would serve to reduce the stability conditions in the long term. This residual risk condition is generally accepted for regional trail usage in the District, however, measures could be installed to mitigate this risk if desired, and generally could comprise the installation of some form of buttresses and protection along the toe of the slab.

A thick layer of boulder fill exists within the area adjacent to the Fishery and Marina jetty (Section 2 and 3). The fill is generally configured at a globally stable inclination, however, local rock mobilization may be experienced during a large earthquake. Furthermore, the ground conditions below the fills are unknown and if such were placed atop normally consolidated clays, there may be a residual risk of long term settlement and possible lateral movements may be experienced if the soils below are weak/loose. In this instance the piles may be at risk of deformation / settlement. That said, based on the shallow bedrock observed within the adjacent areas, and presuming the piles within this area were founded atop the rock surface, we consider that this risk would be low.

The foundation bearing of the piles was observed to be adequate within the majority of the viewable areas of Section 5 through 7, having been cast directly atop the bedrock surface. An

exception was noted within a portion to the immediate north of the Marina jetty within Section 5 where a limited number of the pile footings were observed to have been cast atop the preexisting rocky beach materials (shown in the lower right photograph of page 3). These soils may or may not be fills and the coarse angular rock materials appear to have scoured below the footing locally. The long term support of the piles within this location are therefore not optimum and deformation of such may potentially occur if scour were to continue, or if the soils undergo long term consolidation.

The location at the head of Ganges Harbour may experience minor tsunami inundation as a result of a significant event influencing the Gulf Islands Area. The risk of such to cause damage to the structure is however deemed low due to the location and the anticipated inundation wave amplitude being generally less than 3 m.

## GEOTECHNICAL RECOMMENDATIONS

Based on our review and the above detailed anecdotal information, we consider the existing piles to be generally serviceable from a geotechnical perspective provided that the members are deemed to be competent by the structural consultant, and in this case it would be reasonable to rely of such for the proposed usage. A noted exception would be for the piles within Section 2 and 3 where the lateral loading differential due to the surrounding boulder fill generally exceeds 45 degrees (0.3 m vertical difference from one side of the pile to the other). We consider that the rocks leaning against the piles could be removed to negate this lateral loading condition, or it may be an option to cast concrete around these locations to solidify the rock mass and reduce the potential for strain should the pile begin to yield under the lateral stress imparted by the boulders. Additionally, piles with foundations bearing atop the rocky beach where scour has occurred should have the support conditions improved with addition of concrete to infill the scoured areas. Notwithstanding the preceding, we expect that should the support conditions of isolated piles be marginal and minor settlement occur in the long term, the timber structure would likely be capable of accommodating minor differential settlement if acceptable to the structural consultant.

If additional piles are proposed for the Boardwalk reconstruction we recommend that such be constructed either directly atop the bedrock or driven to refusal by drop hammer under direct geotechnical supervision. For the structural design considerations, the piles can be considered to be end bearing and the bedrock can be assumed to provide bearing resistance of 1MPa for the 0.3 m diameter pile tip or the 0.9 m diameter spread footing area.

## CLOSURE

In summary, from a geotechnical perspective we believe that the existing piles would be generally serviceable provided that the remedial works noted above are implemented. We consider that there would not be a specific requirement for further investigation work at this time unless new piles are proposed, which would likely be within the marine areas. In this case, we

would suggest that the new locations could be probed from a barge to review the mud thickness and the depth to bedrock. We would be pleased to assist with the project moving forward.

We consider that the existing Boardwalk is generally located within a relatively safe area with respect to potential geological hazards. There are some unknowns with respect to localized conditions, however, we believe that any foreseeable mobilization occurring within the slopes above or below the boardwalk are expected not to result in catastrophic failure of the structure, though local repairs may be required.

We trust that the preceding is suitable for your purposes at present. Please don't hesitate to contact our office if we can be of further assistance.

Yours very truly,  
Ryzuk Geotechnical

Andrew Jackson, P.Geo. Eng.L.  
Project Geoscientist



The image shows a handwritten signature in black ink, which is somewhat scribbled. Below the signature are two circular professional stamps. The stamp on the left is from the Association of Professional Engineers and Geoscientists of the Province of British Columbia, identifying A. R. JACKSON with license number # 38270. The stamp on the right is from the Engineering Council of British Columbia, identifying A. R. JACKSON as an ENGINEERING LICENSEE with a Limited Licence # 38270. A handwritten date '2018/07/27' is written across the bottom of the stamps.

Attachment – Terms of Engagement and Site Plan

cc: Wayne Lee, P.Eng. (aqionwater@gmail.com)

## TERMS OF ENGAGEMENT

### GENERAL

Ryzuk Geotechnical (the Consultant) shall render the Services, as specified in the agreed Scope of Services, to the Client for this Project in accordance with the following terms of engagement. The Services, and any other associated documents, records or data, shall be carried out and/or prepared in accordance with generally accepted engineering practices in the location where the Services were performed. No other warranty, expressed or implied is made. The Consultant may, at its discretion and at any stage, engage sub-consultants to perform all or any part of the Services.

Ryzuk Geotechnical is a wholly owned subsidiary of C. N. Ryzuk & Associates Ltd.

### COMPENSATION

All charges will be payable in Canadian Dollars. Invoices will be due and payable by the Client on receipt of the invoice without hold back. Interest on overdue accounts is 24% per annum.

### REPRESENTATIVES

Each party shall designate a representative who is authorized to act on behalf of that party and receive notices under this Agreement.

### TERMINATION

Either party may terminate this engagement without cause upon thirty (30) days' notice in writing. On termination by either party under this paragraph, the Client shall forthwith pay to the Consultant its Charges for the Services performed, including all expenses and other charges incurred by the Consultant for this Project.

If either party breaches this engagement, the non-defaulting party may terminate this engagement after giving seven (7) days' notice to remedy the breach. On termination by the Consultant under this paragraph, the Client shall forthwith pay to the Consultant its Charges for the Services performed to the date of termination, including all fees and charges for this Project.

### ENVIRONMENTAL

The Consultant's field investigation, laboratory testing and engineering recommendations will not address or evaluate pollution of soil or pollution of groundwater. The Consultant will cooperate with the Client's environmental consultant during the field work phase of the investigation.

### PROFESSIONAL RESPONSIBILITY

In performing the Services, the Consultant will provide and exercise the standard of care, skill and diligence required by customarily accepted professional practices and procedures normally provided in the performance of the Services contemplated in this engagement at the time when and the location in which the Services were performed.

### INSURANCE

Ryzuk Geotechnical is covered by Professional Indemnity Insurance as follows:

1. \$ 2,000,000 each and every claim
2. \$ 4,000,000 aggregate
3. \$ 5,000,000 commercial/general liability coverage

### LIMITATION OF LIABILITY

The Consultant shall not be responsible for:

1. the failure of a contractor, retained by the Client, to perform the work required for the Project in accordance with the applicable contract documents;
2. the design of or defects in equipment supplied or provided by the Client for incorporation into the Project;
3. any cross-contamination resulting from subsurface investigations;
4. any Project decisions made by the Client if the decisions were made without the advice of the Consultant or contrary to or inconsistent with the Consultant's advice;
5. any consequential loss, injury or damages suffered by the Client, including but not limited to loss of use, earnings and business interruption;
6. the unauthorized distribution of any confidential document or report prepared by or on behalf of the consultant for the exclusive use of the Client
7. Subsurface structures and utilities

The Consultant will make all reasonable efforts prior to and during subsurface site investigations to minimize the risk of damaging any subsurface utilities/mains. If, in the unlikely event that damage is incurred where utilities were unmarked and/or undetected, the Consultant will not be held responsible for damages to the site or surrounding areas, utilities/mains or drilling equipment or the cost of any repairs.

The total amount of all claims the Client may have against the Consultant or any present or former partner, executive officer, director, stockholder or employee thereof under this engagement, including but not limited to claims for negligence, negligent misrepresentation and breach of contract, shall be strictly limited to the amount of any professional liability insurance the Consultant may have available for such claims.

No claim may be brought against the Consultant in contract or tort more than two (2) years after the date of discovery of such defect.

## DOCUMENTS AND REPORTING

All of the documents prepared by the Consultant or on behalf of the Consultant in connection with the Project are instruments of service for the execution of the Project. The Consultant retains the property and copyright in these documents, whether the Project is executed or not. These documents may not be used on any other project without the prior written agreement of the Consultant.

The documents have been prepared specifically for the Project, and are applicable only in the case where there has been no physical alteration to, or deviation from any of the information provided to the Consultant by the Client or agents of the Client. The Client may, in light of such alterations or deviations, request that the Consultant review and revise these documents.

The identification and classification as to the extent, properties or type of soils or other materials at the Project site has been based upon investigation and interpretation consistent with the accepted standard of care in the engineering consulting practice in the location where the Services were performed. Due to the nature of geotechnical engineering, there is an inherent risk that some conditions will not be detected at the Project site, and that actual subsurface conditions may vary considerably from investigation points. The Client must be aware of, and accept this risk, as must any other party making use of any documents prepared by the Consultant regarding the Project.

Any conclusions and recommendations provided within any document prepared by the Consultant for the Client has been based on the investigative information undertaken by the Consultant, and any additional information provided to the Consultant by the Client or agents of the Client. The Consultant accepts no responsibility for any associated deficiency or inaccuracy as the result of a miss-statement or receipt of fraudulent information.

## JOBSITE SAFETY AND CONTROL

The Client acknowledges that control of the jobsite lies solely with the Client, his agents or contractors. The presence of the Consultant's personnel on the site does not relieve the Client, his agents or contractors from their responsibilities for site safety. Accordingly, the Client must endeavor to inform the Consultant of all hazardous or otherwise dangerous conditions at the Project site of which the Client is aware.

The client must acknowledge that during the course of a geotechnical investigation, it is possible that a previously unknown hazard may be discovered. In this event, the Client recognizes that such a hazard may result in the necessity to undertake procedures which ensure the safety and protection of personnel and/or the environment. The Client shall be responsible for payment of any additional expenses incurred as a result of such discoveries, and recognizes that under certain circumstances, discovery of hazardous conditions or elements requires that regulatory agencies must be informed. The Client shall not bring about any action or dispute against the Consultant as a result of such notification.

## FIELD SERVICES

Where applicable, field services recommended for the Project are the minimum necessary, in the sole discretion of the Consultant, to observe whether the work or a contractor retained by the Client is being carried out in general conformity with the intent of the Services. Any reduction from the level of services recommended will result in the Consultant providing qualified certifications for the work.

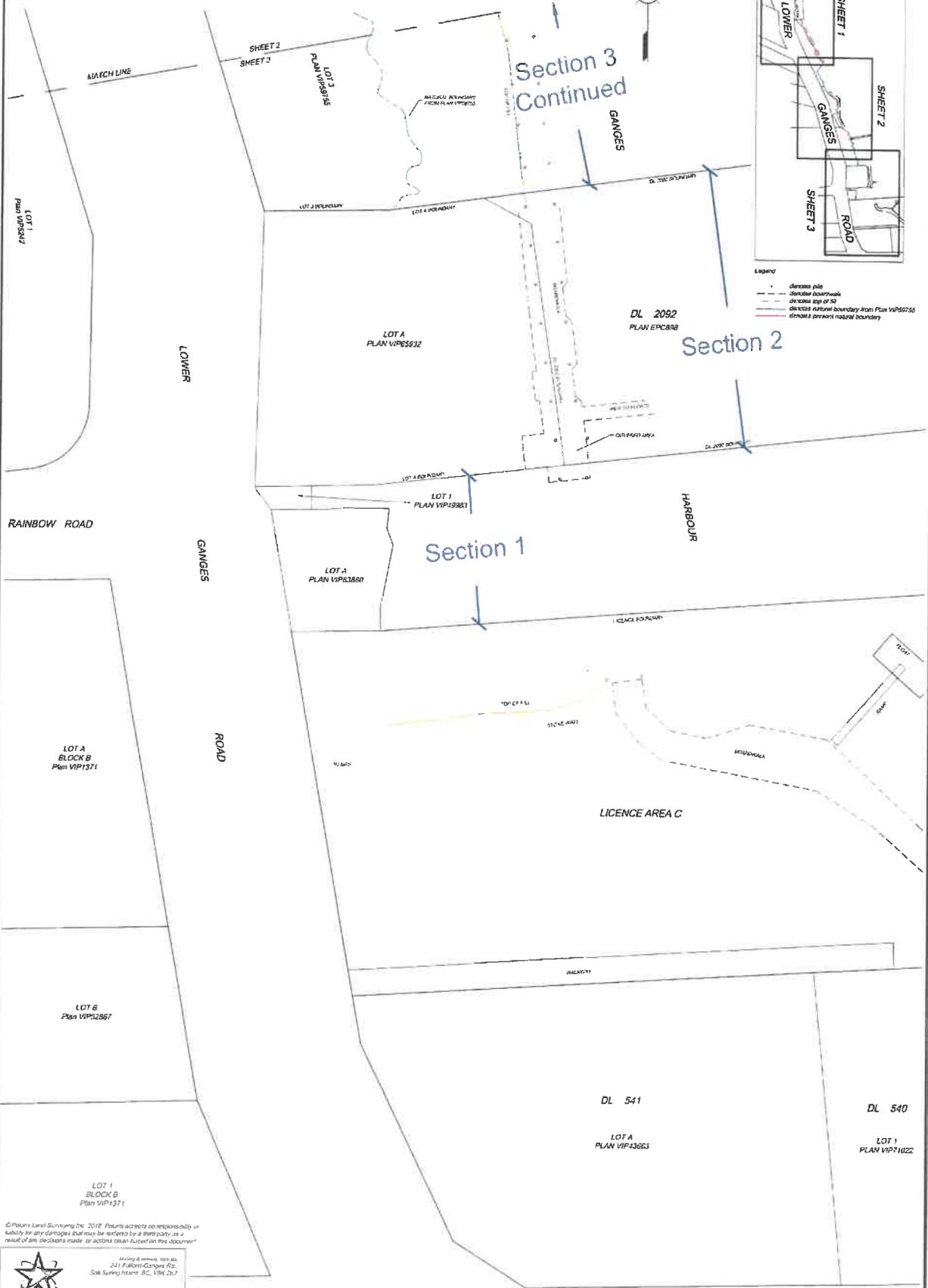
## DISPUTE RESOLUTION

If requested in writing by either the Client or the Consultant, the Client and the Consultant shall attempt to resolve any dispute between them arising out of or in connection with this Agreement by entering into structured non-binding negotiations with the assistance of a mediator on a without prejudice basis. The mediator shall be appointed by agreement of the parties. If a dispute cannot be settled within a period of thirty (30) calendar days with the mediator, the dispute shall be referred to and finally resolved by arbitration under the rules of the arbitrator appointed by agreement of the parties or by reference to a Judge of the British Columbia Court.



All dimensions are in metres and decimals thereof unless otherwise noted.  
The extended plan size of this plan is 250mm in width by 460mm in height (D-Scale) were plotted at a scale of 1:200.

Key Plan  
Scale 1:2500



- Legend
- - - - - distance pole
  - - - - - distance measurement
  - - - - - distance top of 50
  - - - - - distance natural boundary from Plan VPS1975
  - - - - - distance proposed natural boundary

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HEREFORD AVENUE

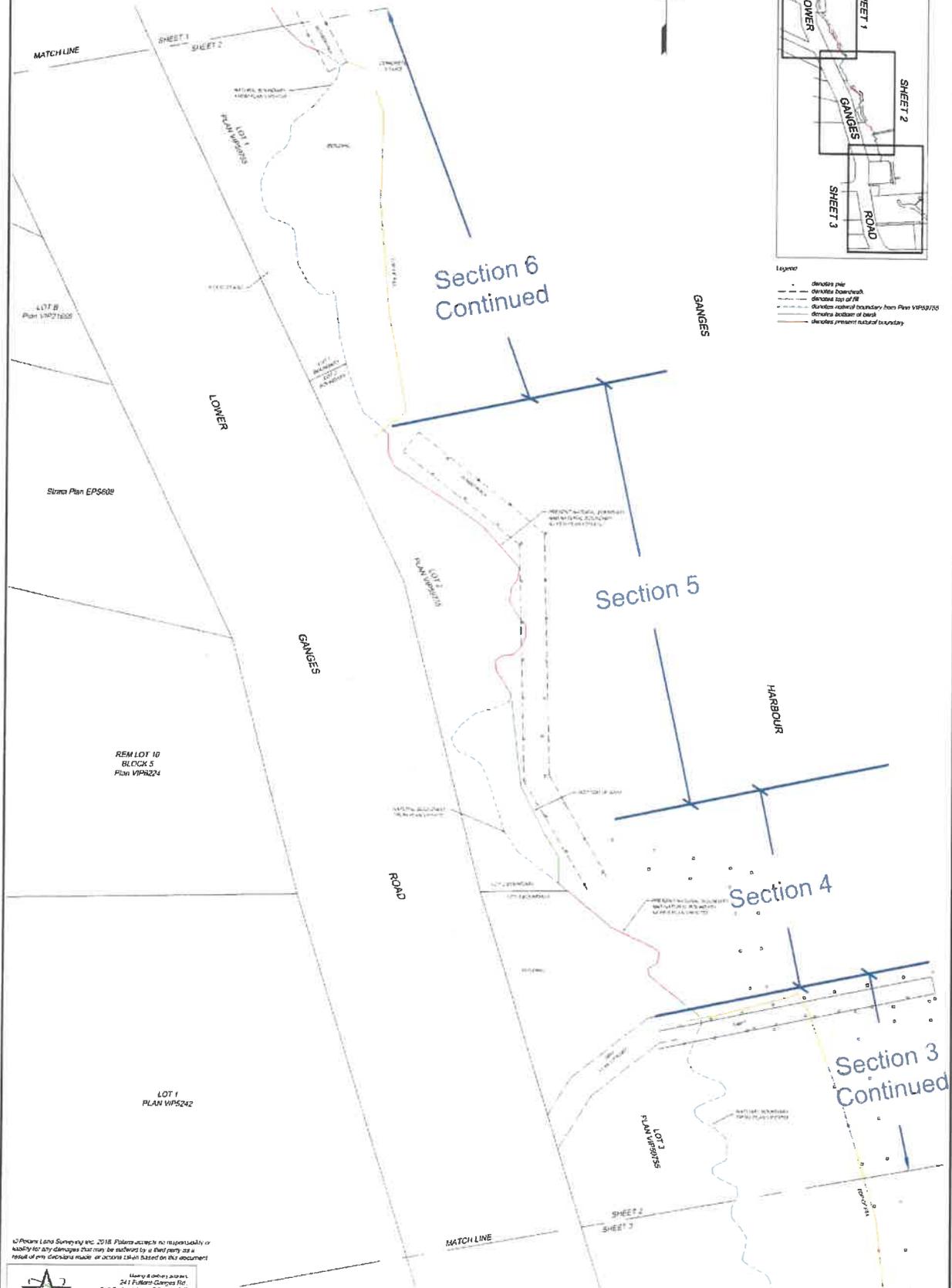
PURVIS LANE

Date: 2018-04-20  
File: 0176-001  
Drawing: 0176-001-LOCAL-CAD  
Layout: S&B Plot



All dimensions are in metres and decimals thereof unless otherwise stated.  
 The scale of this plan is 1:2500.  
 It is made by planimetry or by other means when justified at a scale of 1:2500.

Key Plan  
 Scale 1:2500



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Date: 2018-04-25  
 File: 0478-05  
 Drawing: 0478-05-0004-02-00  
 Layout: SCL Plan



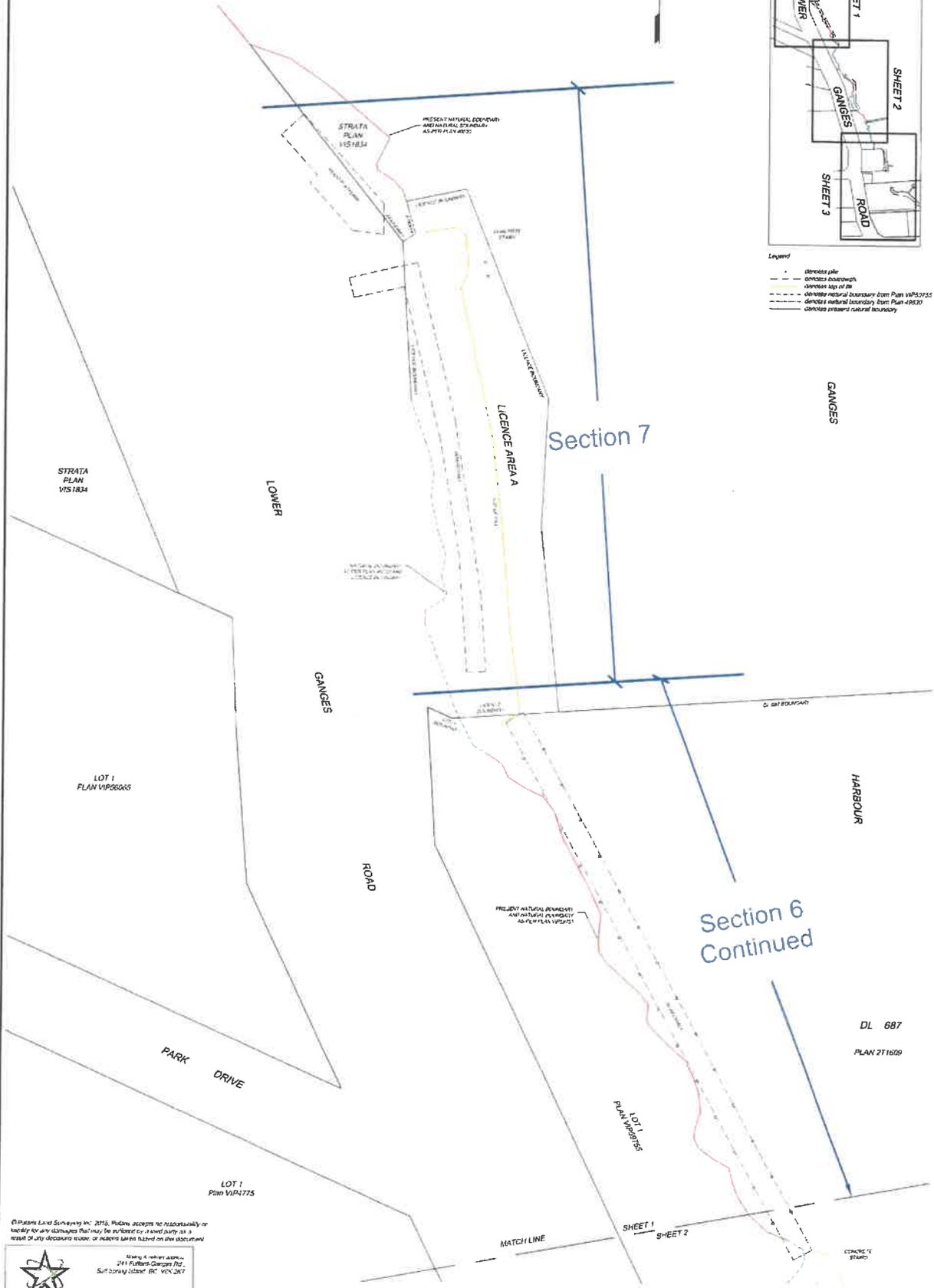
All dimensions are in metres and bearings shown unless otherwise noted.  
The assumed elevation of the site is 55.0m in water by 56.0m in height (D-30m) when plotted at a scale of 1:200.

Key Plan  
Scale 1:2500



Legend

- - - - - proposed path
- - - - - proposed easement
- - - - - proposed top of fill
- - - - - proposed natural boundary from Plan VSP2375
- - - - - proposed natural boundary from Plan VSP237
- - - - - proposed natural boundary



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DL 687  
PLAN 271629

Rev: 2015-04-07  
Plan: 271629  
Drawing: 0476-05-LOCAL-IC20  
Layout: Site Plan

## Appendix E

### Structural Constraint Report (Stantec)



**Ganges Harbour Walk Structural  
Assessment**

115618081

September 10, 2018

Prepared for:

Capital Regional District  
Salt Spring Electoral District

Prepared by:

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## Sign-off Sheet

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Prepared by \_\_\_\_\_

(signature)

**Adrienne Leech, EIT**

Approved by \_\_\_\_\_

(signature)

**Bryan Gallagher, P. Eng.**



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## Executive Summary

Stantec Consulting Ltd. conducted a structural assessment of the existing Ganges Harbour Walk on Salt Spring Island on April 30, 2018. The Ganges Harbour Walk was originally constructed approximately 30 years ago. It appears that the boardwalk was built to appropriate standards and codes at the time of construction. A load rating for the typical configuration of boardwalk was completed and it was found that the sections of the boardwalk in good condition would satisfy the loading requirements of the British Columbia Building Code 2012 (BCBC 2012) for gravity loading. A load rating of the driven piles was not completed as records regarding the depth and capacity of the piles were not available at the time of the assessment. The portions of the boardwalk supported on concrete piers were assessed for current seismic loads and were found to be inadequate. Apart from the in-use sections of boardwalk, which appear to be regularly maintained, the rest of the boardwalk sections have not been properly maintained, which has resulted in significant deterioration. The load rating does not apply to the out-of-use sections of the boardwalk which are currently considered to be unsafe for public use.

There are two sections of boardwalk reviewed that are currently in use. Section 1 refers to the lookout portion of the boardwalk at Rotary Park. This section was found to be in fair to good condition and has been regularly repaired and maintained. A cursory review of the lookout portion of boardwalk was conducted as it may be a possible tie-in location for future boardwalk construction. Section 2 refers to the Fishery boardwalk. This section is in fair to good condition and also appears to undergo regular maintenance and repairs. Stantec has recommended that several repairs are completed for the Fishery boardwalk as part of ongoing maintenance to keep this structure in good condition.

Section 3 and 4 refers to the stand-alone piles located north of the Fishery property. Only piles above the water line in section 3 were accessible during the assessment, these piles were found to be in poor condition. It is assumed that the inaccessible piles in section 4 are also in poor condition based on a visual assessment conducted from the shore. It is Stantec's opinion that these piles, in their current condition, are not feasible for use in any future construction.

There are three sections of boardwalk that were completed during the original construction but are currently not open to the public. Section 5 and 6 are completed portions of the board walk within the high tide region, supported off of the exposed rock by piles with concrete piers. Section 5 was not accessible during the review, but it is assumed to be in similar condition to section 6. The substructure of section 6 appears to be in fair condition aside from the concrete piers which are in poor condition. Replacement of a percentage of substructure components and extensive repair of the concrete piers, would be required in order to restore the existing boardwalk. Section 7 refers to the portion of boardwalk and lookout platform located nearest to Peck's Cove Park. This section of boardwalk is above the high tide line and is supported on driven piles. The substructure appears to be in fair condition and would likely only require minor repairs and replacements. In order to restore the piles, further testing would be required to determine the depth and capacity. The pile-caps and stringers of the out-of-use sections were found to be in fair condition and it is anticipated that only minor repairs and replacements would be required to these components. The deck and railings of all three out-of-use sections are in poor condition and would likely require full replacement.

The expected lifespan of regularly maintained creosote-treated timber piles is approximately 50 years. The Harbour walk is approximately 30 years old and several sections have not been properly maintained during this time. If the existing piles were to be successfully re-purposed, they would likely only have 20 years of useful life, prior to requiring extensive maintenance and repairs. Once a conceptual alignment/ design of the Harbour Walk has been completed,

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a cost-benefit analysis would be recommended to determine whether it is feasible to restore the boardwalk existing boardwalk in sections 5-7 versus a full replacement.

There are two stone/ concrete stairs that access the beach from the boardwalk and one timber stair that accesses the boardwalk from Lower Ganges Road. All three stairs were rated in fair condition; however, it was noted that the stone/ concrete stair do not meet all of the requirements of the BCBC 2012. Minor maintenance and repairs have been recommended for all three stairs.

# GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

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## 1.0 INTRODUCTION

### 1.1 BACKGROUND

The Ganges Harbour Walk project started in the 1980's. The intention of the project was to construct a boardwalk from Rotary Park to Peck's Cove Park. The project was a collaborative effort between public agencies and community volunteers. Construction on the project stopped approximately 10 years later leaving several completed sections of the boardwalk inaccessible to the public and these sections have since fallen into disrepair. Records of the boardwalk construction are not available; however, CRD staff were able to provide some information on the project. Local residents report that the completed portions of the boardwalk were built to BC Ferries wharf standards at the time of construction. It is assumed that no piles have been replaced for any boardwalk section. There is no available information regarding the depth or capacity of the existing piles.

### 1.2 SITE INSPECTION

On April 30, 2018 Bryan Gallagher (Structural Engineer) and Adrienne Leech (Structural EIT) conducted a site review of the accessible sections of the Ganges Harbour Walk on Salt Spring Island. Wayne Lee (CRD representative) and Andrew Jackson (Ryzuk) were also present for this review. All parties arrived at 10 am at Ganges Harbour and discussed the project background and current scope of work. The CRD had acquired land access permits for private property prior to the site visit making it possible to access the boardwalk section located within the Fishery land boundary. Bryan and Adrienne walked the site starting south of the Fishery land continuing north to Peck's Cove Park. Inaccessible sections of boardwalk were viewed from the road or beach. The accessible sections of boardwalk were visually inspected using non-destructive methods. During the inspection the tide was very low, estimated to be approximately 0.9m at 11 am, allowing inspections to take place from the beach in several locations.

### 1.3 SITE DESCRIPTION

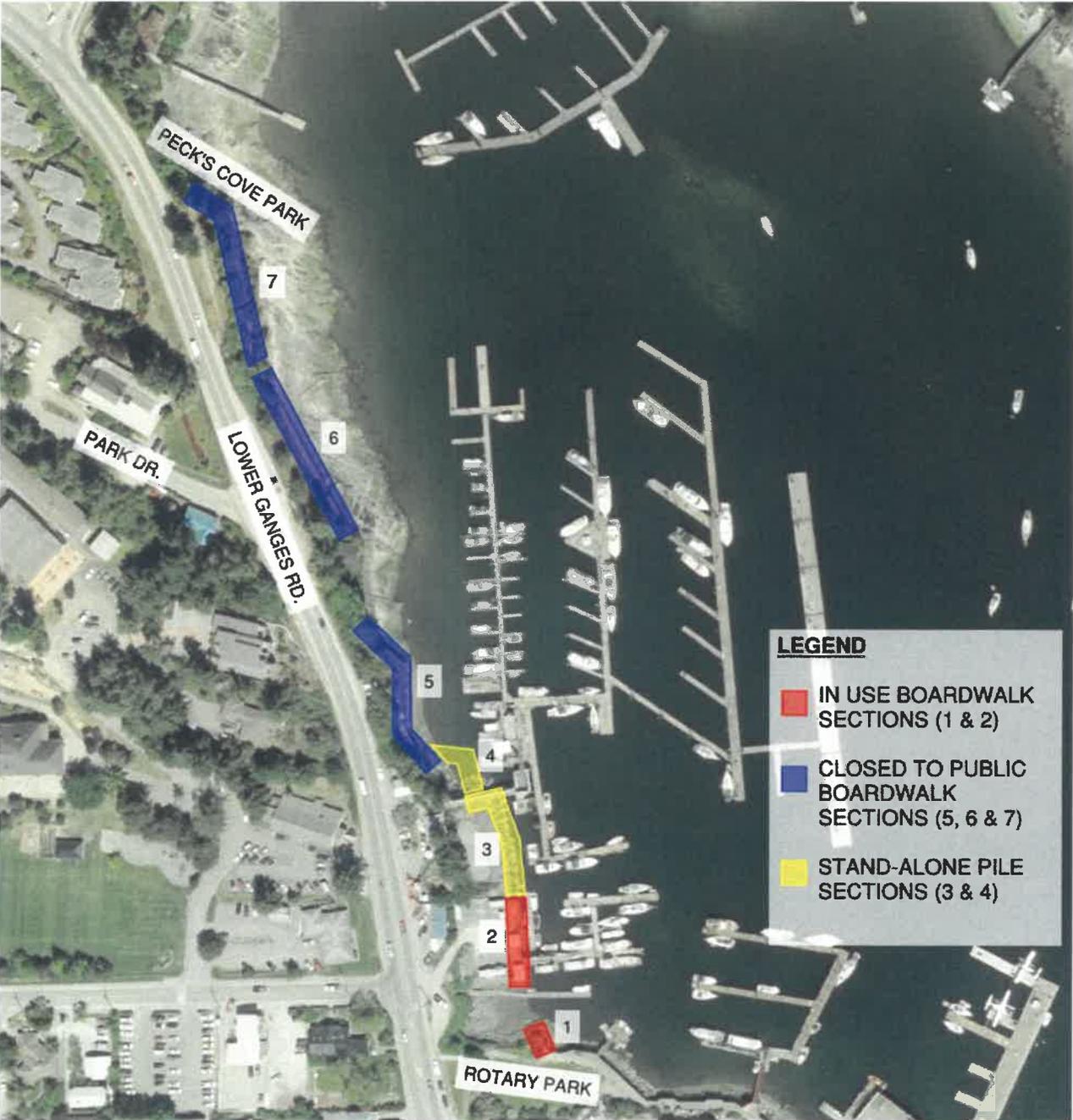
The Ganges Harbour Walk was assessed in 7 sections for the purposes of this report, as shown in the site map provided in **Figure 1**. There are five board walk sections (section 1, 2, 5, 6, 7) that were constructed between 1980 and 1991. Two of these boardwalk sections (section 1, 2) are currently in use and are maintained by public/ private owners. Three walkway sections (section 5, 6, 7) were constructed but are in disrepair and are not accessible to the public. Two sections (section 3, 4) consist of an alignment of timber piles with no constructed boardwalk. There is one wood staircase that accesses the boardwalk from Lower Ganges Road and two stone/concrete stairs that access the beach from the boardwalk. The boardwalk construction was found to be uniform with minimal changes to the structural framing between sections.

### 1.4 SCOPE OF INSPECTION

**Figure 1** below shows the Ganges Harbour walk and the relevant sections that are part of this assessment. Refer to the Land Survey in **Appendix B** for more detailed site information.

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**Figure 1 - Boardwalk sections requiring assessment (see Appendix B for Land Survey)**

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The scope of work consists of two stages:

- 1) An assessment of the completed boardwalk sections and a technical evaluation of existing structural competence and likely life span of the following components:
  - a. Existing superstructure elements: pile caps, stringers, decking and guardrails.
  - b. Existing substructure elements: concrete piers, timber piles and timber bracing.
  - c. Existing stair land access constructed of wood (located on Lower Ganges Road across from Park Drive).
  - d. Two existing stair beach accesses constructed of stone and concrete.
- 2) An assessment of the existing exposed timber piles.

### 1.5 CONDITION RATINGS

Components have been rated according to the following condition states:

**Excellent:** New condition

**Good:** Normal wear and deterioration generally not requiring maintenance or repair.

**Fair:** Minor defects, deterioration or collision damage; generally requires maintenance or repair.

**Poor:** Advanced deterioration, significant defects or collision damage; repairs required

**Very Poor:** Serious defects, deterioration or collision damage, imminent failure of component requiring immediate repair or replacement and/ or load restrictions.

### 2.0 INSPECTION RESULTS

The boardwalk superstructure construction appears to follow typical "light-duty" marine construction. The structural system of the boardwalk consists of timber deck boards supported by pressure treated stringers. The stringers bear on pressure treated pile caps supported by creosote-treated piles, which are braced in some locations with creosote-treated braces. The guardrail components are pressure-treated timber, and the posts are connected with a standard two-bolt assembly. It appears that all piles are original and are approximately 30 years old. In sections that have been maintained (1 and 2), some timber components have been repaired or replaced. In the sections that have never been in use, all wood components appear to be original.

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

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**Figure 2 - Typical boardwalk assembly with concrete piers**

Timber construction typically utilizes some form of preservative treatment in marine environments to prevent rot and damage by marine organisms. All of the piles assessed appeared to be creosote-treated and the remaining timber components appeared to be pressure-treated, with the exception of the original deck boards in sections 2, 5, 6 and 7. These deck boards are deteriorated to the point where it is not possible to confirm the material. This could be an indication that treated timber was not used for the original deck boards.

If installed and maintained properly, creosote-treated timber piles can last as long as 50-years in a marine environment, while pressure-treated timber has a typical life-span of 30-years if properly maintained. If not installed or maintained properly, the life span can be considerably less.

Wood components were tested using a non-destructive technique called sounding. A hammer is used to hit the wood component and the resulting sound is representative of the condition of the wood. A dull or hollow sound will indicate decay or a hollow cavity inside the wood. This method of testing will diagnose significant damage where considerable decay is present near the surface of the wood. No destructive testing such as boring or drilling was done as part of this assessment.

### 2.1 SECTION 1 – ROTARY PARK

This portion of boardwalk is located in Rotary Park, is open to the public and appears to undergo regular maintenance. Only the lookout area shown in **Figure 3** below was reviewed as part of this assessment. Stantec conducted a cursory review of the boardwalk components as this would be the proposed tie-in location for the Ganges Harbour Walk conceptualized project.

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The structural system for this section follows typical marine construction. The decking is pressure treated 38mm x 184mm timber, supported by pressure treated stringers at approximately 800mm on center. The stringers are supported by pressure treated pile caps, typically supported by two creosote treated timber piles. The guardrails follow typical pressure treated timber construction. This section appears to be in relatively good condition.

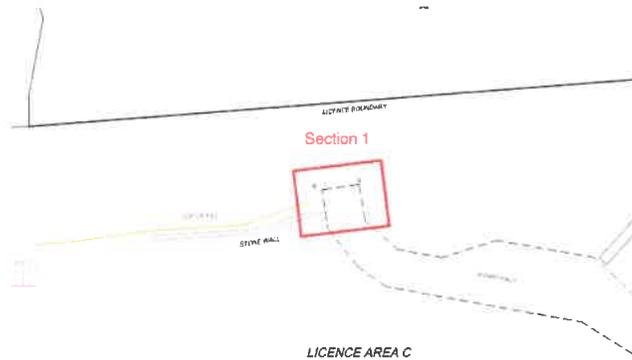


Figure 3 - Location of boardwalk section 1 (see Appendix B for Land Survey)

## 2.2 SECTION 2 – FISHERY

The Fishery boardwalk section is currently in use and appears to be actively maintained by the private property owners. This section of boardwalk has some areas that are currently being used for light storage. A Jib crane is located on the north end of this section of boardwalk.

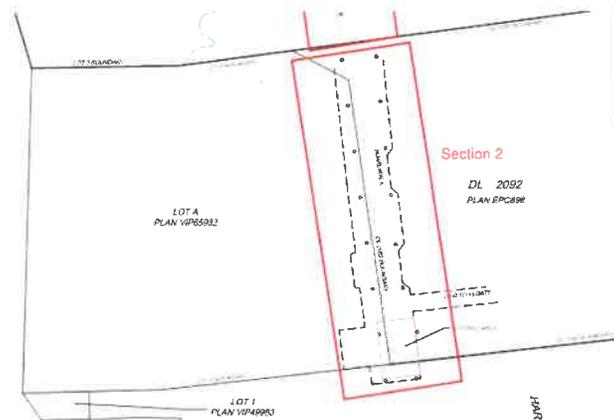


Figure 4 - Location of boardwalk section 2 (see Appendix B for Land Survey)

### 2.2.1 Piles

The piles were visually inspected from the floating dock below. There are two rows of 8 piles in this section, the size of the piles varies from approximately 230mm to 360mm in diameter spaced at approximately 4.9m. It appears that

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rip-rap was placed around the piles at a 1:1 slope after the piles were installed. This has resulted in large boulders leaning up against some of the piles. This has the potential to induce lateral loading that the piles were likely not designed for. The piles are located within the tidal zone and have heavy marine growth. Accessible piles showing signs of damage were sounded with a hammer. One pile was found to have a significant cavity at the tidal zone location. Sounding of this pile suggest that the remainder of this pile is in fair condition. The pile located furthest south-west in this section was found to be visibly leaning toward the water, which could be the result of lateral loading induced by the rip-rap. This pile was not accessible for further review. The top of the piles have a metal cap that protects the pile from weather and separates the pile from the pile cap. these metal caps are deteriorating and have reached the end of their useful life. At this time it is not feasible to replace the metal caps as this would require removal of the deck structure.

### 2.2.2 Braces

The cross-braces are creosote-treated 102mm x152mm sections bracing the eastern line (water side) of the piles in every other bay. The braces are connected to the piles with a single thru-bolt connection at each end. It does not appear that the braces have been replaced at any point in the past. The portions of the braces below the high-water line show heavy marine growth. An accessible brace was sounded and appeared to be in fair to poor condition. It appears that one brace is missing in the center bay which requires replacing. The empty bolt hole, below the tide line, where the brace is missing has exposed the unprotected heart-wood of the pile to potential marine borer activity and could compromise the pile if left for a prolonged period of time.

### 2.2.3 Pile Caps

The pile caps are typically 241mm x305mm pressure-treated sections simply supported by two piles spanning an approximate distance of 2.44m. The only accessible pile cap in this section was the furthest north member. The decking in this area does not extend over the pile cap which has left it exposed. Organic growth is present along the member and there are signs of rot surrounding the connection to the supporting pile. The pile cap was sounded with a hammer and was found to be in poor condition.

The remaining pile caps were visually reviewed from the underside. Only the members visible from the floating dock were reviewed. No concerns were noted with the remaining pile caps.

### 2.2.4 Stringers

The stringers are typically 140mm x241mm pressure-treated members supporting deck boards at approximately 800mm on center. Stringers are simply supported by pile caps spanning an approximate distance of 4.9m. At the north-end of the boardwalk a membrane product (likely "Blueskin") has been placed between the stringers and the decking. This has likely been added to reduce the potential for rot in the lower member. This is not a common practice for marine structures but appears to be working as intended for this area. Minor presence of organic growth is present on the stringers. Accessible stringers were sounded with a hammer and were found to be in fair condition.

### 2.2.5 Decking

The decking is comprised of 38mm x184mm pressure-treated boards supported by stringers. The deck appears to have been replaced at least once since the original construction and was found to be in fair to good condition. The

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decking should be inspected annually, as part of regular maintenance, and any boards found to have major splits or rot pockets should be replaced. Nails that are found to be proud should be sunk down to reduce tripping hazards.

At the north-end of this section of boardwalk there are several deck boards missing, which may accelerate the deterioration of the stringer ends and pile caps due to exposure.

### 2.2.6 Guardrails

The guardrail consists of two 38mm x140mm top members and two mid-height members spanning between 89mm x89mm posts spaced at 2.44m on center. The posts are connected with a standard two-bolt assembly. The top connection is to an 89x89 bull-rail and the lower connection is to the outermost stringer. It appears that the guardrails are actively maintained and that components of the guardrail assembly have been replaced recently and are in relatively good condition. Annual inspections of the guardrails should be completed as part of regular maintenance and members found to have major splits or rot pockets should be replaced.

### 2.2.7 Shelter Structures on Boardwalk

There are two shelter structures on the south end of the Fishery section of the boardwalk. A timber structure with a metal roof and an aluminum tent structure with a plastic roof and plastic walls. Both of these structures would be classified as "light-structures". Detailed review of these structures was outside of the scope of work for this assessment and it is unknown if this section of the boardwalk was designed to support these structures.

### 2.2.8 Jib Crane

A Jib Crane is located on the north end of the boardwalk. Detailed review of the Jib Crane was outside of the scope of work for this assessment. A load rating is not clearly identified on the side of the crane and the base connection would suggest that this crane is only appropriate for very light loads.

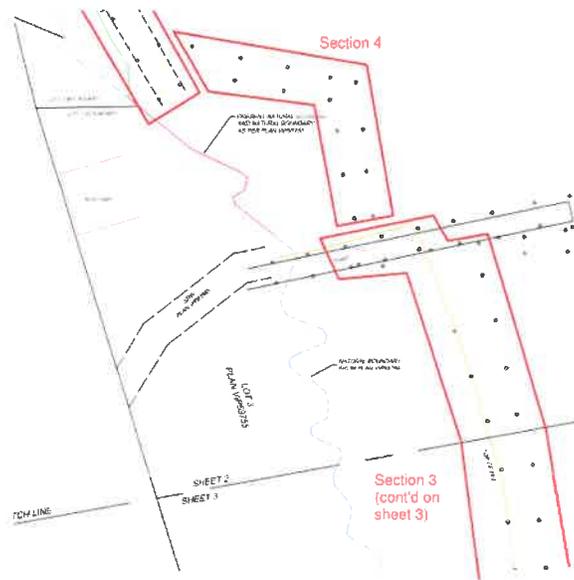
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## 2.3 SECTION 3 AND 4 – STAND-ALONE TIMBER PILES

The stand-alone timber pile sections (section 3 and 4) are located just north of the Fishery. These areas can be seen in

Figure 5 below.



**Figure 5 - Locations of boardwalk sections 3 and 4 (see Appendix B for Land Survey)**

These piles were installed between 1980 and 1991, there is currently no available data on pile depth or soil conditions. Since their installation, the piles have been continually exposed to the high moisture conditions of a marine environment, which makes them highly susceptible to weathering and rot. The piles appear to be creosote-treated; however, it appears that no protection has been provided for the sawn-off ends of the piles since installation, leaving the end grain of the timber exposed. Piles left for extended periods of time in this condition have a significantly reduced lifespan. **Figure 6** below shows the typical deterioration at the top of these piles. These piles were also found to have splitting extending down from the top of the pile. These conditions were typical for all piles located in section 3. Piles located in section 4 are in the ocean and were not accessible for a detailed review; however, the conditions are assumed to be similar as the section 4 piles have also not been detailed with a cap to protect the tops of the piles.

Accessible piles (section 3) were sounded with a hammer and some of the larger diameter piles appeared to be in fair condition approximately 1.2m from the top of the pile. The piles in section 4 and the line of piles to the east (water side) of section 3 are located within the tidal zone. The tidal zone is the location where piles undergo the most deterioration. These piles were not accessible to inspect closely but heavy marine growth was evident for these piles.

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Figure 6 - Typical Deterioration at Top of Section 3 Pile

## 2.4 SECTION 5 & 6 – NORTH OF FISHERY (SUPPORTED ON CONCRETE PIERS)

### 2.4.1 Section 5 – Cursory Assessment

Section 5 is located north of the stand-alone piles, it was not accessible and could only be observed from a distance. The completed boardwalk section is approximately 60m long; however, it was never open to the public. The structure appears to follow the same structural assembly as noted for sections 1 and 2. There are two rows of piles 2.44m apart. Section 5 consists of 13 piled bents spaced at approximately 4.9m. The piles are supported by concrete piers, that have been cast onto rock. The components in this section of boardwalk are assumed to be in similar condition to components in section 6 as both sections are located within the tidal zone and have never been in use or maintained.

Section 6 is approximately 68m long and was reviewed as part of this assessment. The underside of this section was reviewed from the beach. The top side of the structure was only accessed at the ends, as the deck was found to be unsafe to walk on.

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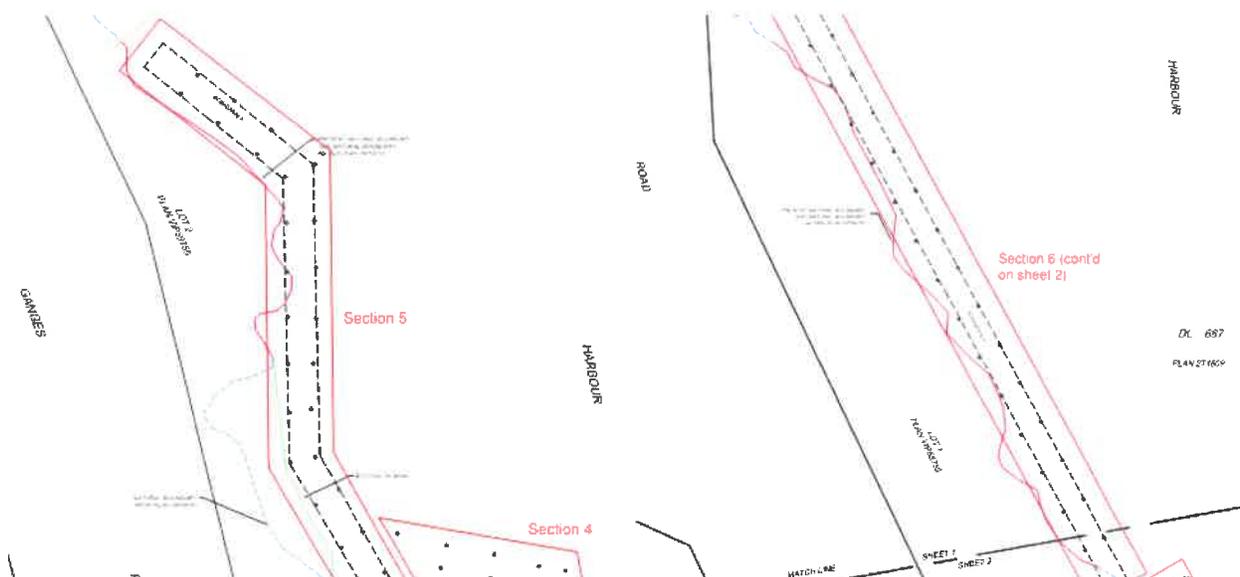


Figure 7 - Locations for boardwalk sections 5 and 6 (see Appendix B for Land Survey)

## 2.4.2 Section 6 – Detailed Assessment

### Concrete Piers

The piles in sections 5 and 6 are supported by concrete piers. Record drawings/ details were not available for the concrete construction. The concrete piers look to have some vertical reinforcement, but it is unknown whether or not the pier is dowelled into the existing underlying rock. These concrete piers are significantly smaller than what is typically used to support piles off of exposed rock. A local resident involved in the construction of the boardwalk stated that the concrete piers have a 15M steel cage dowelled several centimeters into bedrock, with the exception of section 5 concrete piers that were cast directly on top of rocky beach material and therefore not dowelled into bedrock.

Several locations were noted where the concrete pier has been severely undermined, or where the concrete has experienced significant cracking. The concrete piers in this section were found to be in typically poor condition, with 5 piers found to be in very poor condition.

### Piles

The piles are encased and supported at the base by the concrete piers. There are two rows of piles 2.44m apart. Section 6 consists of 14 piled bents, spaced at approximately 4.9m. The piles vary in size from approximately 254mm to 330mm in diameter and are approximately 2m in height. The piles in this section appear to be in fair condition. The tops of the piles are protected by metal caps that separates the pile from the pile cap, the metal caps are generally in poor condition.

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### Pile caps

The pile caps are typically 241mm x 241mm pressure-treated members simply supported by two piles spanning an approximate distance of 2.44m. Pile caps with signs of damage were sounded and were found to generally be in fair condition.

### Stringers

The stringers are typically 140mm x 241mm pressure-treated members 800mm on center, simply supported by pile caps spanning an approximate distance of 4.9m. Typical stringers were sounded and found to be in fair condition.

### Decking

The decking is comprised of 38mm x 140mm timber decking supported by stringers. The deck is in very poor condition and has rotted to the point that it is no longer safe for foot traffic.

### Guardrail

The guardrail consists of two 38mm x140mm top members and two mid-height members spanning between 89mm x89mm posts spaced at 2.44m on center. The guardrails have not been maintained and several locations were found to have heavy organic growth and/ or rot. Several posts were split at the two-bolt connection.

## 2.5 SECTION 7 – NORTH END (SUPPORTED ON DRIVEN PILES)

Section 7 is located at the north end of the Harbour Walk, shown in **Figure 8**, and has never been accessible to the public. The visible portions of the boardwalk has a similar superstructure as section 6; however, section 7 has been constructed on fill above the high-tide zone and the piles have been driven into the ground. No existing information is available for the depth or capacity of the piles. Section 7 is approximately 55m long supported by two rows of piles 2.44m apart spaced at approximately 4.9m. The top of the piles have metal caps providing separation from the pile caps. The 241mm x 241mm pile caps span a distance of 2.44m. The pile caps support 140mm x241mm stringer spanning approximately 4.9m at 800mm on center. The condition of the Section 7 members was found to be similar to what was found for Section 6.

A lookout platform is located at the north end of Section 7. This platform is closed to public access. There was limited visibility of the underside of the structure; however, it appeared to have a similar assembly to the exposed portions of Section 7. From the accessible areas of the structure, it was found that the 38mm x140mm deck boards span between pressure treated stringers at approximately 915mm on center. The deck boards are in very poor condition having rotted away in some areas. The guardrails were found to be in fair to poor condition, with the top rail found to be in very poor condition.

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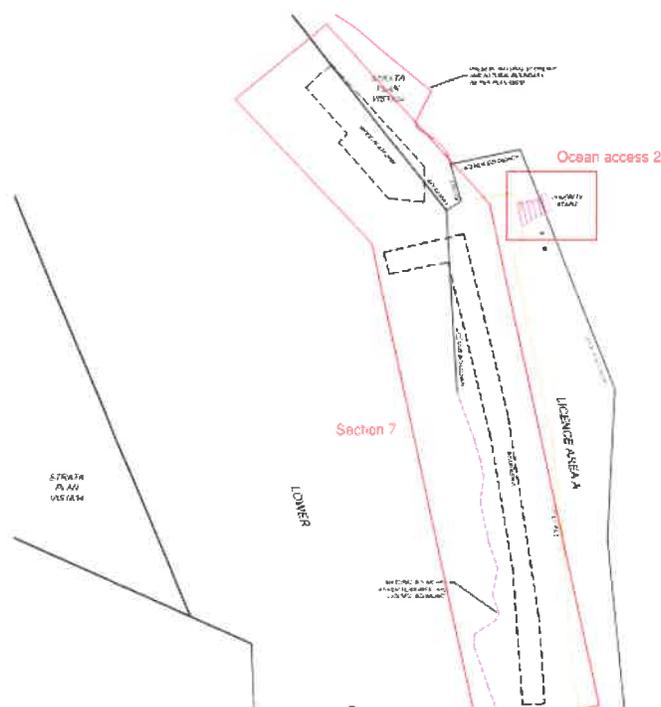


Figure 8 - Location for section 7 boardwalk and lookout platform

## 2.6 BOARDWALK ACCESSES

### 2.6.1 Beach Accesses

There are two beach accesses along the boardwalk, both of which have a combination of stone and concrete constructed stairs. The stairs are within the high-water zone and the concrete at the stair base has become undermined. Several stair risers are in excess of 180mm which is the maximum allowable rise for public stairs in accordance with the 2012 British Columbia Building Code (BCBC). Neither access 1 or 2 currently have handrails. For public stairs less than 1100mm wide, one handrail is required and where public stairs are wider than 1100mm, a handrail on both sides of the stairs is required in accordance with the BCBC 2012.

### 2.6.2 Land Access

The land access is a wood staircase located on Lower Ganges Road across from Park Drive and is currently in use. The stair construction appears to generally conform with current code requirements and there is evidence of some repair and maintenance. The stairs are located in a shaded area and organic growth and vegetation is present. The upper landing is supported by a wood frame bearing on existing rock. The lower landing is supported on piles. Accessible components of the stairs were visually assessed and sounded. The pile caps and stringers appeared to be fair condition but show evidence of minor rot at the sawn-off ends. The deck boards on the lower landing are assumed to be in very poor condition and have been covered with plywood. The deck boards on the upper landing are in poor condition and have rotted away in some areas. The stair treads are in poor condition and were found to have rot and algae growth. The treads are supported by steel angles which appear to be in fair condition. The top rail

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of the guardrail is in poor condition with heavy vegetation growth. The other guardrail components appear to be in fair condition.

### 2.7 SUMMARY OF FINDING

The table below summarizes the finding of the Ganges Harbour Walk condition assessment. Member sizes noted are the typical minimum member size for each section (actual member sizes were found to vary slightly).

The residential life estimates provided are a conservative estimate. If properly maintain, it can be expected that many of the components will have a longer service life than presented in the table. Residual life estimates should be updated with each detailed inspection (recommended at 5-year intervals for marine structures).

Item	Component Description	Condition	Damage/ Comments	Residual Life	Recommendation
<b>Section 1 - In-Use Lookout Portion of Boardwalk at Rotary Park</b>					
Piles	Creosote-treated Timber	Fair to Good		10 years	-Detailed Inspection at 5-year cycle
Pile Caps	Pressure-Treated	Fair to Good		10 years	-Detailed Inspection at 5-year cycle
Stringers	Pressure-Treated	Fair to Good		10 years	Detailed Inspection at 5-year cycle
Decking	Pressure-Treated	Fair to Good		5 years	-Maintenance Inspection annually -Detailed Inspection at 5-year cycle
Guardrails	Pressure-Treated	Fair to Good		5 years	-Maintenance Inspection annually -Detailed Inspection at 5-year cycle
<b>Section 2 - In-use Portion of Boardwalk on the Fishery Property</b>					
Piles	Creosote-treated Timber	Fair	-Cavity at 1 location -Leaning pile 1 location -Heavy marine growth -lateral loading due to rip-rap.	10 years	-Fill Cavity 1 Location -Adjust rip-rap leaning on piles -Detailed Inspection at 5-year cycle to include dive review -Monitor leaning pile
Braces	102x152 Creosote-treated Timber	Poor	-Missing braces -heavy marine growth	2 years	Replace all braces within 2 years

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Inspection Results  
September 10, 2018

Pile Caps	241x305 PT Timber	Fair	-Furthest north pile cap has significant rot	10 years	-Replace north pile cap within 2 years -Detailed Inspection at 5-year cycle
Stringers	140x241 PT Timber	Fair to Good		10 years	-Detailed Inspection at 5-year cycle
Decking	38x184 Timber	Fair to Good	-Evidence of previous repairs/ replacements -Decking missing at north-end	5 years	-Maintenance Inspection annually -Detailed Inspection at 5-year cycle -Replace missing decking at north-end within 2 years
Guardrails	Timber	Fair to Good	-Evidence of previous repairs/ replacements	5 years	-Maintenance Inspection annually -Detailed Inspection at 5-year cycle
<b>Section 3 - Stand-alone Piles just North of Fishery</b>					
Piles	Creosote-treated Timber	Poor	-Rot at top of pile -Significant splitting	0 years	-Remove or replace
<b>Section 4 - Inaccessible Stand-alone Piles North of Fishery and ramp</b>					
Piles	Creosote-treated Timber	Poor	-Rot at top of pile -Significant splitting	0 years	-Remove or replace
<b>Section 5 -Inaccessible Completed Boardwalk Section North of Fishery (closed to public)</b>					
Concrete Piers	600 dia. Concrete	Poor	-Closed to public, reviewed from distance -Undermining and cracking of concrete	2-5 years	-Remove, repair or replace
Piles	Creosote-treated Timber	Fair	-Closed to public, reviewed from distance	10 years	-Remove or incorporate into refurbished boardwalk
Pile Caps	241x241 PT Timber	Fair	-Closed to public, reviewed from distance	5-10 years	-Remove or incorporate into refurbished boardwalk
Stringers	140x241 PT Timber	Fair	-Closed to public, reviewed from distance	5 years	-Remove or incorporate into refurbished boardwalk
Decking	38x140 Timber	Very Poor	-Extensive Rot -Currently unsafe	0 Years	-Remove or replace

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Inspection Results  
September 10, 2018

Guardrails	Timber	Poor	-Smaller components rotten -Some components (posts) may be salvageable for re-use	2 Years	-Remove or replace
<b>Section 6 - Accessible Completed Section of Boardwalk North of Park Drive (closed to public)</b>					
Concrete Piers		Poor	-Closed to public, accessible for review -Undermining and cracking of concrete	2-5 years	-Remove, repair or replace
Piles	Creosote-treated Timber	Fair	-Closed to public, accessible for review	10 years	-Remove or incorporate into refurbished boardwalk
Pile Caps	241x241 PT Timber	Fair	-Closed to public, accessible for review	5-10 years	-Remove or incorporate into refurbished boardwalk
Stringers	140x241 PT Timber	Fair	-Closed to public, accessible for review	5 years	-Remove or incorporate into refurbished boardwalk
Decking	38x140 Timber	Very Poor	-Extensive Rot -Currently unsafe	0 Years	-Remove or replace
Guardrails	Timber	Poor	-Smaller components rotten -Some components (posts) may be salvageable for re-use	2 Years	-Remove or replace
<b>Section 7 - Mostly Inaccessible Completed Section of Boardwalk Approaching Peck's Cove Park (closed to public)</b>					
Piles	Creosote-treated Timber	Fair	-Closed to public, reviewed from distance	10 years	-Remove or incorporate into refurbished boardwalk
Pile Caps	241x241 Pt Timber	Fair	-Closed to public, reviewed from distance	5-10 years	-Remove or incorporate into refurbished boardwalk
Stringers	140x241 PT Timber	Fair	-Closed to public, reviewed from distance	5 years	-Remove or incorporate into refurbished boardwalk
Decking	38x140 Timber	Very Poor	-Extensive Rot -Currently unsafe	0 Years	-Remove or replace

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Load Rating  
September 10, 2018

Guardrails	Timber	Poor	-Smaller components rotten -Some components (posts) may be salvageable for re-use	2 Years	-Remove or replace
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### 3.0 LOAD RATING

A load rating calculation was undertaken for a typically constructed section of the boardwalk. The calculations were completed assuming “good” condition, treated Douglas-Fir timber and “good” condition concrete. For the sections currently not in public use, significant repairs/ replacements would be required to improve the structure to meet the “good” condition rating. The out-of-use sections are currently considered to be unsafe for public use.

The typical assembly used for the load rating was: 38mm x140mm deck boards, supported by 140mm x 241mm stringer at 800mm on center. A span of 4.9m was used for the stringers. A span of 2.44m was used for the 241mm x 241mm pile caps. In compliance with the 2012 British Columbia Building Code (BCBC 2012), a live load of 4.8 kPa (100 psf) was used to represent pedestrian assembly areas. Individual members were also checked for a concentrated load of 1.3 kN (290 lbs) applied at any point to produce the most critical loading situation. It was assumed that no vehicles (even small maintenance vehicles) would be permitted to drive on the boardwalk.

The typical guardrail is comprised of three major parts; the top rail, side rails and the posts, all of which were checked in accordance to the BCBC 2012, (clause 4.1.5.14). The top rail consists of two 38mm x140mm boards spanning 2.44m and were checked for vertical and horizontal loading. The side rails are 38mm x140mm boards spanning 2.44m and were checked for horizontal loading. The guard posts are 89mm x89mm spaced at 2.44m and were checked for typical guard rail loads applied at 1070mm above the deck surface in accordance with the BCBC 2012. The posts are bolted to the wheel stop and the outermost stringer with 1/2" carriage bolts, approximately 10" apart.

The following table summarizes the capacities of the individual boardwalk components for the typically constructed section explained above. Demand over capacity ratios are provided with anything below 100% considered to be a safe loading condition.

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Load Rating  
September 10, 2018

**Table 1 - Typically Constructed Boardwalk Section (Based on “Good” Condition Rating)**

Component	Size (mm)		Governing Load Explanation	Demand/Capacity Ratio
	b	d		
Top Guard Rail	38	140	The top guard must be able to withstand the maximum vertical load of a 1.5 kN/m and a maximum horizontal load of 0.75 kN/m line load or a 1 kN concentrated load applied at any point (BCBC 2012, Clause 4.1.5.14).	55%
Side Guard Rail	140	38	Guard component must be able to withstand a 0.5 kN concentrated load applied at any point (BCBC 2012, Clause 4.1.5.14).	101% Acceptable for this application.
Guard Post	89	89	Guard post must be able to withstand the maximum vertical load imposed by top rail and horizontal line load of 0.75 kN/m or a 1 kN concentrated load applied at any point (BCBC 2012, Clause 4.1.5.14).	103% Acceptable for this application.
Deck Board	140	38	Deck board must withstand the maximum of 1.25DL +1.5LL or a concentrated load of 1.3 kN applied at any point (BCBC 2012, Table 4.1.5.9).	99%
Stringer	140	241	Stringers must withstand the maximum of 1.25DL +1.5LL or a concentrated load of 1.3 kN applied at any point (BCBC 2012, Table 4.1.5.9).	71%
Pile Cap	241	241	Pile caps must withstand the maximum of 1.25DL +1.5LL or a concentrated load of 1.3 kN applied at any point (BCBC 2012, Table 4.1.5.9).	62%
Seismic System	-	-	As per BCBC 2012	200% Unsafe for Seismic Loads

As shown in the table above the guardrail system is slightly over capacity when compared to the BCBC 2012 requirements. However, for trail/ marine structures it is common to guardrail structures to be built to a lower standard than that of the BCBC 2012. The demand/ capacity ratios for the guard rails are only slightly in excess of the BCBC 2012 and have been deemed to be acceptable for this application.

As previously stated, there is no information available on the depth or capacity of the existing piles. This means that there is not enough information to complete a load rating for the driven piles. However, it can be assumed that the piles will have adequate axial capacity to support the (relatively light) pedestrian loads.

A lateral/ seismic load calculation was completed for piles with concrete piers located in section 6. In accordance with the BCBC 2012, the seismic lateral load applied to each pile is a fraction of the structure dead weight and 25% of the maximum snow load supported by a single pile. The seismic lateral load will cause an overturning moment at the base of the concrete pier. A local resident involved in the boardwalk construction stated that the concrete piers were cast on top of bedrock with 15M steel cages dowelled into the rock by several centimeters, with the exception of

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Recommendations  
September 10, 2018

concrete piers in section 5 that were cast on top of rocky beach material. During the site review, it was observed that a concrete pier in one location was severely undermined and no dowelled reinforcement was visible. More information would be required to determine the approximate resistances provided by the dowels; however, the pullout resistance of rebar dowelled only several centimeters into bedrock is not considered adequate to resist the overturning moments of the structure. Based on the limited information available, it is assumed that where the dowels exist they will provide negligible resistance. Consequently, the only resistance the pile has against seismic forces is the dead weight of the concrete pier and structure supported by the pile. It was found that the piles with concrete piers (without severe undermining or significant cracking) are only capable of resisting approximately 50% of the seismic loads imposed on the structure. This results in a demand over capacity ratio of approximately 200%, which would be considered unsafe for current seismic loads.

The driven piles are likely to have a greater resistance to lateral/ seismic loads. However, as the depth of these piles is unknown, a load rating was not possible for these piles.

## 4.0 RECOMMENDATIONS

### 4.1 SECTION 1 AND 2: IN-USE BOARDWALK

The in-use sections of the Harbour Walk were found to be in fair to good condition. It is recommended that any new sections of the Harbour Walk be construction with expansion joints at the locations where they tie into any existing sections. This will allow future replacement of these older sections without negatively impacting the newer construction.

Section 2 of the boardwalk (the Fishery) had several issues that are recommended to be addressed in the next 2 years:

- Adjust rip-rap so that boulders do not lean against the supporting piles.
- Replace/ add timber bracing at east (water) side.
- Epoxy-fill cavity in pile at tide line (see image in **Appendix A**).
- Replace pile cap at north-end of boardwalk.
- Replace deck boards at north-end of boardwalk.
- Monitor leaning pile at south-west end of boardwalk.

The scope of this assessment did not include a detailed assessment of the Fishery boardwalk. For marine structures detailed assessment are typically recommended at 5-year intervals. If a detailed assessment is completed for the Fishery, it is recommended to include:

- Review of the two covered structures supported on the board walk, including how these are anchored to the structure below.

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Recommendations  
September 10, 2018

- Review/ load rating of the Jib crane at the north end of the boardwalk.
- Load rating of the boardwalk structure (if it is desired that the boardwalk be used for storage / vehicle loads).
- Lateral/ seismic assessment of the boardwalk structure.

### 4.2 SECTION 3 AND 4: STAND-ALONE TIMBER PILES

The stand-alone timber piles in sections 3 and 4 were found to be in poor condition. If the piles were to be re-purposed, there would be considerable constraints on the boardwalk construction. The size of piles, location, spacing and top of pile height are not uniform which would limit the options for new construction and the alignment of the structure. The exposed ends of the piles have not been protected since installation. If the piles were to be re-purposed, the ends of the piles would have to be cut-back to a location where solid timber is found. It is anticipated that up to 1.2m of the pile would need to be cut-off, which would complicate any new construction further.

Even if successfully re-purposed, these piles may only have another 15-20 years of useful life before a full replacement would be required. It is anticipated that re-purposing the stand-alone piles would not be cost effective.

### 4.3 SECTION 5, 6 AND 7: OUT-OF-USE BOARDWALK

The out-of-use sections of boardwalk appear to have been initially built to a reasonable standard at the time of construction. However, due to lack of maintenance, these sections have fallen into disrepair. A full replacement of topside components (deck and guardrails) is required to make these sections safe for the public. Once the deck is removed a future detailed review of the stringers, pile caps and piles is recommended. For the piles supported on concrete piers (sections 5 and 6), an additional concrete repair would be required. This requires removing any deteriorated concrete, dowelling reinforcing bars to the bedrock and casting new concrete around the existing base of concrete. The table below provides preliminary estimate percentages of boardwalk components that would require repair or replacement within the given timelines.

**Table 2 - Anticipated timeline for repair / replacement of key boardwalk components**

	< 2 years	2-5 years	5-10 years	10+ years
<b>Sections 5 and 6</b>				
Decking	100%	-	-	-
Guardrails	100%	-	-	-
Stringers	20%	100%	-	-
Pile Caps	15%	50%	100%	-
Piles	10%	25%	75%	100%
Concrete Piers	100%	-	-	-
<b>Section 7</b>				
Decking	100%	-	-	-
Guardrails	100%	-	-	-
Stringers	20%	100%	-	-
Pile Caps	15%	50%	100%	-
Piles	10%	25%	75%	100%

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Recommendations  
September 10, 2018

Re-purposing these out-of-use sections is feasible; however, there are several draw-backs which include:

- The alignment and width of the new Harbour Walk would be constrained by the existing construction.
- The estimated “useable life” of the re-purposed structure will be less than that of a new boardwalk structure, as key components are approximately 30 years old. Estimate of up to 30 years remaining life versus a 50-60 year life for new timber construction.

A cost/ benefit analysis may find that it is not cost-effective to repair/ re-purpose the existing structure.

### 4.4 ALIGNMENT OF HARBOUR WALK

The layout/ alignment of the proposed Harbour Walk was outside of the scope-of-work for this assessment. However, an area of concern was noted at the north end of Section 3 where the new boardwalk would need to cross a privately-owned aluminum gangway.



**Figure 9 - Privately-Owned Aluminum Gangway**

It is recommended that any future alignment of the Harbour Walk consider passing under the aluminum gangway to avoid combining the two structures. A preliminary review of the site identified a location where this should be feasible, which has been identified in **Figure 9** above.

# PICTURE APPENDIX

## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018

### Appendix A ASSESSMENT PHOTOS



Image 1 – Section 1: Typical boardwalk construction in Rotary Park



**GANGES HARBOUR WALK STRUCTURAL ASSESSMENT**

Appendix A Assessment Photos  
September 10, 2018



**Image 2 – Section 1: Typical boardwalk construction in Rotary Park**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 3 – Section 2: Fishery boardwalk section pictured from gangway looking north**



**Image 4 – Section 2: Fishery boardwalk section pictured from below looking south**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 5 – Section 2: Visible cavity and heavy marine growth on pile and brace**



**Image 6 – Section 2: Rip-rap leaning on pile**



**GANGES HARBOUR WALK STRUCTURAL ASSESSMENT**

Appendix A Assessment Photos  
September 10, 2018



**Image 7 – Section 3: Piles (north of Fishery)**



**Image 8 – Section 3: Pile with rot and vegetation at top**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 9 – Section 4: Inaccessible stand-alone piles with no top of pile protection**



**Image 10 - Section 5: Inaccessible boardwalk in visibly poor condition viewed from road**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 11 - Section 5: Boardwalk in visibly very poor condition**



**GANGES HARBOUR WALK STRUCTURAL ASSESSMENT**

Appendix A - Assessment Photos  
September 10, 2018



**Image 12 - Stair access from Lower Ganges Road. Guard-rails and stair treads need maintenance/ replacement**



**Image 13 - Stair access pile cap with signs of rot at sawn-off end**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 14 - Section 6: Typical construction on concrete piers. Guardrail post showing significant split at bolt holes**



**Image 15 - Section 6: Concrete pier in very poor condition with severe cracks in concrete**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 16 – Section 6: Concrete pier showing exposed rebar at top and concrete undermining at bottom. Unlikely that pier is anchored to existing rock.**



**Image 17 – Section 6: Pile cap showing minor signs of rot**



## GANGES HARBOUR WALK STRUCTURAL ASSESSMENT

Appendix A Assessment Photos  
September 10, 2018



**Image 18 – Section 7: Boardwalk showing very poor condition of deck and top rail of guard**



**Image 19 – Section 7: Boardwalk and beach access 2 showing concrete undermining**



**GANGES HARBOUR WALK STRUCTURAL ASSESSMENT**

Appendix A Assessment Photos  
September 10, 2018



**Image 20 – Section 7: Boardwalk lookout showing very poor condition of guardrail and decking**

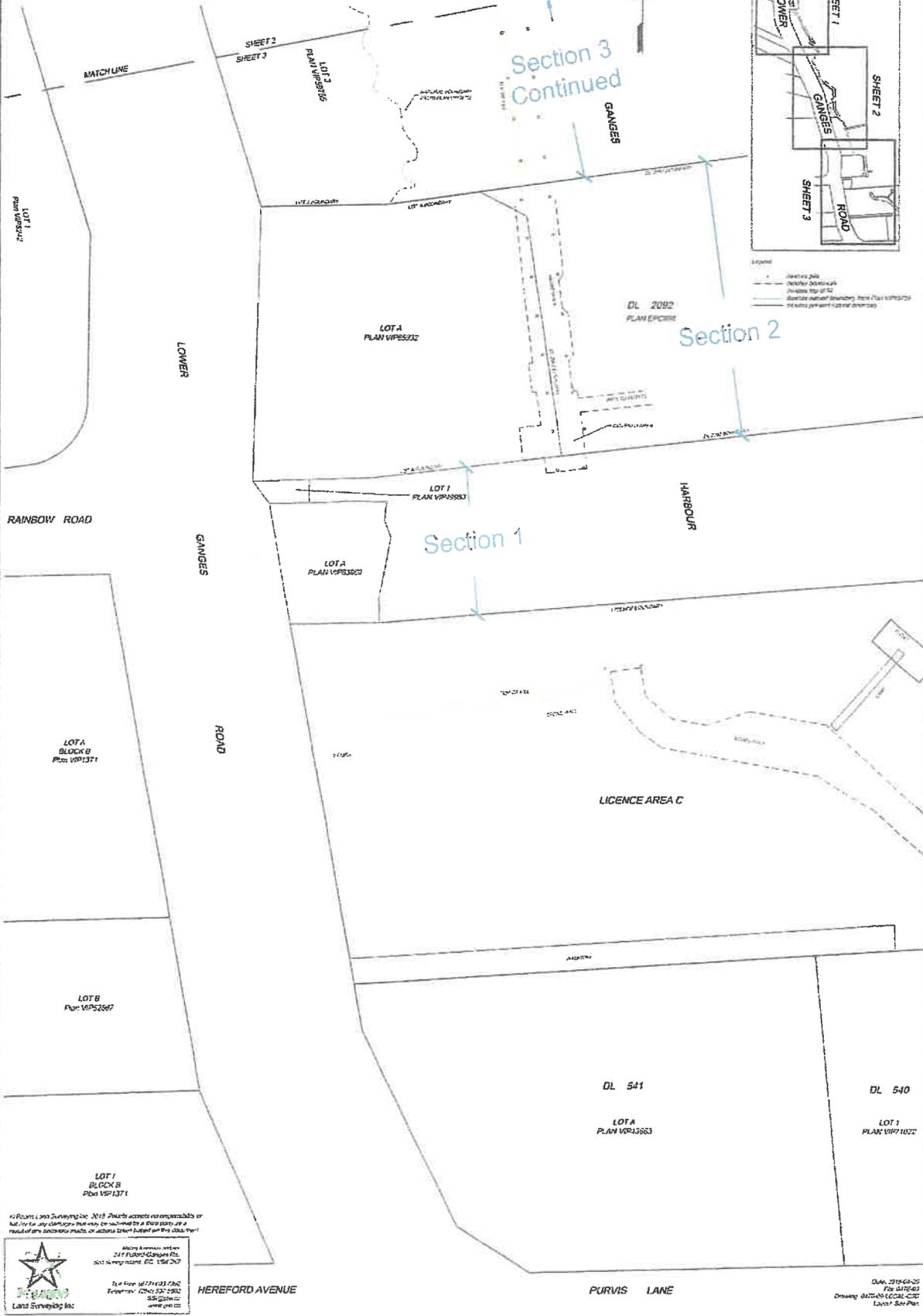


September 10, 2018

## **Appendix B POLARIS LAND SURVEY**

AS DIMENSIONS ARE IN METERS AND DECIMALS THEREOF, UNLESS OTHERWISE NOTED.  
 The horizontal plan scale of this plan is 1:2500  
 It is shown by the dimensions in meters (M) and centimeters (CM) when required at a scale of 1:200.

Key Plan  
 Scale 1:2500



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 Salt Spring Island  
 www.ssi.ca

DL 541  
 LOT A  
 PLAN VP52600  
 DL 540  
 LOT 1  
 PLAN VP52601





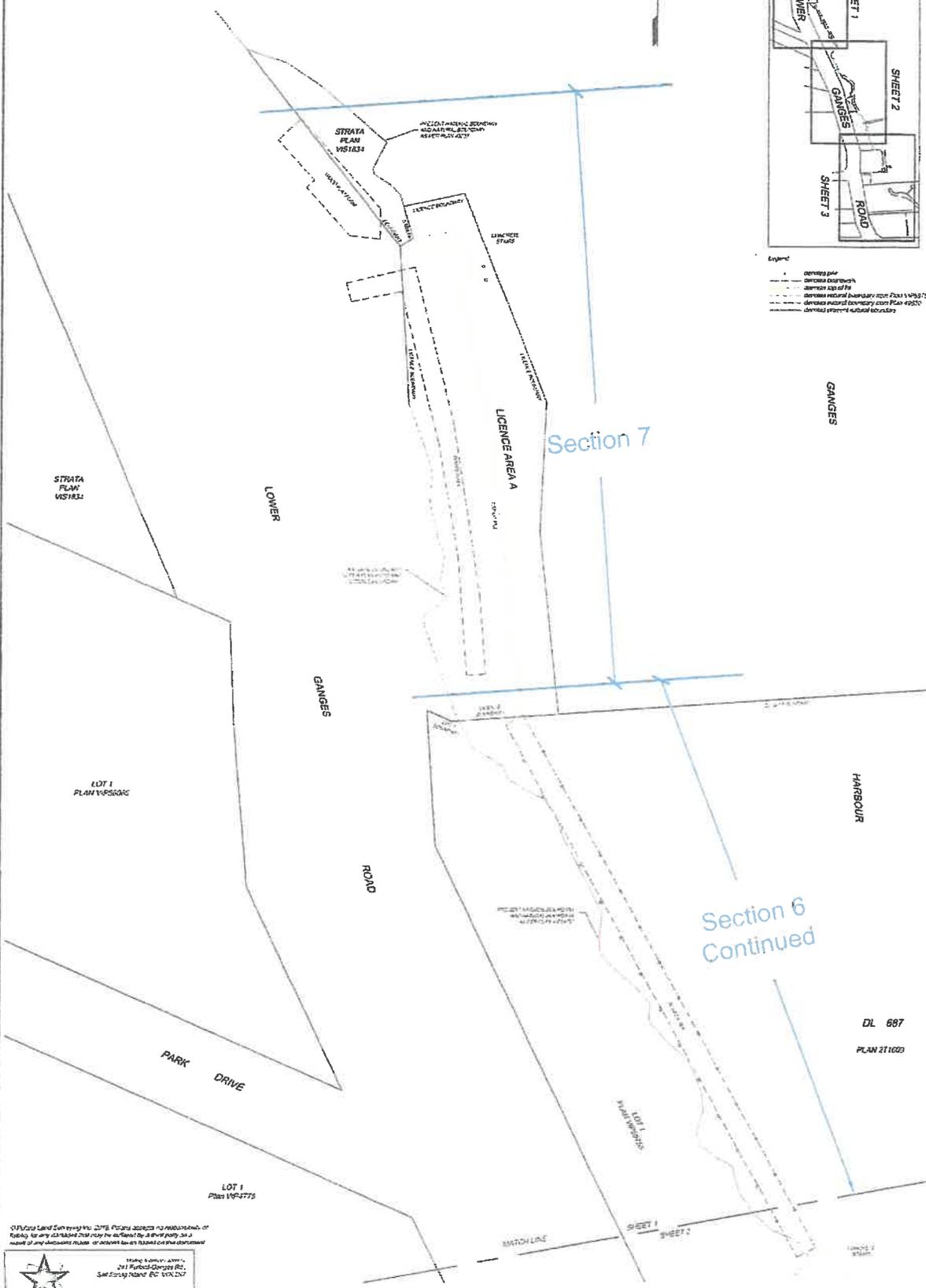
All dimensions are in meters, and decimal fractions are to be used where noted.  
The vertical grid lines of this plan are 250m on a grid by Station 10 (height 10-100) when plotted at a scale of 1:200.

Key Plan  
Scale: 1:2500



Legend

- - - - - proposed plan
- - - - - proposed boundary
- - - - - proposed lot of 10
- - - - - proposed natural boundary from Plan 1/19/07/25
- - - - - proposed natural boundary from Plan 4/9/05/25
- - - - - defined natural boundary



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www.psa.ca

DL 687  
PLAN 211003

Rev: 2018-06-20  
For: 03/01/02  
Drawing: CA/PS/LODM, CID  
Location: SPS PLAN



Salt Spring Island Electoral Area  
108 – 121 McPhillips Avenue  
Salt Spring Island, BC, V8K 2T6

T: 250.537.4448  
www.crd.bc.ca

June 5, 2018

Via email: FLNR [Minister@gov.bc.ca](mailto:Minister@gov.bc.ca)

The Honourable Doug Donaldson  
Minister of Forests, Lands, Natural Resources Operations and Rural Development  
PO Box 9049  
Victoria, BC SW 9E2

Dear Minister,

Re.: Ganges Harbourwalk, Salt Spring Island

As the Electoral Area Director for Salt Spring Island and a member of the Capital Regional District Board I am writing to you in support of the May 16, 2018 letter (attached) to you from Peter Luckham, Chair of the Salt Spring Island Local Trust Committee.

While we have differing roles, the Islands Trust and the Capital Regional District work together in the best interests of our community to ensure we collectively address priorities on a number of matters, the Harbourwalk being one. Your support will reflect the community's strong interest in the Harbourwalk and provide stability for the CRD when the project moves to the design and construction phase; which will proceed subject to the completion and the results of the CRD infrastructure reviews. We expect these reviews will be completed by the end of the summer.

This design and construction stage will also provide a planning base for the CRD to work with the upland property owners on items of mutual interest. This will also involve the Islands Trust as well as to have an opportunity for community input. The goal will be for all parties to work together to evaluate and implement enhancements to our downtown seaside to the benefit of both residents and visitors.

Yours truly,

Wayne McIntyre, CRD Director  
Salt Spring Island Electoral Area

Attached: Letter May 16, 2018, Peter Luckham, Ganges Harbourwalk

cc: Peter Luckham, Chair Salt Spring Island Local Trust Committee  
The Honourable Selina Robinson, MLA, Minister of Municipal Affairs and Housing  
Adam Olsen, MLA, Saanich North and the Islands  
Elizabeth May, MP, Saanich and the Gulf Islands  
Salt Spring Island Local Trust Committee



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May 16, 2018

File: 6500-20 – Ganges Village Area Planning -  
Harbourwalk

via email: [FLNR.Minister@gov.bc.ca](mailto:FLNR.Minister@gov.bc.ca)

The Honourable Doug Donaldson  
Minister of Forests, Lands, Natural Resource Operations and Rural Development  
PO Box 9049 Stn Prov Govt  
Victoria, BC V8W 9E2

Dear Minister,

Re: Ganges Harbourwalk, Salt Spring Island

On behalf of the Salt Spring Island Local Trust Committee (LTC), I am writing to seek your support in helping the Salt Spring Island community achieve its longstanding vision of a harbour boardwalk in Ganges.

Our staff will soon submit an application to the Ministry of Forests, Lands, Natural Resource Operations and Rural Development (FLNRORD) for a statutory right-of-way (SRW) along a portion of the foreshore in Ganges Harbour to allow completion of a critical segment of the Ganges Harbourwalk (see page 3 for approximate SRW route). In the late 1980s a community-led boardwalk was constructed within the area of Islands Trust's forthcoming SRW application, but it fell into disrepair. Much of the original infrastructure remains in place, and the Capital Regional District (CRD) is currently undertaking a structural assessment to determine whether it can be refurbished and used anew. The results of that assessment should be made public shortly.

At the same time, FLNRORD staff are currently considering renewal of a Crown tenure for the Ganges Marina that includes within its boundaries the area proposed for harbourwalk development. For the past 30 years, the old boardwalk infrastructure has been included in the Ganges Marina tenure, with the marina operators assuming liability for it. The LTC is proposing that the general path of the original boardwalk be excluded from the marina's tenure area and instead kept in public hands. While today's overlapping local government and private sector interest in the same water area may appear to present a conflict, the LTC is confident that the harbourwalk and marina can co-exist, and indeed be of mutual benefit. Thoughtful wording within any agreements reached between FLNRORD, Islands Trust and the Ganges Marina operators can ensure that all agencies, as well as the Ganges Marina operators, can achieve their respective objectives.

The LTC respectfully requests that the Minister encourage FLNRORD staff to forego renewal of the Ganges Marina tenure until they have received and assessed an application from Islands Trust seeking public use of the subject foreshore area, and that said application be considered in light of a longstanding Salt Spring Island Official Community Plan policy to work towards completion of the Ganges public pathway system, of which the harbourwalk is an integral part. It should be noted here that because the *Islands Trust Act* does not permit LTCs to incur liabilities, Islands Trust's SRW application will request that the Crown retain liability for the existing boardwalk infrastructure within the SRW until such time as a detailed plan to advance boardwalk construction and operation is developed.

.../2

Honourable Doug Donaldson  
May 16, 2018  
Page 2

We hope that ministry staff will give due consideration to this request. Islands Trust staff are available to provide you with any necessary information concerning the harbourwalk project to date.

Completing the Ganges Harbourwalk is a matter of public interest for the people of Salt Spring Island and its many visitors. Its development will help our Local Trust Committee fulfil the Trust object, "to preserve and protect the trust area and its unique amenities and environment for the benefit of residents of the trust area and of British Columbia generally . . .".

The Salt Spring Island Local Trust Committee looks forward to working with FLNRORD, the CRD, and all stakeholders to bring this important piece of community infrastructure to fruition.

Yours sincerely,

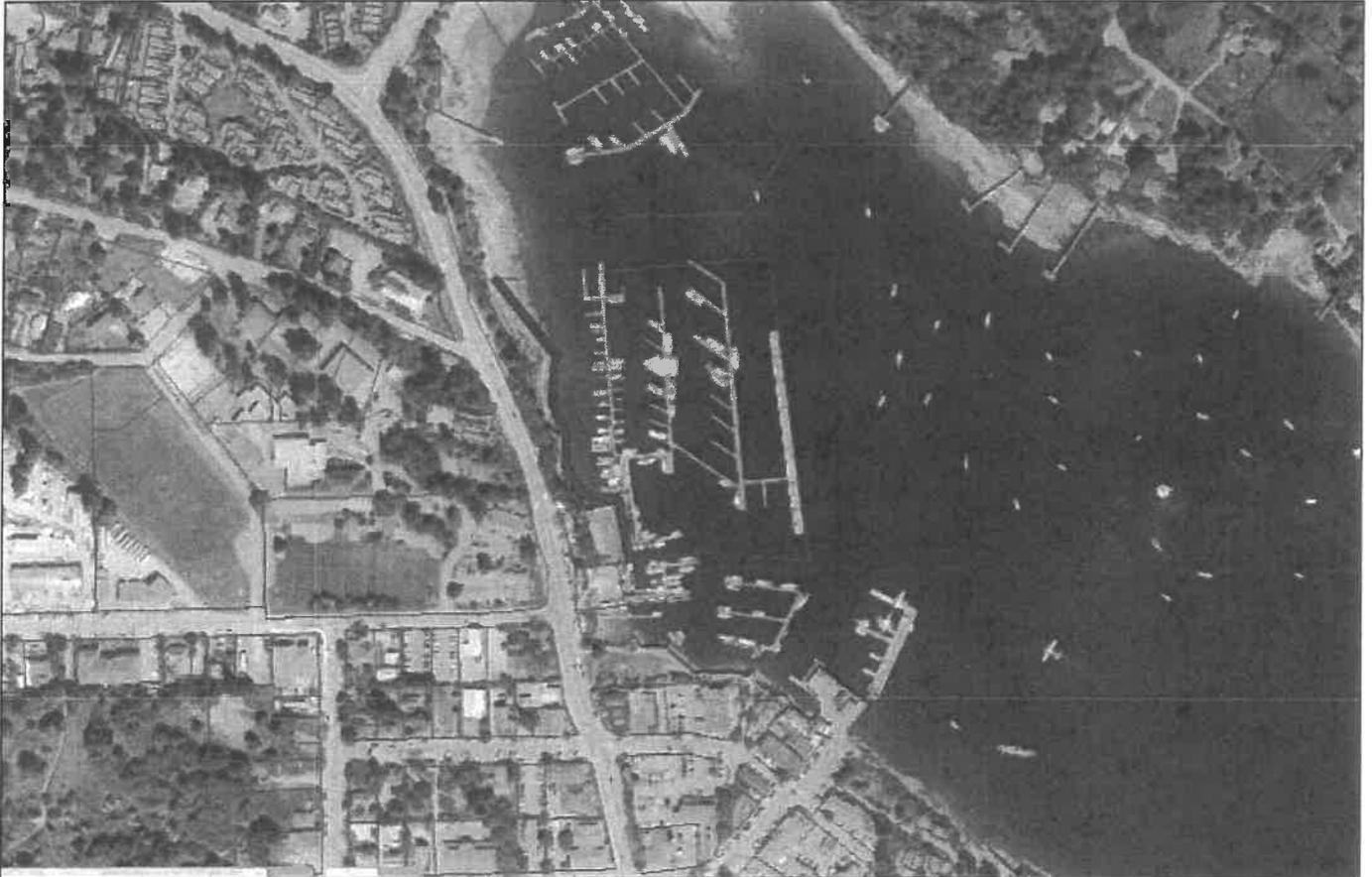


Peter Luckham  
Chair, Salt Spring Island Local Trust Committee

cc: The Honourable Selina Robinson, MLA, Minister of Municipal Affairs and Housing  
Adam Olsen, MLA, Saanich North and the Islands  
Elizabeth May, MP, Saanich and the Gulf Islands  
Wayne McIntyre, CRD Area Director, Salt Spring Island  
Salt Spring Island Local Trust Committee

.../3

**Approximate Location of Subject Harbourwalk Segment**



**Tracey Shaver**

---

**From:** de Bree, Jennifer FLNR:EX <Jennifer.deBree@gov.bc.ca> on behalf of Sutherland, Craig FLNR:EX <Craig.Sutherland@gov.bc.ca>  
**Sent:** Wednesday, July 25, 2018 8:41 AM  
**To:** Tracey Shaver  
**Cc:** Minister, FLNR FLNR:EX; Engels, Simone FLNR:EX  
**Subject:** 240102 McINTYRE re Ganges Harbourwalk Project

Reference: 240102

Wayne McIntyre, CRD Director  
Salt Spring Island Electoral Area  
108 – 121 McPhillips Avenue  
Salt Spring Island, British Columbia  
V8K 2T6

Dear Wayne McIntyre:

Thank you for your letter of June 5, 2018, to Honourable Doug Donaldson, Minister of Forests, Lands, Natural Resource Operations and Rural Development, regarding the Ganges Harbourwalk project. I have been asked to respond.

The ministry supports different levels of government in working towards common goals, such as the creation of community-supported infrastructure, and acknowledges the positive relationship between the Capital Regional District (CRD) and the Islands Trust. In regards to the Islands Trust's letter of May 16, 2018, which you have attached, I recognize the priority the CRD has given to the Ganges Harbourwalk project and assure you that ministry staff will continue to work with the CRD and Islands Trust staff on this project. The infrastructure review is an important first step in the process.

As you have noted, the project may impact the upland property owner's rights, and it is important that all parties work together to address any potential impacts. If an application for a statutory right-of-way for the Ganges Harbourwalk route is submitted as a result of the infrastructure investigation, the ministry's West Coast Authorizations staff will review the application and make a decision. I encourage you to engage in discussions with the upland property and rights holder with the objective of reaching a mutually acceptable arrangement, which would assist in facilitating the timely review of the application.

If you or your staff require any further information, please do not hesitate to contact Simone Engels, Crown Land Authorizations Section Head, by phone at 250 751-7271 or by email at [Simone.Engels@gov.bc.ca](mailto:Simone.Engels@gov.bc.ca).

Again, thank you for taking the time to write and express your views.

Sincerely,

Craig Sutherland  
Assistant Deputy Minister  
Coast Area

pc: Honourable Doug Donaldson, Minister of Forests, Lands, Natural Resource  
Operations and Rural Development

Simone Engels, Crown Land Authorizations Section Head, West Coast  
Natural Resource Region



# MEMORANDUM

File No.: 6500-20 - Ganges Village Area  
Planning - Harbourwalk

DATE OF MEETING: December 7, 2018  
TO: CRD Ganges Harbourwalk Steering Committee  
FROM: Jason Youmans, Island Planner  
Salt Spring Island Team  
COPY: Karla Campbell, CRD General Manager, Salt Spring Island  
Stefan Cermak, Regional Planning Manager, Islands Trust  
Salt Spring Island Local Trust Committee  
SUBJECT: Ganges Harbourwalk – Next Steps

## PURPOSE

The purpose of this memo is twofold. First, it provides the CRD Ganges Harbourwalk Steering Committee with background on actions taken to date by the Salt Spring Island Local Trust Committee (LTC) to advance completion of the Ganges Harbourwalk. Second, it is meant to inform recommendations that the Steering Committee could make to the incoming LTC on how best to advance the project.

## BACKGROUND

Recent work towards completion of the Ganges Harbourwalk has been a priority project of the Salt Spring Island Local Trust Committee (LTC) since 2014. It was identified as a first priority within a larger Ganges Village Planning project. The LTC's role in Harbourwalk development primarily concerns administration of permitted uses (including size, siting, and density) of the land and waters in the vicinity of the Harbourwalk route. Recognizing that a completed Harbourwalk would likely be owned and operated by the Capital Regional District (CRD), it was determined at the outset of this project that a collaborative effort between Islands Trust, the CRD, and other community stakeholders would be required to bring the Harbourwalk to fruition.

Major milestones in the LTC's work on this file to date are as follows<sup>1</sup>:

- Summer 2013: Ganges Village Area Planning added to LTC work program

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<sup>1</sup> Islands Trust staff recognize the significant work undertaken by the CRD director, staff and volunteers, as well as community members in helping revive and advance the Ganges Harbourwalk project. These actions include early lobbying to both the LTC and CRD director to take on the project, as well as the CRD's formation of the Ganges Harbourwalk Steering Committee and the allocation of \$150,000 to undertake engineering and other studies of the existing Harbourwalk infrastructure.

- Summer 2014: Stakeholder meetings to develop Harbourwalk strategy. LTC awards contract to Whitepine Consulting for preliminary Harbourwalk design concepts (see Appendix 1). LTC awards contract to Fletcher Municipal Consulting to develop project charter for Harbourwalk project.
- Winter 2014: LTC adopts Harbourwalk project charter and directs staff to undertake community and First Nations engagement.
- Spring 2015: LTC receives report providing detailed information about properties adjacent to Harbourwalk route, as well as landowner development objectives.
- Winter 2016: LTC hosts two-day Harbourwalk visioning and boardwalk design charrette. Consultant's report attached here as Appendix 2.
- Spring 2016: LTC develops and considers Bylaw 491, a land use bylaw amendment that would rezone waterfront properties adjacent to the Harbourwalk route in anticipation of Harbourwalk development (see Appendix 3). Directs staff to consult upland owners, community agencies, and First Nations about Bylaw 491.
- Fall 2017: LTC hosts community consultation and upland design charrette on Bylaw 491. Consultant's report attached here as Appendix 4.
- Spring 2018: LTC writes to Minister of Forests, Lands, Natural Resource Operations and Rural Development advising of Salt Spring community's desire to see renewal of Ganges Marina water lease made contingent upon securing the Harbourwalk route for public use (see Appendix 5).
- Summer 2018: LTC submits statutory right-of-way application to Ministry of Forests, Lands, Natural Resource Operations and Rural Development to secure Harbourwalk route adjacent to Ganges Marina-owned lands.

Visit the LTC's Ganges Harbourwalk website [here](#) to see all project reports received by the LTC to date.

## ISSUES

Since 2016, LTC work on the Harbourwalk file has focussed on developing land use bylaw amendments that would rezone the properties adjacent to the Harbourwalk route for additional commercial/accommodation uses in a way consistent with the Official Community Plan and that would help create a vibrant pedestrian experience. However, this approach has proven to face several challenges:

### 1) North Salt Spring Water District moratorium on new/expanded connections

The ongoing North Salt Spring Water District moratorium is an impediment to increasing the density or intensity of uses on the properties and water area adjacent to the Harbourwalk route.

Any land use bylaw amendment undertaken by the LTC must be consistent with Policy 4.4.2 of the Islands Trust Policy Statement which states, "Local Trust Committees and Island Municipalities shall, in their official community plans and regulatory bylaws, address measures that ensure neither the density nor intensity of land use is increased in areas which are known to have a problem with the quality or quantity of the supply of freshwater, water quality is maintained, and existing, anticipated and seasonal demands for water are considered and allowed for."

As the upland owners have no immediate plans to redevelop their sites, there is no imperative for them to provide an alternative potable water servicing plan. As such, it is unclear how potable water would be proven as required by Policy 4.4.2 above.

## 2) First Nations concerns

The filled foreshore in front of both Ganges Marina and the “Fishery” building, where any new development would, in part, occur is considered water under both the Salt Spring Island Land Use Bylaw and Salt Spring Island Official Community Plan. In the case of the Ganges Marina, this filled foreshore area is not owned by the marina, but rather by the provincial government. Turning this water to land, from a regulatory standpoint, requires amending the OCP. Amending the OCP, in turn, requires sign-off from the Minister of Municipal Affairs and Housing who will be looking, first and foremost, to ensure that First Nations with an interest in the subject area have been adequately consulted and, where possible, accommodated.

To date, First Nations that responded to the earlier referral of proposed Bylaw 491 have rejected plans for Harbourwalk completion that involve a new waterfront hotel or residential construction and that will further strain the Island’s resources. Failure to secure First Nations support for the necessary OCP amendment will make it challenging to secure ministerial sign-off on the bylaw. In a similar vein, in order for any development to occur on the filled foreshore, the Ganges Marina would have to buy it from the provincial government which, again, would look to consult First Nations before making a decision.

## 3) Community Vision

The message that emerged from the well-attended November 2017 community consultation meeting was clear. Those in attendance did not support significant development of the Ganges Harbour shoreline as the price of Harbourwalk completion. Further work toward this end, or a new community planning process predicated on further upland development, risks arriving at the same conclusion. See Appendix 4 for community consultation report.

Based on the above-noted issues, among others, the LTC will have to consider what levers are available to help bring the Ganges Harbourwalk to fruition, particularly as the LTC’s authority is limited to controlling permitted uses on the lands and waters in the area of the Harbourwalk route. Development of the Ganges Harbourwalk will benefit the upland owners adjacent to it, and as such Islands Trust staff suggest that any OCP or land use bylaw amendments made in its pursuit should serve to fulfill the greater objectives of the Official Community Plan.

## **NEXT STEPS**

Next steps in advancing the Ganges Harbourwalk project at the LTC table are contingent on the LTC maintaining the Harbourwalk as a priority project. With the first LTC meeting of the new term scheduled for December 6, political priorities are subject to change. Assuming the Harbourwalk is maintained as a top priority project, the

Steering Committee could consider providing strategic direction in the form of recommendations to the LTC around the following types of actions, or variations thereof.

#### Statutory Right-of-Way

The LTC has submitted a statutory right-of-way application to the Ministry of Forests, Lands, Natural Resource Operations and Rural Development (FLNRORD) to assert public control of the water area parallel to Ganges Marina's properties, approximate to the route of the existing boardwalk. In November of this year, staff received correspondence from FLNRORD requesting clarification and additional information to supplement the original SRW application. Staff are in the process of responding to this request, however the absence of a proposed boardwalk design and project timelines will limit staff's ability to provide the level of detail being requested by FLNRORD. CRD staff have indicated they do not plan to undertake further work on this file without confidence that an SRW can be secured.

The LTC could:

- Take no further action until a decision is rendered from FLNRORD regarding consideration of the SRW; or
- Continue to advance the project through the types of options below

#### Land Owner Negotiation/Incentive

In a letter received in follow-up to the LTC's letter to the FLNRORD minister, FLNRORD Assistant Deputy Minister Craig Sutherland wrote "West Coast Authorizations staff are aware of Islands Trust's intent to apply for a statutory right of way for the Harbourwalk route, and will support the review of this application once it is received. At the same time, I would like to advise and encourage you to initiate and engage in discussions with Ganges Marina, the upland property and rights holder, with the objective to reach a mutually acceptable arrangement" (see Appendix 6).

Further discussions with the upland land owner could take a range of forms:

- Request the upland owner's support for the LTC's SRW application in exchange for the LTC's support of the upland owner's marina tenure renewal; or
- Discuss options for increasing density or range of uses both/either upland or on the water surface; or
- Other as-yet undetermined options

Such discussions could be undertaken by Islands Trust staff or a hired negotiator who could then in turn make recommendations to the LTC.

#### Advance/Amend Bylaw 491

Bylaw 491 was given first reading by the LTC in March 2016. It has advanced no further following the November 2017 community consultation event. The bylaw remains available to realize a form of development in Ganges Harbour that is generally consistent with the OCP and that may provide an incentive for the upland owners to actively participate in the Harbourwalk project.

The LTC could:

- Direct staff to investigate how to address some of the issues associated with further development under Bylaw 491 (absence of NSSWD water connection, etc); or
- Advance Bylaw 491; or
- Amend Bylaw 491

### Community Visioning

The Ganges Marina owners have stated they do not want to commit resources to assessing land use scenarios without confidence that the community – via the LTC – has endorsed a particular vision for Ganges Harbourfront development.

The LTC could lead, or be part of, further community and land use planning for the harbourfront. Such a planning process could include only those properties between Rotary Park and Peck’s Cove Park, or it could be broader, encompassing the Ganges waterfront as a whole, or be pulled back further still to form part of a comprehensive Ganges Village planning exercise.

Such planning could be led by staff, an appointed advisory planning commission, or hired consultants.

The vision that emerges from such a planning project could be used to inform amendments to the OCP that “set the table” for future commercial development of the properties upland of the Harbourwalk route. Examples of the types of policy changes that could be made include:

- Recognizing the filled foreshore as land, rather than water, within the OCP
- Establishing general policies concerning future development in the subject area (height limits, density limits, types of uses, etc).
- Establishing policies outlining expectations of land owner contributions toward Harbourwalk construction and operations

As noted above, further community planning may help cement a vision for type, density and intensity of land uses in the harbour area, but it will not address the challenge posed by the NSSWD’s ongoing moratorium.

### **CONCLUSION**

There are a range of options available to the LTC to help advance the Ganges Harbourwalk project. However, there is a measure of uncertainty associated with all of them. The LTC will benefit from the strategic advice of the Ganges Harbourwalk Steering Committee.

Submitted By:	Jason Youmans, Island Planner	November 20, 2018
Concurrence:	Stefan Cermak, Regional Planning Manager	November 20, 2018

## **ATTACHMENTS**

1. Ganges Harbourwalk – Preliminary Design Concepts
2. Harbourwalk Visioning Community Consultation Report, Islands Planning Services
3. Bylaw 491
4. Ganges Harbourwalk Upland Community Consultation Report, Dillon Consulting Limited
5. LTC letter to Minister of Forests, Lands, Natural Resource Operations and Rural Development (FLNRORD)
6. FLNRORD ADM letter to LTC



What's YOUR vision for the Ganges Boardwalk?



# GANGES BOARDWALK

Community Consultation Report

## ABSTRACT

Reflecting the discussions and ideas of the Salt Spring Island community with regard to their hopes and dreams for the completion of the Ganges Harbour Walk.

Beverly Suderman, RPP, MCIP  
IPS Island Planning Services

February 4, 2016

# GANGES BOARDWALK COMPLETION:

## COMMUNITY CONSULTATION REPORT

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## EXECUTIVE SUMMARY

Community consultation meetings were undertaken on January 27 and January 30, 2016. The purpose of these meetings was to gather ideas and perspectives from a broad diversity of the Salt Spring Island community for the design of the Ganges Boardwalk, and to create a vision for the place. Both of these meetings were structured as Special Meetings of the Local Trust Committee, which permitted full trustee participation without potential procedural issues.

A key result of the meetings is that the community as a whole is strongly supportive of the completion of the “boardwalk,” which came to be known as a “Harbour Walk” over the course of the meetings. Ideas ranged from the immediately practical, i.e. clean up the harbour, to the most visionary, i.e. “provide for the intersections and multidimensionality of the human spirit and the human experience.” It is important to note that some citizens are concerned that they need to know the cost of the project and how it will be funded before they can be fully supportive.

The design of the Harbour Walk will need to provide opportunities for a broad range of activities: education, building relationships with First Nations, art, music, sport, transportation, food and wine (commercial), as well as resting places for its users. There was a strong suggestion from both the First Nations speaker and the biologist that moving the boardwalk away from the shoreline would address both archaeological and ecological concerns. It would also provide the user of the Harbour Walk with the feeling of being on the water.

The meetings also highlighted:

- the need for better stewardship of the harbour: addressing debris in the harbour waters, as well as derelict vessels; and
- the need to address issues related to zoning and statutory rights-of-way before spending a lot more time and energy in pursuing other, more exciting aspects of design.

There appears to be a dynamic tension within the community between the desire to see the project finished as quickly as possible and the desire to create something that is uniquely “Salt Spring” that reflects the community and meets its needs. One suggestion from Saturday’s workshop for bridging these tensions is to ensure that the project is scale-able. In other words:

- If not everything can be done at once, design in a way that additions can be phased in over time;
- Think of the area from Rotary Park to Peck’s Cove as a first stage, but find a way to move beyond to better connect downtown to the beach near Moby’s Pub and to the trail system beyond;

While participants seemed eager to think beyond the scope presented during the workshops, it will be important for the Trustees to be able to demonstrate progress on these initial stages of the project to maintain momentum for any future work.

## INTRODUCTION

Two meetings were held at the Hart Bradley Memorial Hall (Lions Club), on January 27 and January 30, 2016: a fact-finding meeting related to community concerns about the project, and a community visioning meeting. Both meetings were structured as Special Meetings of the Salt Spring Island Local Trust Committee. Official minutes are presented separately, according to the standard process.

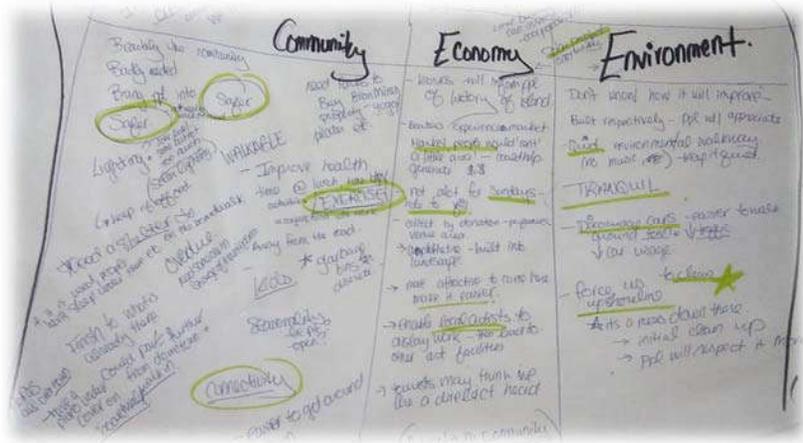
## A NOTE ABOUT MONEY

Please note that both meetings deliberately avoided addressing cost calculations at this stage of the process. While recognizing the importance of money to actually accomplishing the project, there are two perspectives to be highlighted here:

1. Once the key design elements have been identified by the community, and prioritized, then choices can be made to minimize costs, or phase them over time, i.e. types of decking, length, amount of art.
2. The project champions, particularly in the private sector, felt that if the vision for the project was compelling enough, the money would come: through grant funding, through private fundraising, and through other means.

Several community members expressed discomfort with this approach. They didn't feel that they could make a decision as to whether or not they support a boardwalk project without

knowing how much it will cost. This is fair, and their views are respected and understood. Discussions about money will take place very soon, in the next stage of the project.



## FACT-FINDING MEETING: JANUARY 27, 2016

This meeting was designed as a fact-finding meeting for the trustees alongside the community. In addition to the Local Trust Committee members, Islands Trust staff, consultants and presenters, approximately 50 people attended this event, which started at 4:30 pm and adjourned at about 6:45 pm.

## PRESENTATIONS

The substantive portion of the meeting was hosted by Matt Steffich, Vice-President of the Salt Spring Island Chamber of Commerce. Speakers included:

- Kathleen Johnnie, Land and Resources Coordinator for the Lyackson First Nation

- Nancy Krieg, Concept Design and Master Planner, Whitepine Consulting
- Kathy Reimer, Biologist, Salt Spring Salmon Enhancement Society
- Karla Campbell, Senior Manager for Salt Spring Island, Capital Regional District
- Chris Hall, Planner, IPS Island Planning Services

It had been hoped that Corey Johnson of Island Marine Construction Services would be able to participate to bring a perspective from engineering and construction. His observations were relayed by Matt Steffich to the group, and can be summarized as follows:

- The underlying structure of the existing boardwalk is in very good shape, and can be modified as desired to accommodate a variety of uses and features;
- The decking needs replacement.

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#### SUMMARY: FIRST NATIONS VALUES AND INTERESTS

- Salt Spring Island is a very special place
- Rich history of First Nations use of Salt Spring Island
  - To be maintained alongside settler history
  - Inland portions of the island important to First Nations, as well as foreshore areas
  - Lots of medicine and spiritual places
  - Some information is confidential, and cannot be shared broadly
  - Development has eroded the archaeological record – this has to stop
- Goal is to maintain (protect and preserve) cultural values on the Island
  - Archaeology:
    - Protect archaeological sites
    - Work with archaeologists knowledgeable about Coast Salish culture
    - Work with First Nations knowledge keepers alongside the archaeologists
  - Reflect First Nations culture in the seawalk design
    - Use of Hul'qumi'num names (for places, plants, or other interpretation) along the boardwalk
    - If structures, then reflect Coast Salish architecture, i.e. long house style (instead of pergolas, for example)

## First Nations Architecture

The Coast Salish house is typified by a single-pitch shed roof over horizontal plank walls which are situated parallel to the waterfront. The higher side of the pitched roof is either facing the water, or opposite the prevailing winds in the village.

Salish houses are probably the most flexible of all the Northwest Coast housing, in that they are composed of linear post and beam modules, and can be added onto at both ends of the house whenever space is required for a new household related to the father's or mother's line. Therefore, Salish houses can be extremely long and narrow, in which case the interior apartments are spread out along both long walls, with a central aisle in between.

Coast Salish people attach large carved planks to the inside or outside of Big Houses or carve the large posts that form the structure of Big Houses.

Salish house boards and posts depict mythical creatures associated with family history, notable ancestors, events which displayed ancestors' spirit powers, or magical privileges of the family. They faced into the large winter houses, declaiming the long history, wealth and high status of the family. (Source: Bill Reid Centre, SFU: [https://www.sfu.ca/brc/art\\_architecture/nw\\_coas](https://www.sfu.ca/brc/art_architecture/nw_coas))

- Create story spaces (Parks Canada example)
- Create opportunities for First Nations to sell crafts

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## SUMMARY: DESIGN CONSIDERATIONS

A successful design must:

- Be responsible: environmentally, culturally, and fiscally
- Be responsive: OCP, public, and adjacent landowners
- Be intrinsic to Salt Spring: use local vernacular, show the essence of the community, be an example
- Instill community pride

Design considerations include:

- Site attributes and challenges: natural, ecological, historic
- Accessibility and linkages must be addressed
- Provide experiences for the user: as a pathway and as a destination
- Address the needs of key stakeholders
- Address maintenance and security considerations

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## SUMMARY: ECOLOGICAL VALUES AND INTERESTS

- Shading of the sea floor:
  - With a north-south orientation, shading is generally not an issue
- Riparian vegetation along the shoreline should be protected
  - Stabilizes the slope
  - Filters road runoff, so the water flowing from the road to the harbour is cleaner
  - Provides habitat
- Whole harbour is herring and clam habitat
- No endangered or rare species have been identified in the area of the proposed boardwalk
- Design ideas to improve ecological outcomes:
  - Steel pilings are better than creosoted pilings
    - Provide surface for revegetation
  - "Seawalk" an improvement over "boardwalk" if further from the riparian area
    - Doesn't change slope of bank
    - Doesn't block wildlife access

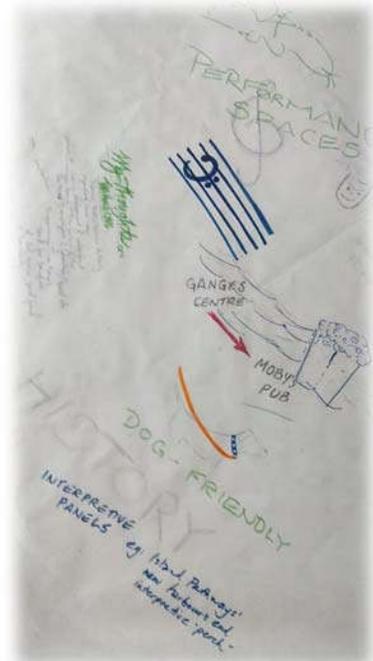



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## SUMMARY: LAND USE CONSIDERATIONS

- Community support is critical to success of the project.
- The existing policy framework supports completion of the boardwalk

- The OCP encourages a wide range of uses for the area
- Tangible and ongoing action is required to maintain public and landowner confidence
- Property owners are willing to participate as long as it does not compromise their land’s potential for other uses, but they cannot commit funds or grant consent for the boardwalk to pass over their lands without assurances that it makes business sense.
- Further work is required on:
  - Riparian access agreements
  - Fill requirements
  - Site development parameters (setback, height, density, parking, etc.)
  - Amenity contribution calculation.



**SUMMARY: PROCESS AND IMPLEMENTATION CONSIDERATIONS**

- The Capital Regional District (CRD) is willing to assume responsibility for liability and operations and maintenance of the boardwalk once it is completed.
- The CRD and Islands Trust will work together jointly to get this project built.
  - The Islands Trust takes the lead on the visioning and concept development, as well as negotiations with land owners regarding zoning changes;
  - A joint CRD/Islands Trust advisory committee or task force will be established;
  - The CRD will assume the lead for the project at the design and feasibility stage of the project. (A tentative project process is attached as Appendix A.)

**COMMUNITY VISIONING MEETING, JANUARY 30, 2016**

This meeting was designed as a visioning session for the trustees alongside the community. In addition to the Local Trust Committee members, Islands Trust staff, consultants and community volunteers, approximately 50 people attended this event, which started at 1:00 pm and adjourned at about 4:00 pm.

The event started with a brief presentation by Nancy Krieg regarding design considerations, similar to the presentation on Wednesday evening. This presentation emphasized that the boardwalk or seawalk:

- Is a connector: mentally, emotionally, and physically
- Is a destination in its own right, as well as connecting people and places



- Has the potential to be iconic for the island, representing the uniqueness and the specialness of Salt Spring Island
- Is an experience, which can be intensive and active or passive, based on the rhythm of activities built in
- Will tell a story, and send a message, about the vision of the people who live here about their centre place.

Reports from various community groups that had been approached in advance for their ideas were also relayed to the meeting:

- Salt Spring Seniors:
  - Accessibility
  - Ability to walk the dog
  - Areas to have a picnic
- Arts Council (for full input, please see Appendix C):
  - Public art: murals, sculpture
  - Dynamic art, i.e. busking
  - Stages
  - Art for all ages
  - Art exhibitions
  - Making art in public
  - Spoken word: theatre, poetry
  - Festivals of all types
  - Concerts
  - Showcase for the community

*"It will be a focal point for locals, a destination, a gathering place. It will enhance the appeal of downtown Ganges especially for boaters and tourists." – from a comment sheet*

Harbour Authority Salt Spring Island (HASSI):

- Kayaks:
  - Secure area for visitors to store their kayaks while visiting
  - Launch area
- Dinghy dock needed
- Commercial fishing dock, that is scheduled and well known
- Scheduled inter-island ferry

Chamber of Commerce (presented by Matt Steffich):

- "Get it done."
- Economic driver for Salt Spring
- Benefits to us as locals to enhance enjoyment of the harbor



Additionally, all the schools on Salt Spring Island were approached for their input, which should be arriving sometime soon.

After the presentations, the small group discussions began, using the World Café meeting method, addressing three questions:

1. How will a boardwalk benefit the community? The economy? The environment?
2. What kinds of things might provide the “wow” factor for a boardwalk in the Ganges Harbour?
3. What do you (personally) want to do or experience on the Ganges Harbour boardwalk that you can’t do on other shoreline trails on Salt Spring?

Key ideas that emerged during the report backs from the first questions included that:

- The idea of a boardwalk or a seawalk contained an image of what was expected right in the name. The idea of a harbour walk was proposed instead, and well received.
- The community is the steward of the harbor, and that in its current state, the harbor is a “disgrace” (to quote a participant) due to the level of debris and pollution in the harbor, including derelict vessels. Harbour cleanup is a priority.
- The interactions that would be promoted by a Harbour Walk can lead to sustained and sustainable connectivity within the community.

The second set of small group discussions addressed a different question, organized according to section of the planning area. The question was “How could the boardwalk connect people to the water and the land in the \_\_\_\_\_ area?”:

4. Peck’s Cove
5. Natural area/undeveloped shoreline parallel to Upper Ganges Road
6. Developed/developable shoreline, near downtown Ganges



An alternate table was available for participants to work out their ideas using modelling clay. There were some intense conversations around this table, and a model was worked out that incorporated a significant number of the ideas as discussed over the course of the day. This model included:



- A bridge over the Cudmore Gap;
- Downward pointing lighting
- Organic shape with lots of curves
- An alcove or bump out with a roof to provide a meeting space, and possibly art space or other
- A series of very small bump outs which could be used for smaller conversation groups.

There were also side conversations between people over the course of the day. One notable idea which emerged was to engage a group of university design students to do a couple of designs for the boardwalk, based on the ideas that emerged from today's meeting. A key proviso would have to be that the design would have to be buildable, and not theoretical. Based on this design, it would be possible to develop some initial cost projections.

Some ideas that emerged over the course of the afternoon were "outside the box" ... A number of people identified the need to expand the scope of the Ganges Boardwalk Completion project to extend more fully around the harbour at the northern end, and to connect also on the south side of Ganges, into the area of the Ganges Creek estuary.

Full reports on the World Café discussions can be found in the Appendix B.

## COMMENT SHEETS

In total, twelve (12) comment sheets were submitted after the meetings. Of these, eleven (11) were generally in support of the completion of the Ganges Boardwalk.

Concerns about the boardwalk project included the following (in their own words):

- Some of the concessions boardwalk property owners want are unrealistic.
- Our community needs many other projects more. Obviously this project has a lot of support. Those who do not support don't come to such meetings.
- Design should be eclectic or fit into the SSI rural/marine/village/farming themes. Should have lots of seating (which should also be artistic) for seniors/disabled/youth/wheelchair supportive.
- We are concerned with the infilled areas and that they should be "green" space, public space.
- Getting it done in a timely manner. To have such shambles of a boardwalk, of what was almost complete so long ago, is an embarrassment to the community. We need to either tear it down

or build something amazing! Almost ALL communities along the shoreline have a walkway. Why not US!!!

- Cost management is critical. Capital costs and ongoing operations and maintenance are major concerns. Cost overruns and excessive operations budget are common on this island (pool, library).
- Partying – late night carousers.
- Getting it completed! A good example is the boardwalk along the Nanaimo waterfront. Shops and concessions on or beside it.
- I would like there to be food carts set up on the boardwalk.
- Too much planning without the agreement of landowners whose agreement is essential to anything happening.
- The process, and getting it past the stalemate that has held the boardwalk up for so long, while maintaining community values.

A number of people provided their names and contact information, and are interested in sitting as a member of a community advisory committee or task force for the project, should one be established. This information is recorded separately, to maintain their privacy, until such time as a committee or task force may be established.

#### COMPILED LIST OF IDEAS FOR THE HARBOUR WALK

*"The harbour is the 'wow' factor. The more that can be done to improve the harbour generally (eg. Derelict vessels, trash, ecological enhancement), the better the boardwalk will be."*

As design of the harbour walk moves forward, the natural beauty of the Ganges Harbour must be enhanced. One metaphor used in discussions was of the harbour walk as a necklace of jewels: connecting parks on the land to the harbour area, sculpture or other public art that could serve as focal or anchor points along the Harbour Walk. It should be seen as a linear park, offering a variety of experiences. Or it could be series of pocket parks, with different parks that do different things. One participant encouraged the design to demonstrate leadership in environmental form: a "marriage of beauty and sustainability."

The design needs to acknowledge that there is a wider context that the facility is meant to enhance, and be enhanced by. For example, it can be part of a story connecting with First Nations culture, or Ganges history, or the ecological/aquatic environment. It should assist people to have a better relationship with the ocean.

The design needs to be coordinated with the work of the Salt Spring Island Transportation Commission, which is



planning to build sidewalks along Ganges Road, on both sides, so that the two projects complement each other. For example, the road-side path could potentially better cater to the needs of bikers, runners, and other more intense users, whereas the harbour walk could be designed for slower uses.

The design needs to be scale-able, with the possibility of being extended beyond what is currently within the scope of the project, including the potential extension of the project to Moby’s pub, allowing for pedestrian traffic off the road.<sup>1</sup>

The design also needs to acknowledge the transition from the urban environment to nature. Activities should become increasingly quiet towards Peck’s Cove, and “bustling” uses should take place near Ganges Village. One participant indicated the need to shift the orientation of Downtown Ganges from inward to outward (to the waterfront) – but that is outside of the scope of this project.

<u>Activities</u>	<u>Design Elements</u>	<u>Other Considerations</u>
<ul style="list-style-type: none"> <li>• Walking</li> <li>• Dogwalking</li> <li>• Powerwalking</li> <li>• Making art/music</li> <li>• Eating</li> <li>• Education/interpretation</li> <li>• Kayak launch &amp; secure storage</li> <li>• Commercial fish sales</li> <li>• Visiting boat anchoring (somewhere nearby)</li> <li>• Climbing wall (opportunity to bridge the grade between street level and boardwalk)</li> <li>• Spiritual connection</li> <li>• Sitting -- benches</li> <li>• Scuba diving opportunities</li> <li>• Kite-flying</li> <li>• Sailing</li> <li>• Public tours of the harbour</li> </ul>	<ul style="list-style-type: none"> <li>• Safe: people of all ages and dogs</li> <li>• Accessibility</li> <li>• Linkages to parks and commercial areas (continuous, linking to existing boardwalk in commercial areas)</li> <li>• Connection to nature</li> <li>• Marine uses: fish sales, water taxi, pocket cruise vessels, float planes, inter-island passenger ferry</li> <li>• Performance space (dance, music, art): amphitheatre?</li> <li>• Public art installations as anchors</li> <li>• Spatially generous – wide</li> <li>• Scale-able<sup>2</sup></li> <li>• Garbage/recycling collection<sup>3</sup> along the harbour walk</li> <li>• Range of activities from passive to active</li> <li>• Pleasant atmosphere</li> </ul>	<p>Story(ies):</p> <ul style="list-style-type: none"> <li>• First Nations</li> <li>• Salt Spring Island</li> <li>• Environment</li> </ul> <p>Funding or financing construction<sup>4</sup></p> <p>Keep bikes off</p> <p>Respect for natural environment; keeping harbour clean (derelict vessels)</p> <p>Respect for First Nations cultural footprint, and archaeological record</p> <p>Re-energize Rotary Park and Peck’s Cove Park</p>

<sup>1</sup> There was a considerable amount of discussion about purchasing the property to the north of Peck’s Cove, to allow the harbour walk to be continued beyond the current reach which is being planned. One option would be to convert this house into a museum. Apparently the Royal BC Museum is looking for a satellite location.

<sup>2</sup> “So we can build what we can afford today, with the ability to easily improve/add on when funds or ideas are available.”

<sup>3</sup> Design so that people can’t offload household trash.

<sup>4</sup> “If classes were held on the boardwalk, a fee could be charged, and funding from this could go towards developing the park.”

### Activities

- Watching fireworks over the harbour
- Open air art classes or painting events
- Listening to music or poetry
- Tai Chi
- "Show" boat for theatrical performances
- Food carts
- Permanently fixed binoculars/ telescope for viewing the harbour
- Stormwatching
- Moonwatching
- Float plane watching
- Sea life watching (fish, seals, birds, etc.)
- Birdwatching

### Design Elements

- Pavilions or kiosks for interpretation or entertainment or display of art
- Anchors at either end, i.e. public art installations (maybe First Nations, maybe Island artists)
- Futuristic style – to communicate that Salt Spring is a forward-looking community
- Integrate upland uses to incorporate art demonstrations or industrial viewing (like Granville Island)
- Flowers along the Harbour Walk
- Access to commercial services (wine & food), community amenities (parks), beach/water access for small boats, wading, fishing
- Gathering spots – greeting and meeting
- Siting away from shoreline to protect ecological values and riparian area
- Organic shape, branching, with pods for gathering
- Make it love-able, so people take care of it
- Celebrate/incorporate First Nations culture: space for story-telling and meaningful participation, architectural structure such as long house with posts
- Rotating interpretive panels with corresponding public art (changing seasonally)
- Habitat for purple martens
- Jut outs or bump outs to be included for seating or busking or other uses, including a concert platform
- Benches for seating
- Picnic tables

### Other Considerations

Sun corridor from Peck's Cove through the harbour (sunrise and sunset)

Protection of the night sky

Address hazardous trees

### Activities

### Design Elements

### Other Considerations

- Railings
- Surfacing should be non-slip, and trip-proof – Scooter-friendly
- Opportunities for romance
- Well-lit for safety, but also to preserve the dark sky
- Should be a floating portion somewhere
- Glass bottom in areas to be able to see below the harbour walk
- How to address elevation change and associated vertical challenge: funicular?
- Dinghy dock urgently needed
- Accessibility for people with physical challenges

### EXAMPLES OR REFERENCE SITES SUGGESTED

- Granville Island
- Promenade near Chateau Frontenac
- Seawall in Vancouver (integration of First Nations language)
- Coast Salish architecture, particularly house posts
- Urban magnets ([www.urbanmagnets.com](http://www.urbanmagnets.com))
- Gulf Islands National Park Reserve, Parks Canada – Story spaces
- Nanaimo sea walk
- Crofton sea walk

### KEY BENEFITS

- The seascape is an important piece of Salt Spring's identity – the boardwalk will enhance this community image.
- Improve the vibrancy of downtown
- Catalyst to unify and develop the Ganges Village
- Creating an experience, a gathering place
- Increase attractiveness of harbour – kickstart other development
- Improved public safety for pedestrians
- Change in island mindset – "we CAN do it"
- Drive towards Canada's 150<sup>th</sup>
- Increase pride in our harbour

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## COMMUNITY

- A 'big draw' for all: Free community amenity in Ganges, a gathering place, a safe place to walk and get exercise
- Providing better access and connection to the water – "Love the Shorefront"
- Opportunity to dip down to the 'special places' from the road level
- A lovely place to work, stroll and eat
- Opens up recreational possibilities



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## COMMERCIAL

- It will encourage tourism year round, including shoulder seasons and weekdays and Sundays (off-market days)
  - Provide an entry way and positive first impression for tourists arriving by boat
  - Local art on the boardwalk can be linked to nearby businesses that sell local art
  - Market-like venues on the boardwalk
- Just finishing it will be an economic driver, by opening up opportunities for commerce
- Ganges as a boating destination improves
- Economic stimulus for artists: strong arts community emphasis
- "Shopping by the sea"

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## ENVIRONMENTAL

- Harbour cleanup: derelict vessels, garbage, other pollution
- Educational focus on protecting nature, i.e. preserving harbour vegetation, understanding the marine environment
- Increase profile of other major harbours: Fulford, Vesuvius, and Bourgogne

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## CAUTIONS

- Danger in too much emphasis on entertainment – should be carefully managed
- Noise management is important: Boardwalk should be kept "quiet and tranquil"
- Boardwalk should not be visually intrusive

- Make sure the thing is properly funded for ongoing maintenance
- Ensure that experiences on the boardwalk are carefully selected, but not overly programmed
- Art should enhance the natural beauty of the harbour, not deter or distract from it
- Music could be problematic: must consider interference from car traffic, and issues for boats in the harbour if the music is too loud – could sound barriers mitigate noise issues?
- Be careful about creating competition for existing businesses. There should be some food trucks, but not too many.
- Respect marine rights of way
- Too much engineering may create visual intrusion and reduce the mental/emotional connection to nature
- Additional car traffic and parking requirement that might be generated by development of the boardwalk



**APPENDIX A: ANTICIPATED BOARDWALK PROJECT PROGRESSION**

Islands Trust	Capital Regional District		
Visioning & concept development			
Land use decision-making			
Establish project Task Force (joint Islands Trust and CRD, with community members)			
Design requirements, including a topographic survey that includes significant surface features (vegetation, infrastructure, pilings, tides, etc.)			
Feasibility study, including: <ul style="list-style-type: none"> <li>• Cultural impact assessment</li> <li>• Environmental impact assessment</li> <li>• Financing options</li> </ul>			
Statutory Rights of Way with property owners			
		Preliminary design (general framework)	
		Community consultation re financing options (possibly a referendum)	
		Detailed design	
		Development of procurement processes	
		Establish financing mechanism	
		Construction drawings	
		Selection of a contractor	
		Construction	
		Operations & Maintenance	

## APPENDIX B: TABLE DISCUSSION SUMMARIES

Table discussions occurred in two different sets:

1. Questions 1-3 were addressed first, with meeting participants able to pick two of the three questions to address in small groups.
2. Questions 4-6 were addressed second, with meeting participants able to pick one of the three areas to address in small groups.

This section

### QUESTION #1: HOW WILL A BOARDWALK BENEFIT THE COMMUNITY? THE ECONOMY? THE ENVIRONMENT?

#### How will a boardwalk benefit the community?

- It will provide a safe place for local residents to walk (and get exercise) while in downtown Ganges (i.e. after work, but before Pilates)
- It will boardwalk will be a free community amenity in Ganges, (ie. a gathering place, a venue
- It will improve the vibrancy of downtown, and connect sections of down town
- The seascape is an important piece of Salt Springs Identity – the boardwalk will enhance this community image

#### How will a boardwalk benefit the economy?

- It will encourage tourism year round, including shoulder seasons and weekdays and Sundays
- It will provide an entry way and positive first impression for tourists arriving on Salt Spring via boat
- Having food available on the boardwalk will support local businesses
- Having local art on the boardwalk can then be linked to nearby businesses that sell local art
- There could be market-like venues on the boardwalk, people could 'rent' spaces
- Place to advertise community events

#### How will a boardwalk benefit the environment?

- The boardwalk will force locals to clean up the nearby shoreline, and keep it clean
- Environmental education can be provided through signage and kiosks
- The boardwalk could be a place for schools to go and use as an outdoor classroom (learn about the sea, climate change etc.)
- Boardwalk should be kept quiet and tranquil
- Boardwalk will encourage walking, therefore reducing car use and Greenhouse Gas Emissions

(Table Host: Shelley Miller)

#### FIRST ROUND

- Call the project the HARBOUR WALK to avoid preconceptions of meaning that the words 'Boardwalk' or 'Seawalk' may have.
- Catalyst to unify and develop the Ganges Village
- Creating an experience
- Just finishing it will be an economic driver
- Open up opportunities for commerce

- Tourism potential for “Greater Ganges Basin”
- Ganges as a boating destination improves – provides economic boost from this large sector of visitors
- Increase attractiveness of harbour - kickstart other development – north end of harbour
- Public safety improves – pedestrian-friendly
- Change island mindset – “we CAN do it”
- Drive towards Canada’s 150<sup>th</sup>
- Increase pride in our harbour
- Clean up the north end of harbour
- Educational experience in a harbour clean-up
- Focus on protecting nature
- Educational component to preserving harbour vegetation
- Elevate importance of marine environment
- “Love the Shorefront”
- dip down to ‘special places’ from the road level – as a contrast to “the village”
- The water IS the experience
- Raising the relevance of our other major harbours – Fulford, Vesuvius and Bourgogne
- Lifestyle tourism
- Impetus to become much more tourist-friendly
- We are no longer dependent on fishing for our economy
- Economic driver – connect with nature
- Incorporate a Kayak launch – interact with harbour

## SECOND ROUND

### **COMMUNITY**

- A lovely place to work, stroll and eat
- A catalyst
- A ‘big draw’ for all
- Danger in too much emphasis on entertainment – should be carefully managed

### **ECONOMY**

- Enhance tourist experience (avoid disappointment)
- Open up recreational possibilities
- Create something of interest
- Ability to see boats and observe the harbour activity for locals
- Yachts in harbour – most people never get to see what’s going on
- Shopping by the sea
- Pleasant environment for tourists
- Strong arts community emphasis
- Pleasant atmosphere – a better experience
- ‘Pavilions’ – entertainment displays
- a continuous strip of activity
- economic stimulus for artists

### **ENVIRONMENT**

- Encourage clean up of harbour – new rules and regs?
- Interact with sea, rocks, land – observe what happens at the shoreline

- Historical aspects
- Educating visitors and ourselves
- What happens – people, ocean, industry - what measures have to be taken?
- “Stewardship of the foreshore”

(Table Host: Robert Steinbach)

Round 1: “One more reason to get outdoors throughout the year and get some exercise; schools for birdwatching & learning about the harbour; another way for visitors to enjoy the harbour & enjoy meeting others; people learn about environment by spending time in it seeing waterbirds, sea mammals, eelgrass, or even seeing the negative impacts like garbage may inspire them to live differently or create change through action like beach clean ups. – Sharon Bywater!

Further discussion with the participant led to the recognition that a boardwalk would lead to creation of many opportunities for people to interact with other people, with local economic enterprise, and surrounding nature of the harbour. This would in turn support the sustainability of connections afforded by the construction of the board walk.

Round 2: Benefits discussed included seeing the boardwalk supporting the conversion of the Ganges core into a walking area (Healthy Community); protecting wildlife; cleaning up the harbour; enticing visitors to extend their stay to overnight. Potential for an increase in resident satisfaction was also suggested.

One negative concern focused on the additional car traffic and parking requirement that might be generated by development of the boardwalk.

(Table Host: Michael Kelly)

## QUESTION #2 – THE “WOW” FACTOR

Overarching theme of discussion: **The harbour is the “wow” factor.** The more that can be done to improve the harbour generally (eg. derelict vessels, trash, ecological enhancement) the better the boardwalk will be.

Other potential “wow” features

- Scent (hanging floral baskets, planters, etc)
- Performance space for dance/music/art (see promenade near Chateau Frontenac for inspiration)
- Something to anchor it at either end – probably big public art installation (maybe First Nations, maybe Island artisan)
- Must be spatially generous so you don’t feel like you’re on a conveyor belt
- Climbing wall, since the grade between street level and boardwalk at Peck’s Cove is steep

General sometimes contradictory non-“wow” comments

- Connectivity to Treehouse Café is important – make it know that this is continuous public space for the enjoyment of all
- Boardwalk should not be visually intrusive
- Boardwalk should have a futuristic style, to let people know that Salt Spring is a forward-looking community
- Must be dog friendly
- Should be scalable so we can build what we can afford today, with the ability to easily improve/add on when funds or ideas are available
- Make sure the thing is properly funded for ongoing maintenance
- Adequate garbage/recycling facilities, but designed so people can't offload household trash

(Table Host: Jason Youmans)

The "Harbour Walk" is the wow ... Can have enhanced experiences, carefully selected but not overly programmed. Example: sculpture feature in bay/Peck's Cove area.

Design needs to acknowledge that there is a wider context that the facility can enhance, i.e. story/network/ connecting based on First Nations culture, type of park, etc.

Urban connection and transition needs to be acknowledged:

- Range of activities from passive to dynamic (urban carnival)
- Integrate marina-type uses into design considerations, i.e. fish sales, water taxi, pocket cruise vessels, float planes
- Integrate upland uses to incorporate art demonstrations, industrial viewing (like Granville Island)

(Table Host: Chris Hall)

**QUESTION #3 – WHAT DO YOU PERSONALLY WANT TO DO OR EXPERIENCE ON THE GANGES HARBOUR BOARDWALK (THAT YOU CAN'T DO ON OTHER SHORELINE TRAILS ON SALT SPRING)?**

Responses focused around the arts, health, and tourism.

- Pleasure: Food, art, performances, rest, leisure
  - Touch all the senses
  - Satisfy curiosity
- Education: Ganges history, First Nations, ecological, aquatic, etc.
- Access to:
  - Commercial services (wine & food)
  - Community amenities (parks, etc.)
  - Waypoints, connectivity
  - Beach/water access: small boats, wading, fishing
- Art: performances (to view and to participate)
- Gathering spots or areas: nooks, gazebos, benches, bump-outs, etc.

- Fireplace?
- Greeting and meeting: locals and visitors
- Make it WIDE
- Keep bikes off

(Table Host: Stefan Cermak)

- Spiritual Connection
- A sense of calm
- Places to sit and observe
- Opportunities to dive (scuba diving centre)
- Respect for natural environment/Addressing environmental harms associated with derelict vessels
- Community gathering place
- Dynamic and evolving vibrancy
- Situated away from shoreline to protect ecological values and riparian area
- Provide access to the beach so you can dip your feet in the ocean
- Shift the orientation of Downtown Ganges from inward to outward to waterfront
- A safe place to walk – with my dog or my kids!
- Education about marine ecosystems
- Organic branching shapes with pods for gathering
- People save what they love, so create a boardwalk that you can fall in love with
- Tie-in with other recreational activities such as kite flying, sailing, and kayak launching
- Protect and educate about middens as industrial sites and First Nations history
- Engage First Nations to tell their story and meaningfully participate in current project
- Rotating interpretive panels with corresponding public art (changing seasonally)
- Inter-island passenger ferry
- Leadership in environmental form – marriage of beauty and sustainability
- Clean up industrial debris
- Re-energizing Rotary Park
- Provide education and habitat for purple martens
- Contemporary art should not deter or distract from natural beauty
- A destination involves some form of attractions
- Commercial enterprise – public tours of the harbour – kayak storage for a nominal annual fee – sales of fish from boats at dock
- More places to watch Canada Day fireworks

(Table Host: Seth Wright)

- Include spaces for open air art classes
- Space for music
  - Buskers

- Performances by choirs
- Must consider noise ie) noise interference from car traffic, and issues for boats in the harbor if the music is too loud
- Might sound barriers mitigate noise issues?
- Jut outs should be included on the boardwalk
- Saltspring Painters Guild has held “performance painting” around the island, during which they typically paint the landscape in front of them. Could do this on the boardwalk
- There should be quiet areas as well (increasingly quiet towards Peck’s cove)
  - Activities like Tai Chi
  - Poetry reading
- More of the “bustling” uses should take place near Ganges village
  - ie) music/ performances
- Although it is outside of the plan area, the old “dilapidated” house to the north of Peck’s cove park would be a great addition to the boardwalk/ park. Could be part of the quiet area.
- Should be able to walk from Ganges to Moby’s off of the road (which is quite dangerous for pedestrians). The boardwalk should function as a practical walking route for transportation purposes.
- Should not be too secluded, for safety’s sake
- Have a “show boat” that would putter around the harbor, acting as a stage that could be viewed from the boardwalk
- Make the food carts accessible from the seawalk
- Benches for seniors to rest on at regular intervals along the boardwalk
- A concert platform
- In the past, there was a group that performed a circus act on a sailboat in the harbor. Would be great to see more of this type of thing
- Permanently fixed binoculars/ telescope for viewing the harbor (maybe charge for this, maybe not)
- Should cater to powerwalkers
- Debate over whether it should cater to bikers/ runners/ other ALTRA users. It was the belief of the group that there are plans afoot to develop a cycling/ running lane next to the road along the same stretch as the boardwalk, so that could accommodate other users.
- If classes were held on the boardwalk, a fee could be charged, and funding from this could go towards developing the park
- Food vendors
  - Careful about creating competition with existing businesses.
  - Some food trucks, but not too many
- Picnic tables should be included along the way
- Railings should line the boardwalk, so that seniors can use them for support with walking
- Surfacing should be trip proof – emphasized that large gravel rock should not be used.
- Should be romantic with places to kiss that are semi-private. (When asked “what do you want to experience on the boardwalk?”, a participant answered “I want my boyfriend to kiss me” <3)

- Could include a map of the world on which boardwalk users can indicate where they are from ie) with a pin
- It was asked if tacky “boardwalk” souvenirs should be sold along the boardwalk
  - The group then decided that it would be more suitable to have boardwalk related souvenirs sold at the market
  - The comment suggests that the boardwalk could become a larger part of Saltspring’s destination marketing for tourism
- Informative plaques re: flora and fauna of the area, First Nations history, other Saltspring history
- Beach access is important
  - Must be safe
- Garbage and recycling bins at regular intervals along the way.
- Currently a TUP in process to allow foodcarts closer to Ganges
- Have a kayak launch spot
- Watching seaplanes land is very enjoyable.
  - Would be great to have a place to watch from.
  - The existing height of the boardwalk would allow for a good view of this.
- Facilitate storm watching opportunities
- Important that this ends up being a pedestrian linkage between Moby’s and Ganges
- The proposed cycling path along Lower Ganges Rd should be on the west side of the road, so it does not compete with the boardwalk as much
- Boardwalk should be scooter friendly
- Celebrate/ incorporate First Nations culture
- “Jut outs are good”
- It makes sense to use what is there as much as possible, to complete the project efficiently
- It should be well lit for safety
- Focusing on the relationship to the ocean is very important
- Should facilitate sea life viewing
- What makes the boardwalk different than other seaside trails on the Island is the ability to go out for food and drinks, and socialize with people you meet along the way
- Whatever is incorporated into the boardwalk should “add interest”
- Noise management is important

(Table Host: Maddy Koch)

#### QUESTION #4 – GETTING CLOSE TO THE WATER AT PECK’S COVE

- Should be a floating portion to the boardwalk somewhere – either whole thing should float, or at least a ramp down to a floating portion

- Glass-bottom to floating portion proposed – but noted by one table member that this portion of the harbour is a bit of a dead zone, owing to lack of tidal movement and years of accumulated logging debris

Our conversation then veered into discussion of how to highlight First Nations culture through the boardwalk, both as a mark of respect, and to exploit the grant opportunities available for First Nation related projects – (maybe small post house, carvings, interpretive panels, etc).

(Table Host: Jason Youmans)

- Learning centre
- Sun corridor
- Marine rights of way
- Viewing Platform
- Elevation change – vertical challenge – funicular?
- Terminus – a place of significance
  - Mass
  - Anchor
  - Parking
  - Look-out
  - Sea cans as part of structure?
  - Big public art piece as a marker
  - Dinghy dock urgently needed
  - Recreational facilities
  - Amphitheater at/near shoreline near the bank
  - Commercial services – “the BEST ice cream/coffee/fish & chips, etc.
  - Kayak storage for locals?
  - Exploit more connections between land and water, town and harbour

(Table Host: Robert Steinbach)

**QUESTION #5: NATURAL AREA/UNDEVELOPED SHORELINE: HOW COULD THE BOARDWALK CONNECT PEOPLE TO THE WATER AND THE LAND IN THE UNDEVELOPED AREA OF THE SHORELINE?**

- Boardwalk could provide a physical connection (i.e. stair), so that people can walk on the beach at low tide
- People will be connected physically, which will increase their mental/emotional connection ( i.e. they will be more in touch with nature)
- Utilizing First Nations art/structures etc. will create social/cultural connections with First Nations culture
- Need to find a way to ensure accessibility so that older/disabled people can be physically connected to land and sea
- The ‘book ends’ of the board walk are the best places for physical connections
- Too much engineering may create visual intrusion and reduce the mental/emotional connection to nature

(Table Host: Shelley Miller)

- Consider incorporating First Nations architecture ie) a post house (?)
- There is an existing staircase by the “steel hut” (at the end of the existing boardwalk near Peck’s)- this may be a place to consider a wheel chair access,
  - there was doubt in the group that this would be feasible due to the grade
  - Suggested that a clifftop path would be more accessible for scooters
- Find a location along the boardwalk where divers could retrieve sea life for school children to observe
- Beach access points
- Will need to address hazardous trees
- Have different pocket parks along the way
  - “different parks that ‘do’ different things”
- Convert Williams house (to the north of the plan area) to a museum

(Table Host: Maddy Koch)

#### QUESTION #6: DEVELOPED/DEVELOPABLE SHORELINE AREA

Discussion identified several barriers that would need to be overcome in the design: stairs; low level of accessibility (universal access); constrained economic context, seasonality, darkness.

Interaction among people and place was seen as a way to encourage development of a dynamic project. ([www.urbanmagnets.com](http://www.urbanmagnets.com)).

(Table Host: Michael Kelly)





### **The Arts on the Boardwalk**

“Public art has the power to energize and enhance our public spaces, make us think, and transform where we live, work, and play.”

Victoria BC policy on public art

Salt Spring sells itself as an island of artists. One of the most visible and impactful ways for art to get to the public in contemporary society is public art and public art education/experience. With the expansion of the boardwalk (to say nothing of arts utilization of our existing public spaces in Ganges) we have a huge opportunity to promote art as a public expression of our identity through public art and the promotion of arts education in this new public space.

#### ***What is public art?***

Public art is art in any media (visual, performing, literary) that has been planned and executed with the intention of being staged in the public domain, usually outside and accessible to all. Public art can involve physical, permanent artworks and sculptures, either permanent or temporary, and it can also entail public performance and public participation in the arts.

Public art can be spontaneous, commissioned, promoted through open competition, and/or curated. It can deal with the environment, public issues (e.g. democracy, citizenship, integration), or quite simply pure entertainment. Pleine air arts education is somewhat different from public art, but is also staged in the public domain.

#### ***How common is public art?***

In Europe public art is everywhere and very much a feature of daily life. Increasingly we are seeing public art as a major pre-occupation of cities and towns in Canada. Witness the wall murals in Chemanus and the sculptures by the harbour and on Beacon St in Sidney-by-the-Sea. And some municipalities have already sprung onto action in a formal way. For example, Victoria has a formal policy on public art and active community programs to encourage it. See <http://www.victoria.ca/EN/main/departments/parks-rec-culture/culture/art-culture/public-art/policy.html>. Salt Spring needs to “spring ahead” here or we will not reap full benefit of an investment in the boardwalk!

#### ***What sorts of activities do we envision for the boardwalk?***

The Salt Spring Arts Council (SSAC) would like to see the boardwalk becoming a place of excitement, opportunity and engagement for our community and our visitors. We want to see spaces that are dynamic and invite people of all ages to gather; for example a busking space that encourages people to sit, listen or interact near, far, standing and on benches. We want to see spaces that invite everyone

and anyone to perform: for example, nothing encourages children to perform more than being on a stage (large or tiny); suddenly they will break out into song and dance.

Whether in a more structured or planned approach to public arts activity, we are limited only by our imagination. Among the activities we would like to see promoted:

- Public art exhibitions – temporary shows mounted by the artist or curated by others. We would love to see a mural along the boardwalk that is created by the community.
- Public art installations – works of art that may be permanent, temporary, and/or ephemeral (e.g. some environmental art that naturally decays with time).
- Making art in a public space – e.g. pleine air painting, a place where artists could set up a small table to carve wood sculptures, and basket weaving on the boardwalk .
- Public performances/performance art – planned and advertised, or surprise events like flash mobs. This would be a particularly interesting opportunity for GISS GISPA students. Why not poetry readings and dance events?
- Buskers – with a couple of locations specifically designated for buskers we should attract some of the excellent busking talent that we see in other venues. And how about clowns? Consider a buskers and/or clown festival as a feature event for tourists.
- Music – in addition to buskers, we could have concerts by the harbour. With a band or orchestra on a barge in the harbour we could line the boardwalk with spectators. Alternatively – use the Centennial Park band shell for the start and end point for roving musicians.
- Art classes – with the natural beauty and activity in our harbour, is there a better place in the summer for art classes – especially if the boardwalk has some areas large enough to accommodate a dozen students. This would be a natural extension for the SSAC Workshop programs oriented towards youth.

### ***The role of SSAC***

As the portal to the arts on Salt Spring Island, SSAC would see itself working with local planning groups to flesh out these concepts and, ideally, help work towards development of a public art policy that would engage the community in constructive and creative ways. SSAC has already introduced a land art (also called environmental or nature art) segment to Art Craft and it is now moving into its second year of running. This could easily be expanded onto the boardwalk. Also the Arts Council would be in a strong position to help with any jurying of public art solicited through competition as a permanent or temporary display.

### ***In conclusion***

The boardwalk would unite the town of Ganges and showcase its beauty and unique culture for visitors. From an artists' perspective, we simply need people to come to SSI; the boardwalk is the single most proactive way to encourage that. See also the appendix for some proposals received by SSAC as contributions to this discussion.

Ron Crawford, Chair, SSAC

Yael Wand, Arts Administrator, SSAC

## ***Appendix - Boardwalk proposals received by SSAC***

### *Moving sculptures*

Over the period of about 1 week, a group from the community under the guidance of Jennier Mascall from Vancouver's Mascall Dance, will create a series of moving sculptures. At the end of the week, the performers will present the work along and near the Ganges Boardwalk.

Viewers will walk the Boardwalk and be treated to many interesting visual perceptions of human and other movement.

A proposal would be submitted to Made in BC - Dance on Tour to cover the costs of mounting and advertising such an event.

### *Celebrating 2017*

This project would present to the SSI community a banner four feet high and about 300 feet long that would celebrate our sense of identity as islanders during 2017, the Canada 150 celebration.

### *Creating the boardwalk as a dynamic public space*

What is envisioned are non-permanent or semi-permanent installations (lasting maybe no more than 6 months). 2-D or 3-D installations and performances could be selected through a jury process that draws from the arts community, the non-arts community, as well as PARC and/or CRD reps.

As a public, juried art space, there would be yearly submissions. Judgment of the value of these submissions would be based on explicit criteria published and open to refinement through public dialogue.

The boardwalk is quite long and various 'zones' could be established (equivalent to different 'galleries' in a traditional space). These 'zones' could have different aesthetic priorities. Applicants could be required to address the specific priority of the 'zone' for which they are applying – e.g. environment, social well-being etc.

# PROPOSED

## SALT SPRING ISLAND LOCAL TRUST COMMITTEE BYLAW NO. 491

### A BYLAW TO AMEND SALT SPRING ISLAND LAND USE BYLAW, 1999

The Salt Spring Island Local Trust Committee, being the Trust Committee having jurisdiction in respect of the Salt Spring Island Local Trust Area under the *Islands Trust Act*, enacts as follows:

#### 1. Citation

This bylaw may be cited for all purposes as “Salt Spring Island Land Use Bylaw, 1999, Amendment No. 3, 2016”.

#### 2. Salt Spring Island Local Trust Committee Bylaw No. 355, cited as “Salt Spring Island Land Use Bylaw, 1999,” is amended as follows:

##### 2.1 Section 1.1 Definitions, Sub-Section 1.1.1 is amended by adding the following new definition:

“**Creative industry**” means music, art and craft studios, picture framing, schools (commercial, art, vocational), and small-scale research facilities.

##### 2.2 Section 9.2.1 - Permitted Uses of Land, Buildings and Structures, Section 9.2.2 Size, Siting and Density of Permitted Uses, Buildings and Structures, and Section 9.2.3 Subdivision and Servicing Requirements are deleted in their entirety and replaced with the following:

#### 9.2.1 Permitted Uses of Land, Buildings and Structures

	C1	C2	C3	C4	C5	C6	C7
<b>Principal Uses, Buildings and Structures</b>							
<i>Indoor retail sales and rentals</i>	◆	◆	◆				◆
<i>Indoor retail services, excluding Laundromats</i>	◆	◆	◆	◆			
Laundromats	◆						
<i>Outdoor retail sales of nursery plants and home gardening supplies</i>	◆	◆					◆
<i>Indoor production of food and drink items, clothing, crafts, artwork, jewellery and similar items for retail or wholesale sales, provided there is a retail outlet on the premises and provided water consumption does not exceed 1600 litres/day</i>	◆	◆	◆				◆
Offices	◆	◆		◆			◆
Banks and credit unions	◆	◆					
<i>Indoor commercial recreation and amusement facilities</i>	◆	◆			◆		◆

Restaurants	◆	◆	◆				◆
Churches	◆	◆					
Libraries	◆	◆					
Offices for use by building construction professionals and trades	◆	◆				◆	
Automobile service stations			◆				
Automobile rentals with a maximum of five vehicles stored on-site	◆	◆	◆			◆	◆
Veterinarian clinics and animal hospitals	◆	◆					
Indoor commercial and vocational schools	◆	◆					
Daycare centres for children, seniors, or people with special needs	◆	◆					
Funeral homes						◆	
Multifamily dwelling units		◆					◆
Commercial guest accommodation in hotels or guesthouses.		◆					◆
Retail sales of building supplies, appliances and furniture						◆	
Light industry						◆	◆
Wholesale sales						◆	
Storage of goods and vehicles, with the exception of outdoor storage of derelict vehicles or equipment, or waste materials						◆	
Boat building, servicing and repairs						◆	◆
Service, repairs and sales of vehicles and equipment.						◆	
Collection of recyclable materials, excluding outdoor sorting and storage	◆		◆		◆	◆	
Public service uses	◆	◆	◆	◆	◆	◆	◆
Neighbourhood or marine pub, accessory to a marina							◆
Marina and marina services							◆
Marina administration and services, including washrooms, showers and laundry facilities							◆
Marine fuelling stations							◆
Marine related retail stores and offices							◆
Boat rentals and docks							◆
Marine equipment rentals, excluding personal watercraft							◆
Creative Industry							◆
<b>Accessory Uses</b>							
Indoor retail sales accessory to another permitted use						◆	
Dwelling units accessory to a commercial use	◆	◆	◆	◆	◆	◆	
Restaurant accessory to another permitted use					◆		
Home-based businesses accessory to residential use		◆					◆

### 9.2.2 Size, Siting and Density of Permitted Uses, Buildings and Structures

- (1) The following uses identified in Section 9.2.1 are not permitted on the ground level in the C7 Zone only: *Offices, Multifamily Dwelling units, Commercial guest accommodation.*
- (2) *Building setbacks from the natural boundary of the sea do not apply to buildings or structures used for boat building, boat repairs, boat rental and marina administration and services.*
- (3) Subject to Articles 1 and 2 above, and Part 4 – General Regulations (Siting), *Commercial Zones* must comply with the following regulations regarding size, siting and density:

	C1	C2	C3	C4	C5	C6	C7
<b>Lot Coverage and Floor Area</b>							
Maximum combined <i>lot coverage</i> of all <i>buildings</i> and <i>structures</i> (percent)	75	75	33	33	33	75	33
Minimum size of a <i>dwelling unit</i> (square metres).	N/A	30	N/A	N/A	N/A	N/A	N/A
<b>Number of Units</b>							
Maximum number of <i>multi-family dwelling units</i> per hectare	N/A	37	N/A	N/A	N/A	N/A	37
Maximum number of <i>commercial guest accommodation units</i> per <i>lot</i> , operation or site	N/A	50	N/A	N/A	N/A	N/A	50
<b>Setbacks and Siting</b>							
Despite Subsection 4.3.1, the following <i>lot line</i> setbacks apply in the specific zone indicated:							
Minimum <i>Front lot line setback</i> (metres)	0.0	4.5	*	*	*	*	0.0
Minimum <i>Rear lot line setback</i> abutting non- <i>commercial</i> or non- <i>industrial zone</i> (metres)	6.1	*	*	*	*	6.1	10
Minimum <i>Rear lot line setback</i> abutting a <i>commercial</i> or <i>industrial zone</i> (metres)	0.0	*	*	*	*	6.1	10
Minimum <i>Interior side lot line setback</i> abutting non- <i>commercial</i> or non- <i>industrial zone</i> (metres)	6.1	*	*	*	*	6.1	*
Minimum <i>Interior side lot line setback</i> abutting <i>commercial</i> or <i>industrial zone</i> (metres)	0.0	*	*	*	*	6.1	10
Minimum <i>Exterior side lot line setback</i> (metres)	0.0	*	*	*	*	6.1	*
<i>Dwelling units</i> are not permitted in a <i>basement</i> or below any <i>commercial use</i>	N/A	◆	N/A	N/A	N/A	N/A	N/A

\* Indicates provisions in Section 4.3 apply.

*Information Note:* Under the Highways Act, special permission from the Ministry of Transportation and Infrastructure is required to place structures within 4.5 metres of a highway right-of-way.

*Information Note:* Required setbacks from water bodies are outlined in Sections 4.4 and 4.5.

*Information Note:* Vegetation screens are required where commercial or industrial uses take place outside of a building adjacent to non-commercial or non-industrial uses. See Subsection 3.4.2.

### 9.2.3 Subdivision and Servicing Requirements

(1) The regulations in this Subsection apply to the *subdivision* of land under the *Land Title Act* and the *Strata Property Act* for the Commercial 1, Commercial 2, Commercial 3, Commercial 4, Commercial 5, Commercial 6, and Commercial 7 Zones .

*Information Note:* For land in the Agricultural Land Reserve, see Subsection 5.1.2.

	C1	C2	C3	C4	C5	C6	C7
<b>Minimum Lot Areas, Water Supply, and Sewage Treatment Servicing Requirements.</b>							
Minimum area of an individual <i>lot</i> that may be created through <i>subdivision</i> , provided each <i>lot</i> has an individual on-site sewage treatment system and an adequate supply of <i>potable</i> water (hectare)	1	1	1	1	1	.6	1
Minimum area of an individual <i>lot</i> that may be created through <i>subdivision</i> provided each <i>lot</i> has an individual on-site sewage treatment system and connection to a <i>community water system</i> (hectare)	1	.4	1	1	.2	.15	1

2.3 Schedule “1” – Zoning Map, is amended by changing the zoning classification of:

- (a) Lot A, Section 1, Ranges 3 and 4 East, North Salt Spring Island, and of District Lot 2066 Cowichan District, Plan VIP83860 and a portion of the Shoreline (PID 027-249-794, 145 Lower Ganges Road) from Residential 6 and Shoreline 6, to Commercial 7.
- (b) Lot 1 Sec.1, Ranges 3 and 4, North Salt Spring Island, Cowichan District, Plan 49983 and a portion of Shoreline from Residential 6 and Shoreline 6, to Commercial 7.
- (c) Lot A, Section 2, Range 4 East, North Salt Spring Island and District Lot 689, Cowichan District, Plan VIP 65932 (PID 023-882-921, 151 Lower Ganges Road) from Industrial 1, to Commercial 7; and water lot lease DL2092 from Shoreline 6 to Shoreline 1.
- (d) Lot 3, Section 2, Range 4 East, North Salt Spring Island, Cowichan District, Plan VIP 59755 (PID 018-951-775) and a portion of the Shoreline from Commercial Accommodation 1(d) and Shoreline 1, to Commercial 7.

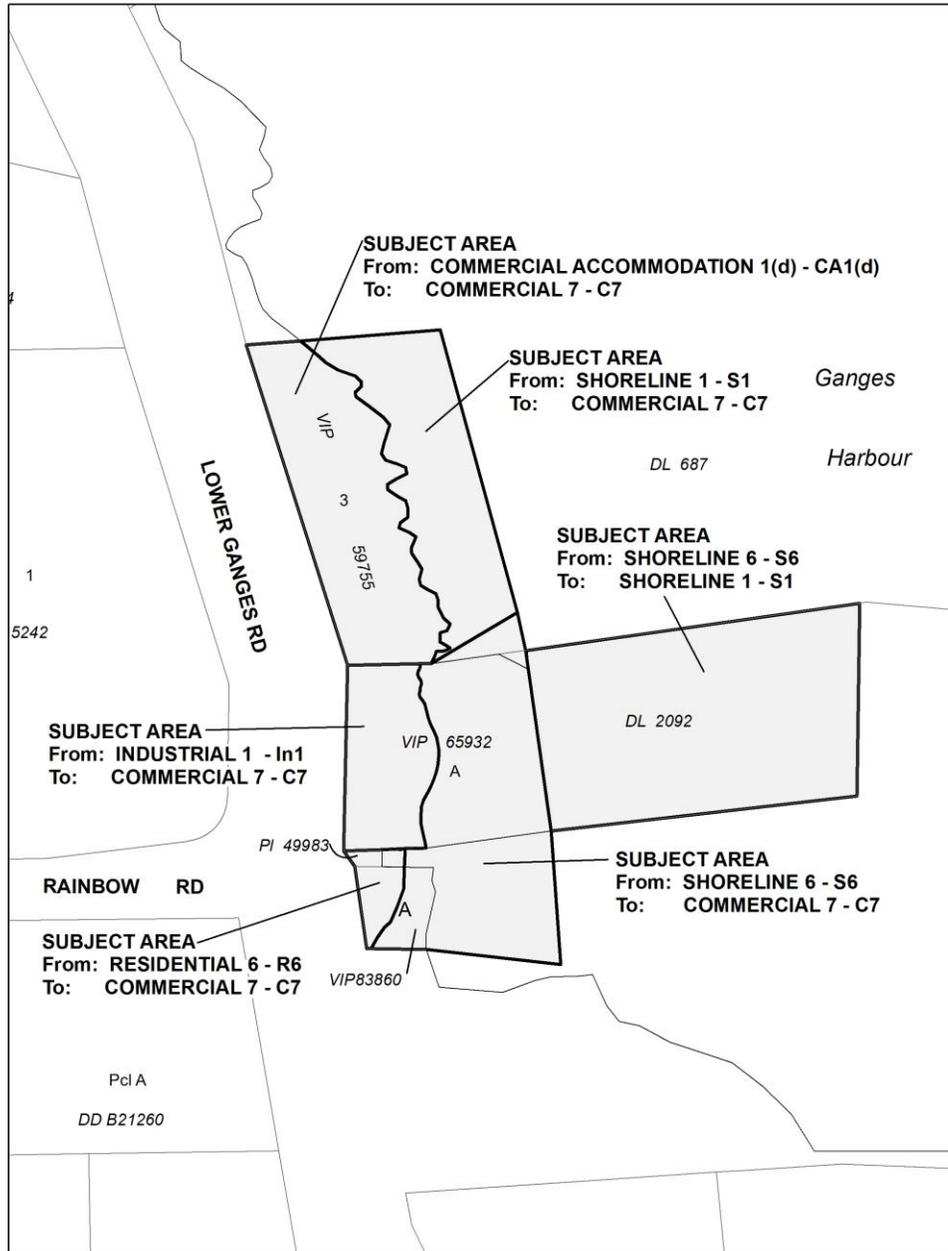


# PROPOSED

SS-BL-491

## SALT SPRING ISLAND LOCAL TRUST COMMITTEE BYLAW NO. 491

### Plan No. 1



Islands Trust  
**Community Consultation  
Summary and Recommendations**

**Ganges Harbourwalk Upland**

November 22, 2017



# Executive Summary

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The Salt Spring community has been working toward the completion of a harbourwalk in Ganges for approximately 30 years. Issues with access and funding have delayed the completion of this important piece of community infrastructure. To advance the harbourwalk project, the Salt Spring Island Local Trust Committee (LTC) has proposed rezoning four upland properties to allow greater development potential in exchange for landowner cooperation in harbourwalk development.

The LTC determined that a community engagement session would be useful to inform the LTC's understanding of the community's perspective on the appropriate scale and siting of future upland development on the four subject properties.

After issuing a Request for Proposals, Dillon Consulting Limited was selected and worked with Islands Trust staff to prepare the community workshop.

This report documents the planning and hosting of the event along with the comments and input from the community.

The engagement session was held on November 4, 2017 at the Lions Hall. Turnout far exceeded the expectations of staff and Dillon. As a result, the facility was not large enough to accommodate the over 150 people who wished to attend. Nonetheless, the workshop continued and a significant amount of input was generated and is summarized in this report.

The general conclusion based on the input from the community is that the community is generally in support of a harbourwalk, but not in support of greater development on the adjacent upland properties portion. The community generally wants to pursue getting the harbourwalk statutory right-of-way either through modifications to the existing marina leases, expropriation, or even bypassing that portion of the harbourwalk and using an improved sidewalk on Lower Ganges Road.

The report provides more details on the community input.



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## Executive Summary

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- Appendix A Comments from the Workshop
- Appendix B Comment Summary Sheet
- Appendix C Workshop Materials

# 1.0 Introduction

On August 3, 2017, the Islands Trust issued a Request for Proposals to hire a qualified engagement consultant to facilitate a community engagement session to capture community input on the future development potential of four waterfront properties adjacent to the proposed route of the Ganges Harbourwalk. Dillon Consulting Limited (Dillon) was the selected consultant. The subject properties include the Cudmore land, the Farmers Institute, the Fishery and the Ganges Marina (see **Figure 1**).

The Local Trust Committee (LTC) has initiated a rezoning process to increase the developability of these lands as an incentive to secure harbourwalk development and public access. While the community is generally in agreement that a harbourwalk would be a positive development for Ganges and the harbour, the proposed rezoning has been hotly debated.

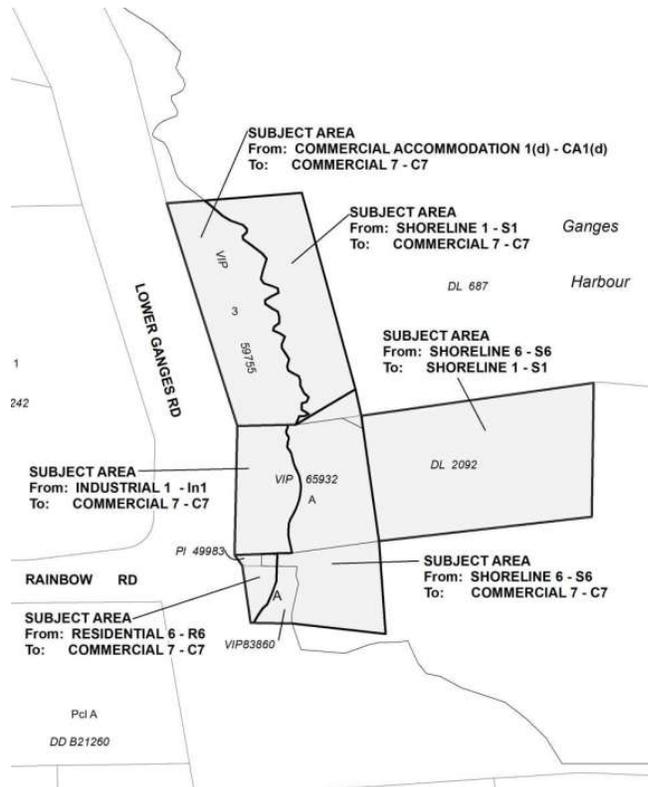
The scope of work for the Ganges Harbourwalk Engagement required Dillon to “design and facilitate a focused community consultation process to help the community visualize various basic development scenarios on the upland properties and to provide input to the LTC on the scale of building and uses the community feels are appropriate.” Dillon was advised not to deal with form and character, but rather to look at building size, siting and massing.

The required deliverables were:

- Development of, and leading a process to communicate a variety of potential building scenarios to a public audience including one kick-off meeting with staff and one community engagement session;
- Soliciting feedback from the participating public on those building scenarios facilitated through the community event and submitted comment sheets; and
- Aggregating and conveying that feedback to the LTC by preparing a full summary of the report and presenting the findings to the Islands Trust Meeting on November 30, 2017.

The visual materials to be prepared by Dillon included materials that illustrated a range of potential development scenarios illustrating a variety of heights (from 1 to 3.5 storeys), various setback conditions, massing and site coverage, and options for stepping back the second and third storey. The Islands Trust staff was responsible for advertising the workshop and booking the venue.

Figure 1: Properties for C7 Zoning



## 2.0 Background Information

Implementation of the vision for a harbourwalk in Ganges has been in the works for almost 30 years. Some portions, such as that in front of the Fishery property, have been constructed with a statutory right-of-way held by the Capital Regional District (CRD). Other portions, such as those north of the Marina property, were constructed but the necessary legal agreements for ongoing access and maintenance were not secured. Many of the constructed portions are in poor condition. The CRD has engaged consultants to conduct structural studies to determine the condition of the portions of the harbourwalk that exist as well as the existing pilings.



The proposed harbourwalk route passes through water areas leased to the upland property owners. To date, Islands Trust has been advised that leaseholder consent is necessary in order to authorize a statutory right-of-way through an existing lease.



The LTC, in an effort to keep the dream of the harbourwalk alive, has considered adopting a new amended Salt Spring Island Land Use Bylaw to create a new commercial zone (Commercial 7), that would provide the landowners/leaseholders a greater development level than they can achieve under current zoning, in exchange for a statutory right-of-way and possible contribution to harbourwalk construction. The LTC, before adopting the

proposed C7 zoning, determined that a community engagement session would be critical in determining the actual appetite for increased upland development if it meant securing completion of the harbourwalk.

As noted, there are four properties involved in this C7 zoning proposal: the Cudmore property, the Farmers Institute property, the Fishery property, and the main Ganges Marina property.

## Community Consultation – Ganges Harbourwalk Upland Background Information

The Cudmore property is immediately adjacent to Rotary Park. There has been no fill on this property. This property is known as the Cudmore Gap because the boardwalk is currently constructed around the village to Rotary Park ending at the foreshore of the Cudmore property with a gap, and then there is a small stretch of boardwalk constructed on the Fishery lot. The water area in front of the Cudmore property is the gap between the two. This property is currently zoned Residential 6 (R6), but there is very little building site due to the setbacks and lack of fill.

There is a tiny parcel of land between the Cudmore property and the Fishery that is owned by the Salt Spring Island Farmers Institute. The primary purpose for this property is to provide a location for a sign that advertises a number of community events. There is also a stair access to the water. The Farmers Institute has indicated that they would make this a public access and open the stairs, if the boardwalk was provided as a public access for the community.

The Fishery property includes a number of uses in their building. Parking and marina storage is located on the filled area along the foreshore. The upland is zoned industrial, and the filled foreshore area is zoned as “water”. The Fishery is also the only landowner to have actually constructed a portion of the boardwalk. There is no public access either from the Fishery property or from either end of its boardwalk segment. This portion of the boardwalk is currently used by the Fishery property owners for seating and storage.

The Ganges Marina owns one developed lot and two undeveloped lots running north almost to Upper Ganges Road. They have filled in some of the foreshore and have piles in place left over from the previous attempt to construct the Ganges Harbourwalk in the late 1980s. The Marina applied to rezone the area now being considered for C7 zoning in 2002, proposing a large mixed use building. That rezoning was unsuccessful, but the landowners have put forward their earlier plans again as a model of what they would like to achieve on the property. This development plan is inconsistent with what the proposed C7 zoning would permit.



## 3.0 Community Workshop



On Saturday, November 4, the Salt Spring LTC hosted a community workshop to review the potential for upland development on four properties along Lower Ganges Road with access to the harbour. The format for the session was to be a workshop where attendees would be able to participate in hands-on activities that would allow them to review and comment on a variety of different scales of development.



It was anticipated that 60 to 70 people would attend. The workshop was planned to accommodate six tables of 15 people per table. However, over 150 people came to the meeting and many people had to be turned away at the door due to Fire Code capacity. The sheer number of people made the workshop a challenging format to manage and record the conversation. It was difficult to seat people at tables, and it was difficult to use the interactive exercises.



The people who could not get in took comment sheets with them and several of these were submitted to the Islands Trust office. Other people left early because the noise level was extremely high. Other people left because they were under the impression that the event was to talk about the harbourwalk, not the upland development. Some people were under the impression that this was an open house and thought they could drop in. Ultimately,

the venue was too small to accommodate the interest and because of that, the room was very noisy.

The agenda was organized as follows:

1. Open the Special Local Trust Meeting;
2. Introductions;
3. Background Information;
4. Questions and Answers;
5. Workshop; and
6. Summary.

After the Special Local Trust meeting was opened, Islands Trust Staff provided a presentation and overview of the history of the harbourwalk and the proposed upland rezoning. This provided a background for the group to use as a starting point for further discussions.

Following the presentation, the floor was opened up for a 40 minute question and answer period. While many attendees took the opportunity to express their position, rather than asking questions, the participants generally appreciated the opportunity for a question and answer period and would have liked it to have been extended.

Some attendees were very concerned about the setup of the tables. Many people felt that all comments should be heard by all attendees. The concern was that the format created a “divide and conquer” scenario and that the process ‘wasn’t democratic’. It was explained to the participants that the LTC has accepted that there is general support for a harbourwalk (although the specific form and character of the harbourwalk remains an open question) and that the overriding goal of the day’s event was to consider what level of development would be appropriate for the four parcels of the proposed harbourwalk route.

It was made clear to the group that the C7 zone had received first reading only. It was also clarified that there is no official application for development – nothing has been formally submitted by any of the landowners. It was also made clear that the LTC wanted to gather more input from the community to determine the future steps to be taken.



Several people spoke about the concern that while the harbourwalk was a generally accepted community goal, the development of the foreshore to facilitate the

development of the harbourwalk felt like the community was being ‘held hostage’. It was also noted that it should not be the LTC facilitating this rezoning.

The meeting facilitators tried to introduce the planned interactive exercises working with cut-outs to determine various development scenarios to look at different heights, setbacks and forms of potential structures. Some tables did not get to use these tools as the participants at the table refused to accept any level of development on the uplands.

## Community Consultation – Ganges Harbourwalk Upland Community Workshop

The topics that the facilitators tried to raise for discussion included:

- Address options for access and potential amenities for the community as a result of development;
- Review proposed C7 Zone and the uses it might allow;
- Developer responsibility for the provision of the right-of-way and to construct and maintain the harbourwalk;
- Open discussion on the pros and cons of upland development;
- Potentially viable uses for the upland development;
- Interaction between boardwalk and the upland;
- Form and character of the harbourwalk;
- Review image boards and identify which examples of boardwalks might be acceptable to the community;
- Future ocean levels and storm surges and uses on lower levels of development;
- Massing, ground floor uses, setbacks, height, storeys, interaction with marina; and,
- Future interaction with Lower Ganges Road.



The participants were presented with an image board illustrating a number of boardwalk examples from around the world. Participants were asked to identify which options they felt were examples of what would be acceptable for the Ganges Harbourwalk.

It was clear that the majority of the people who stayed for the workshop portion were against any development on these properties; however, some in attendance respected the existing zoning, understood that the developer could be required to contribute to the construction of the harbourwalk and some small “cute Salt-Springy type” small businesses might be appropriate fronting onto the harbourwalk.

The following images are photos of boardwalks that the attendees thought might be suitable. It was also pointed out that there is a design for the harbourwalk that incorporates more First Nation art and culture. Other boardwalks that the attendees used as good examples of boardwalks/harbourwalks were Crofton, Bamfield, Lund, Tofino, Qualicum Beach and Powell River.

Community Consultation – Ganges Harbourwalk Upland  
Community Workshop



There was a segment of the participants who arrived angry. There was feedback from the community that the venue was too small, the question and answer period should have been allowed to go longer, and they wanted specifics from the proposed development (not understanding that there is no formal application). There were other people that thought there was likely a compromise between development and the harbourwalk, but they did not want to feel that they were being held hostage (e.g., no harbourwalk unless the development allowance is greatly increased). There were many people who were concerned about the environmental condition of the Ganges Harbour, while other participants recognize that this is an active harbour and needs to continue to operate. Some people wanted to ensure that the interests of the First Nations are recognized.

During the workshop, the facilitator and recorder assigned to each table collected comments on flip charts, maps and through sticky notes. All of the comments recorded at each table are provided in **Appendix A** of this report.

A comment sheet was provided to attendees (and to the people who had to leave due to lack of room). The comment sheet asked four open ended questions. Each comment sheet has been fully transcribed and all of the comments submitted are provided in **Appendix B**.

The facilitators and recorders endeavoured to capture comments and ideas from each table. The role for Dillon Consulting Limited was to facilitate this workshop, collect the information provided by the participants and provide this summary. It was clear that the community is passionate about this issue. Some community members felt that to present this controversial issue to the community so soon after the vote on incorporation was not appropriate.

Overall, the workshop achieved the goal of capturing the community views regarding the C7 zoning proposal and the level of development that would be acceptable to the community. While not a consensus, the majority of the attendees were opposed to the C7 zoning initiative. The majority were in agreement that the LTC should not be offering higher development density to obtain the harbourwalk. The attendees were in agreement that the Islands Trust should push government to take the statutory right-of-way from the leaseholders (even if that requires expropriation) and if that is not possible, the harbourwalk should detour around the properties and improve the sidewalk environment along Lower Ganges to connect to the northern segments of the harbourwalk. While the harbourwalk is seen as a desirable outcome, there was agreement that it should not be at any cost.

## 4.0 Community Input

While there was not consensus on some items, this section provides the general direction expressed by the 150+ participants on several themes. This section provides a summary of those themes and the general direction provided by the community.

### 4.1. Harbourwalk

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The attendees at the community workshop were generally in agreement that the harbourwalk would be an asset to the community. However, the attendees were mixed on whether it should be geared to residents, tourists or both. There was general agreement that while the existing portions of the Ganges harbourwalk does have commercial enterprises along the waterfront and the harbourwalk is well used by outdoor restaurants, seating areas, access to marine and float plane uses, the currently unbuilt portion of the harbourwalk should not have commercial development. There was little rationale provided as to why commercial on the harbourwalk would be unacceptable, with the exception that participants were concerned about competition with existing businesses, or that new commercial development would not be in character with Salt Spring. If there was to be commercial development it should be small and characteristic of Salt Spring type businesses (small, “cute” and support locals).

There was a lot of discussion about what the harbourwalk will cost. The community felt that they were lacking the information about what it would cost to build. While the CRD engineering assessments will start to provide that cost estimate, the community would like to see a full construction strategy. This will allow community members to decide if they want to fundraise, how the project should be phased and other implementation points that the community could become involved in.

### 4.2. Development on the Foreshore

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There was concern that the C7 zoning would allow for too much development (even though it provided no building site on the Cudmore property). The primary concern is maintaining the views to the water. Currently the Fishery and the Ganges Marina block the views of the water, and the only existing views are at the Farmers Institute parcel and the Cudmore property. The concern was raised that additional development or increased density will aggravate current traffic issues.

There was also the comment that the village does not need another coffee shop, but there was also concern that some of the current businesses struggle (particularly seasonally) and it might not be a good idea to increase competition.

### 4.3. Environment

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There are several environmental issues that were raised at the workshop including quality of water, light for marine vegetation, protection of spawning areas and revegetation of the natural shoreline. In addition, future sea level rise is a concern for island residents. The participants were also concerned that the existing fill in

front of the Ganges Marina is illegal and should be removed and reclaimed, and erosion reduced. There is concern that the current uses are polluting and industrial development poses risks.

The other environmental issue that was raised was the issue of limited potable water and sanitary sewer services. The feeling was that the environmental footprint should be what restricts development.

#### 4.4. Access and Views

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The discussion on access was divided into two categories: access from the foreshore to the water, and the right-of-way to construct the harbourwalk. The issue of the right-of-way for the harbourwalk was addressed by way of continuing to push the government to take the statutory right-of-way from the leaseholders, or expropriating the right-of-way or the lease, or waiting for the lease to renew (there was no information as to when that will be) and not renewing unless the statutory right-of-way is provided. The alternative or interim solution to move the pedestrian route up to Lower Ganges Road was raised. The Transportation Plan provides a strategy for upgrading the pedestrian realm on Lower Ganges by constructing sidewalks and separation from the traffic. There was also discussion about mobility and access for scooters and bicycles.

It was noted that the current built portions of the harbourwalk are in bad condition. The stairway at the northern end is derelict and access is blocked. The Farmers Institute has stairs that access the water. They have indicated they may consider this for public access to a harbourwalk if there is no commercial development. Access to the harbourwalk will be important. It was also noted that the house at the corner of Lower and Upper Ganges Road (115 Upper Ganges Road) that is on the waterfront should be purchased by the CRD to add to the public access to the water.

As previously discussed, views were a major topic of discussion. It was felt that protecting the views of the harbour was an important component of the character of Ganges. Development should not block the views.

#### 4.5. Culture

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Many people spoke in regards to the history, the culture and the traditional lands of the Coast Salish people and the First Nations' interest in the land. The community wants to embrace the First Nations culture and heritage, to incorporate their history in the interpretive and art component of the harbourwalk, and to work with First Nations.

#### 4.6. Process

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There was general agreement that while the harbourwalk is a community goal, the LTC should not be initiating the rezoning of the land. And, if the land is rezoned, the need to use the amenity zoning provisions of the Official Community Plan is critical. Several people did not feel it was fair to other people who have had to pay for their rezoning applications that the LTC is doing this without a formal application from the applicant paying for any of the work being done.

Several comments were related to the Islands Trust Mandate to “preserve and protect”, and that this initiative did not follow that mandate. Many people did not think that the C7 zoning initiative complied with the Official Community Plan. Other procedural comments related to the opinion that Bylaw enforcement is not enforcing current properties that are operating beyond their permitted uses.

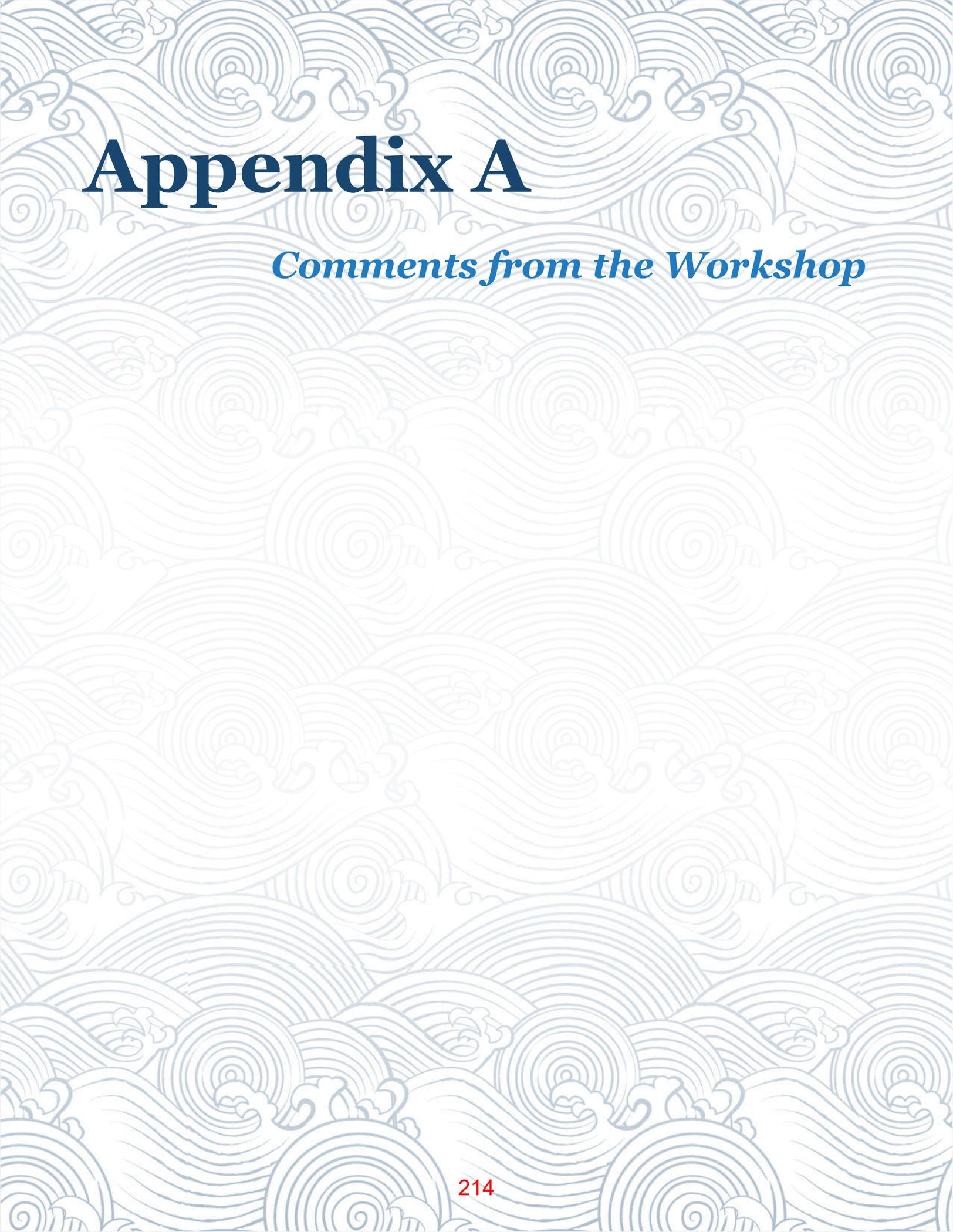
The participants at the workshop developed a set of principles for the Trust Committee moving forward:

- No development without First Nation support;
- Create livable communities;
- Maintain quality of life;
- Reflect nature and slower pace of life;
- Maintain existing viewsapes;
- Create a safe environment; and
- Provide community access.

## 5.0 Recommendations

Based on the input from the community at the Ganges Harbourwalk workshop held on November 4, 2017, the following recommendations from the community are presented here. Dillon provides no professional recommendations at this time.

- The C7 Zone should not be adopted.
- Actively pursue the statutory right-of-way through CRD and the Province; the rationale is that this is a community amenity and the leases should provide this access.
- If the government will not force the statutory right-of-way, wait until the leases come up for renewal and not renew the leases without the provision of the right-of-way, or expropriate the right-of-way.
- Hold another community meeting about the harbourwalk when there is some direction on the statutory right-of-way.
- Commission a final design of the harbourwalk.
- Prepare an interim harbourwalk plan, showing an upgraded sidewalk along Lower Ganges Road that would span the Ganges Marina to the Cudmore Gap and use that as the connector until such time as the statutory right-of-way is acquired.
- Work with the community to develop a construction plan and funding applications; the community may be motivated to fundraise for the construction of the harbourwalk.
- Deal with any applications submitted by the property owners and not take any initiatives to pre-determine development.
- Allow the development on the upland properties that corresponds to the current zoning.



# Appendix A

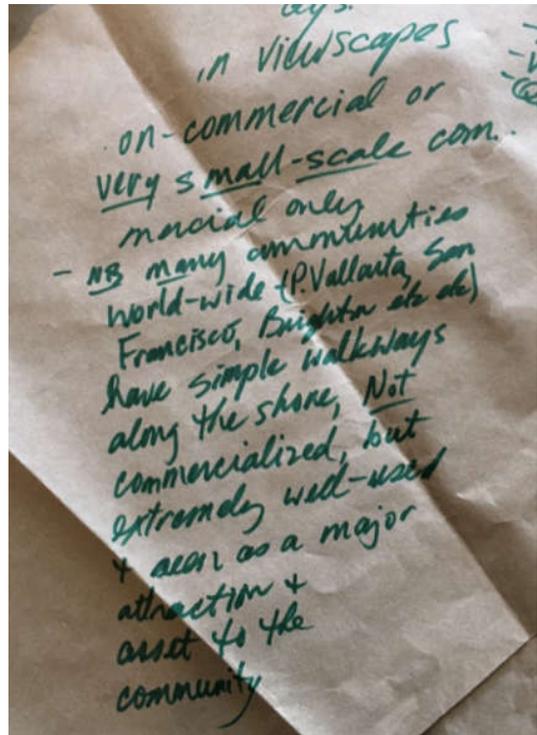
## *Comments from the Workshop*

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The following is a transcription of all of the comments provided through the table discussions. This includes comments written on the table paper, sticky notes pasted on maps, the flip chart pages, and other notes provided by the recorders at the Lions Hall on November 4<sup>th</sup>. The comments have been grouped into topic areas including: commercial development, environment, First Nations, boardwalk, residential development and access/views.

**Community Involvement**

- Need community involvement before the solutions
- Islanders need to make decisions
- Don't want to be held ransom
- Community feels like hostages – developer gets what they want or the boardwalk will not happen
- LTC has responsibility for public interest
- The developer should have an obligation to be a good citizen and be part of the community
- Any development should serve the community and not just private interests
- Grass roots process instead of Islands Trust initiated (bottom up)
- Will our opinions be heard, is this meeting manipulating us
- People had the impression that the engagement was to discuss the boardwalk not the development
- Vision would be clear view scape and small scale development
- Community should be allowed to investigate other choices

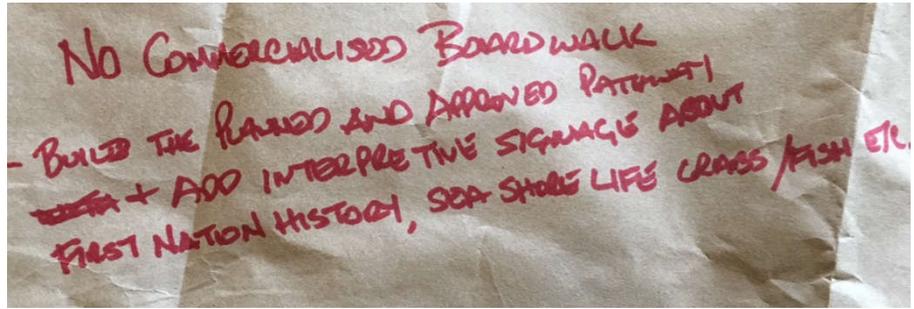


**Commercial Development**

- Non-commercial or very small scale commercial
- No commercialized boardwalk, build the planned and approved pathway and add interpretive signage about the First Nation history, sea shoreline life (crabs and fish)
- Regarding development – less is more
- Can the proposal go ahead without MARC consultation
- Any development should have respect for character and design that fits Salt Spring
- At present we have vacant businesses and shops – why do we need more that is “outside” the town?
- Pop-ups are good – food truck or cart are good examples
- Commercial on island should develop according to islanders’ needs

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- New generation goes to restaurant more. It's nice to see that we have just enough good restaurants in town
- Don't need the commercial
- Should not have commercialization and tying harbor walk to commerce
- Nothing more than already exists
- No development- worried about it being a slippery slope
- Commercial: some thought small scale along harbourfront is acceptable, others worried about too much competition
- No residential, no accommodation of any kind
- Community use as primary land use
- No industrial use- environmental implications
- Concerns re businesses dying along road if additional commercial uses are allowed on harborwalk
- Quirky and quaint storefronts would be okay
- The intervention of commerce is not necessary
- Make developers responsible for all inputs and outputs
- Needs to be expertise among the people making the decisions
- Ensure that the buildings provide their own water
- Don't build anything that exacerbates the resource issues that we have
- Parking should be removed off the infill
- Amount of fill is larger than Martin's Rainbow Road lot
- Have business owners all along harbourfront contribute to cost (they already do)
- Rent space along boardwalk in the summer to carts, seasonal vendors, non-temporary structures (not all in agreement)
- More retail will compete with already marginal businesses
- Take the market into consideration
- Need some economy for building
- Cute design – need to consider the character
- The developer is living in a dream world – the level of development is so out of scale
- The intervention of commerce is not necessary
- This is not an urban area – don't try and develop it as such
- Not residential on the waterfront, small scale business but not on the fill



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- Additional commercial will compete with current – vacant spaces, businesses currently close during the winter
- Cost of commercial space is too high (bike shop closed because rent was too expensive)
- No justification to remove commercial from the infill areas – need a long term vision – 50 to 70 years
- We need commercial in the most effective place, can't develop on ALR

***C7 Zoning Initiative and Zoning in General***

- Don't want to see upzoning of water
- Want affordable housing- what are benefits to community of C7 zone?
- Worried about precedence of allowing these properties new zoning- would other property owners along the harbor be able to intensify use? Turn all of the lots in question into park – the best area is the greenery. The ugly part is the businesses. Infill space should not be rezoned. No further densification. Property owners and developers were involved too much. The zoning bylaw needs to be adopted before building the boardwalk. Stop C7. We need one comprehensive Harbour Planning Board. Smaller commercial activities along foreshore. Minimum building footprint, no more infilling
- 2 storeys on Lower Ganges is OK
- If we give the C7 upland zoning to the properties I would be surprised if the properties were not flipped very soon thereafter i.e.: no development
- Don't need more fill in
- Return to a working harbor
- The C7 is very bad
- Amenity zoning – value is 75% of the increased value of the development
- Harbour is a community amenity
- Change the zoning to PR1 not C7 with a variance for what exists
- Marina uses such as marina office
- No large scale development
- No upzoning
- Love coffee shops, cute shops, benches on the boardwalk
- Make the upland nicer
- One storey on Lower Ganges
- Need Parkspace for easy access and a pleasant walk space
- Restore the fill area for a salt marsh
- Do not want to privatize the viewscape or the water front
- Do not want 3 storeys
- Nothing on the fill including parking (pollutants into the water)
- Small tasteful buildings would be OK
- Developer responsibility should start with the community amenities

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- Developer should protect and enhance marine environment
- Would like to see form and character design guidelines- rules and regulations on what can be built. We can have both but need a clear vision
- Nothing where the fill is
- Fill area should not be used for parking
- No concern with the development on the upland (they can develop to 2 storeys) but don't want development on the fill
- Appropriate uses would be marine oriented: office, chandlery, NO coffee shop, uses for residents year round
- Would reclaimed land be stable enough to support being built on?
- Expense of floating platforms to build on
- Any new development should address water, sewer, energy and design guidelines
- Need an honest rendering of what C7 would allow
- Should not fill in more foreshore
- Resource sustainability is valued over height and aesthetics
- Long term plan of development and boardwalk connection for pedestrians for Ganges
- Rain water capture and water conservation
- No infill of foreshore
- Do not renew the lease for waterfront and infill areas
- No parking lots
- No fossil fueled vehicles on site
- Beyond LEED standards, develop above utility, sustainability standards
- Need more robust sustainability regulation



**Environment**

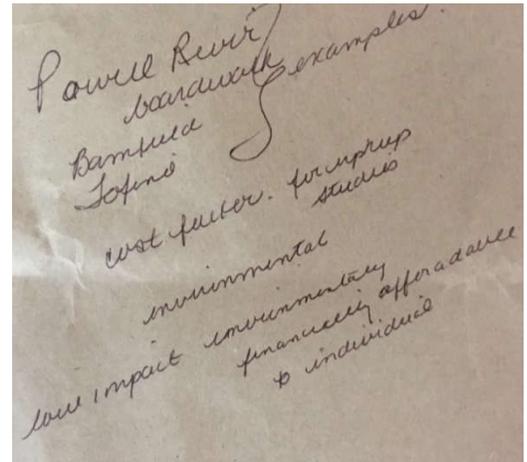
- Need to consider sea level rise
- Concerned about water use
- No development should have a negative impact on the environment
- Significant environmental impact
- Environmental impact
- Environmentally friendly and safer

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- Remove illegal infill
- Sustainable
- No infilling, sea level, changes to the foreshore are a worry
- Upland restored to native plants
- Infill should be grasses and natural vegetation
- Eco systems
- Enforce an open ocean environment
- Take the infill area out of the application
- Limited clean water on the island
- Natural concern
- Rise of sea level will influence the boardwalk and the development
- Trees along the bank
- Emphasize the natural restoration on the fill area
- Reduce erosion
- Consideration of marine life
- High tide boardwalk
- What does a restored shoreline look like – need some images
- Protect the marine environment
- High tide should belong to the community
- Riparian rights say you can't restrict public access
- Water needs cleaning
- Cause future fish habitat issues
- Current zoning does not allow for community uses
- Current industrial zoning is not good
- Concern about the capacity in the water and sewer system for new development
- Allow lots of trees and vegetation consistent with the natural ambience
- Incorporating local flora and fauna, rehabilitate the foreshore
- No habitable rooms in future sea level rise area
- Sea level changes
- Plant the foreshore with native grasses
- Boardwalk constructed to sea level rise
- Don't shade out the marine vegetation
- Design to allow light
- Respect nature and surroundings to embrace history of Salt Spring Island – interpretive signs

### First Nations

- First Nation collaboration
- If any development the style should be “Salt Spring” and incorporate historic and First Nations
- Embrace First Nations culture and heritage
- First Nations land, not just 4 properties
- Collaboration with First Nations – chance to do it right
- Unceded Territory – Grace Island example
- Respect OCP height, First Nations and public accessibility
- First Nation acknowledgement
- Archaeological permit – FN burial
- Filled over the beach values and the First Nations values
- Would like to see interpretive plaques that show names of local geographic features, First Nations place names, First Nations history, include sculptural elements- has to be done properly though, authentic, historic- not too touristy, Disney-like
- Need to engage the Salish community re ownership and treaty rights
- No development should occur without First Nation support First Nation interpretive centre



### Boardwalk

- Boardwalk is very expensive
- Who will pay for the harbourwalk
- CRD will decide the details of the harbourwalk, not Salt Spring
- Some good BC examples of a boardwalk are Davis Bay, Powell River, Vancouver Seawall, Qualicum, Crofton, Bamfield, Tofino
- Seawall/boardwalk with no commercialization
- No buildings except facilities (such as washrooms)
- Can the boardwalk be out over the water?
- The harbourwalk will provide a safe pedestrian route, will increase tourism and uses existing infrastructure
- Elevate the board walk out of the flood/sea level
- Boardwalk all along the shore, it is dark and ugly now
- Boardwalk is good in principle
- Who benefits from the boardwalk – tourism? Residents?
- Boardwalk over the water would be good
- There is a problem with the design of the boardwalk, does not seem to be accessible

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- No boardwalk on the 4 properties that are holding us for ransom. Put stairs down on property (Farmers Institute) otherwise, road path, stairs to shore access, and restoring remaining boardwalk by Peck Park and more stairs to shore like the walk way an stairs by the beach at Moby’s Pub.
- Boardwalk is a good idea, walkable but no massive development
- The word boardwalk has more commercial connotation, harbourwalk is more appropriate
- Love walk on Boardwalk but not a commercial one
- Harbourwalk is people oriented and friendly
- Sustainable development needs walkability
- Look at development and services close to where the people are
- Don’t block the view
- Need a pragmatic pedestrian plan for downtown and take emphasis off cars
- Natural boardwalk, no commercial
- Community is supportive of the harbourwalk
- If the boardwalk is to move forward, the vision of the property owners and the community don’t jive – accept the delay
- Boardwalk is a long term vision – walkways should be planned together with land use
- Don’t have to “sell our souls” for Boardwalk
- Ends veer to CRD walkway, roadside and back on boardwalk again
- See compromise – Salt Spring-y – infill site is a real opportunity
- No building between road and water
- Need to ensure long term maintenance of the boardwalk. Use existing pilings if possible (engineering studies will confirm)Give the boardwalk a “Salt Spring feel. Many communities world-wide (Puerto Vallarta, San Francisco, Brighton etc.) have simple walkways along the shore, not commercialized but extremely well used and seen as a major attraction and asset to the community. Boardwalk would increase safety on the roadside. Businesses can contribute to the ownership of the boardwalk. Build boardwalk over the marina. Finish it!
- Would be great to finish the boardwalk
- Go with original simple boardwalk design- done by Nancy Kreig ~1.5 years ago, White Pine Consultancy
- Incorporate art and sculpture
- A boardwalk similar to Crofton
- Need to get back to a natural water front and enhance the environment
- What is important: history (what was there before), Safety (connection for pedestrians), Natural Features, Environment, Respecting First Nations
- It would be great to finish the boardwalk
- Pros for the boardwalk include an extension to Rotary Park, access for the general public, partner with the Farmers Institute.
- Finish the boardwalk

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- Incorporate ocean and features into interpretive panels- info about marine life, etc.
- Need Boardwalk accessibility (including for scooters for nearby aging community)
- Need to think about the longevity of the development and the boardwalk
- Consider purchase of Lois Lane property (asking \$1.7 million)
- Promote pedestrians and bicycle use on the boardwalk
- Need low impact, environmentally sound and financially affordable to the individual
- Need a walk along the water, paths, public access, beautification, simple solution, viewscapes, public park
- Finish the boardwalk
- What other partners can the Trust work with
- We need artists renderings of the area and what would seem reasonable

***Boardwalk Cost Implications***

- Costs: can't afford all the bells and whistles, a low maintenance path would be cheaper
- Taxes and infrastructure costs
- Don't want to be held hostage to development- want pathway but not at any cost
- Path needs to be affordable now and into the future
- How much will the harbourwalk cost
- Islanders have already volunteered a lot
- We should have a referendum to see if the community is willing to pay to construct the boardwalk
- The community could pay for the harbourwalk with funding from CRD
- Boardwalk is good but at what expense

***Residential development***

- Would take a 7 storey residential building over a 3 storey commercial building
- Affordable housing – family multi-unit
- Cudmore Gap is zoned residential but there is no building site unless they fill
- If residences are approved, there should be a percentage as affordable
- Should not consider any residences on lower level due to sea level rise
- Aging – need to think about future (only off island?)
- Big need for variety of housing types
- Different people have different noise tolerance
- Small apartments for different people
- Mixed use is OK – more eyes on the street
- Consider small scale multifamily units so people could have walking options to the grocery store

***Road and Transportation***

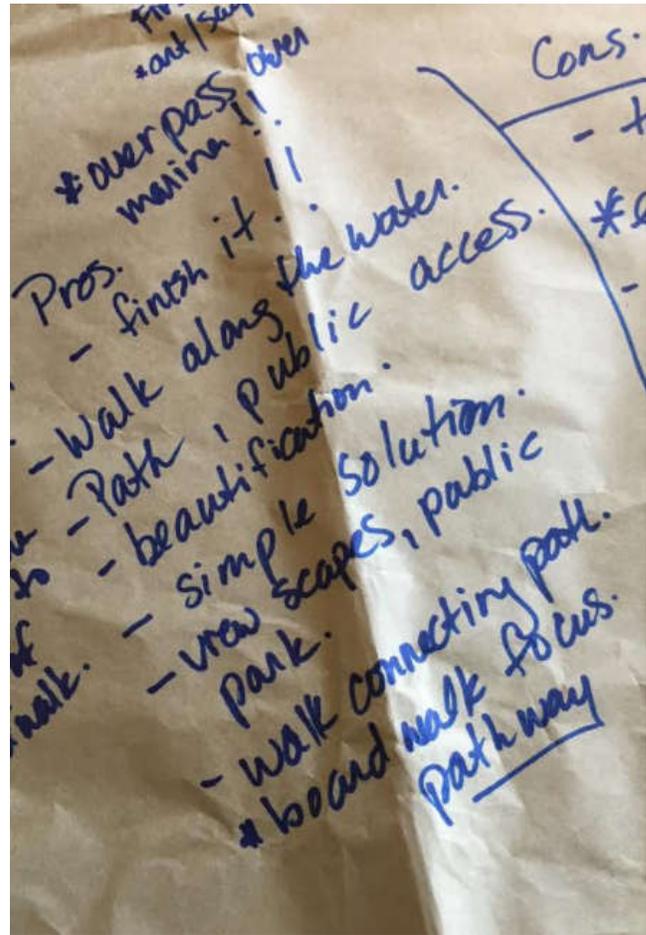
- Transportation planning is in isolation from land use
- Safe public and vehicle access along the existing road
- Have the discussion about shutting down Lower Ganges road and finding a bypass to improve pedestrian safety
- Sidewalks in the area (on Lower Ganges) – there are none
- Develop Ganges around Hereford and Rainbow Road
- Side walk
- Boardwalk can go around Fishery
- Bikes?
- Develop the harbourwalk or sidewalk around the properties
- Enhance the sidewalk on Lower Ganges and not worry about the Harbourwalk

***Access/Views /Statutory Right of Way***

- Beautiful views into the harbor
- The stairway at the north end is derelict
- Ask firefighters about what is safe re them accessing someone who has a heart attack, for example
- Access points shouldn't be every 40 feet- need to determine what is needed and practical
- Need to consider mobility, scooters
- Has to be safe
- CRD said they would not pursue the Statutory right of way without the permission of the owner
- Look at expropriating the right of way
- What about expropriating the land above and lease back to the present owner under the restraint that they keep the property in a "good and suitable" state
- A boardwalk can be built in other ways. The CRD can expropriate property and then pay the landowner a fair price for the land expropriated
- Our family would really love to see a harbourwalk along the shoreline, along with positive commercial development. It is such a beautiful part of our Ganges Village and we'd love to see more access as well as active business presence there. Thanks for all your hard work on this.
- I like Jason's idea to re-approach the BC ministry to consider granting crown access without requiring the upland owner permission. It de-couples the harbourwalk from the development
- Access can be provided through the Farmers Institute land
- Public access to clean harbor and water
- Complete plan to retain 10 m setback (open air space) on the infill area for the 4 properties
- Waterfront should be for people and boats – not cars
- If we don't get property owners permission, do we have a fall back plan
- Want accessibility

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- CRD could expropriate the land
- No development that blocks the view of the harbor
- Work with the crown to get the Statutory Right of way
- CRD should buy the house (Lois Lane) that links the boardwalk parcels
- Need to work with the CRD and province
- Sunrise, moon views and ocean breeze
- Want access for public
- Look for other examples such as Port Alberni
- Question about bicycles being allowed on the path, has implications for width, budget, environmental implications-use design standards
- Why can't the statutory right of way for the boardwalk be expropriated
- Public access to the waterfront, walkway interconnected with other pathways
- Maintain viewsapes
- Should be connecting paths from Moat's to Moby's
- Traffic would be safer if boardwalk in place
- Would like to know cost of a boardwalk- access issues aside, if the community were to fundraise, how much is needed?
- Pedestrians along the roadway, transportation
- All public park along there- have CRD buy it (or buy it from them?)
- A nice stroll from downtown to Moby's
- Generally people like the existing boardwalk in the village but do not want new development
- Stairs to access
- Need sustainability
- Protect the views from the water
- Want to keep existing view
- View corridors, view scapes , breaks and setbacks
- Better development to allow connection
- Need connectivity from other side of Lower Ganges
- Connect to the ocean through view corridors



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- Like to see the sea from the village – not only on the waterfront but from the road
- When foreshore leases come up for renewal, require the “owners” to provide access as a condition of renewal
- Need to find a way around the owners
- Keep views open
- Buy the house between the Ganges Marina and Moby’s for public access
- CRD get the statutory right of way

***Legislative Implications and LTC Role***

- LTC should ensure amenity zoning regardless of who applies for the rezoning – it should be on the developer
- Bylaw enforcement is not enforcing current properties that are operating out of capacity
- LTC is trying to be proactive but is proceeding without resident approval
- Principles for the Trust committee moving forward should include:
  - No development without First Nation support
  - Livable communities
  - Maintain quality of life
  - Reflect nature and slower pace of life- not tacky tourist town
  - Maintain existing viewsapes
  - Create a Safe environment
  - Provide Community access
- Make the Harbourwalk accessible for mobility challenges
- Islands Trust should not be initiating this rezoning for the developers
- Always remember the Islands Trust mandate to “preserve and protect”
- Why do other landowners have to pay to rezone their land – these owners are getting it for free
- Keep it simple and get it done (or not) – we need a decision
- LTC has forgotten their mandate – they should not be facilitating development
- Read and follow the OCP
- Do not reward developers for previous illegal activity
- By rewarding scofflaws with zoning to complement their illegal filling, the Trust makes the Trust unable to govern

## Community Consultation – Ganges Harbourwalk Upland Appendix A – Comments from the Workshop

- Wait til the election next year of CRD and Trustees and make that a focus
- Read the OCP – this document represents the vision of Salt Spring Islanders and specifically states to building above 3 storeys. At this point on, elected Trustees and CRD Director had a clear direction from this community to refuse to go further with this rezoning application. This exercise is being put forth to tie in a public interest in a boardwalk as a carrot to approve the rezoning. How much money is this “consultant” being paid for a meeting this is a poorly disguised attempt at co-opting the OCP. How can our EO’s sanction the expense of all the paperwork, coloured dots, coloured pencils, hasti notes and other childish materials used to promote the idea of “public consultation”. This exercise was unnecessary – both financially and time spent. This community is not stupid.
- LTC driven process causes problems
- Islands Trust Policy statement 4.4.2 – water quality and levels are very important when talking about increasing density
- Proposed sidewalk along Lower Ganges should go ahead even if the boardwalk does not
- Provide disabled access
- The LTC process is bad and encourages greed
- Uses appropriate are marine office, laundromat, chandlery – more commercial will challenge the existing businesses
- Suggest you come to a no decision now before dragging this on or spending more money and taking more staff time.
- No tit-for-tat development – no pressure on the Islands Trust
- No linkage between the desired development and the boardwalk

**Questions about draft bylaw 491 - to up-zone properties on the shore**

1. **First Nations** - Has the Local Trust Committee acted to accommodate the wishes of First Nations? The Lyaekson and Hahali bands have written to the LTC saying they do not object to the boardwalk over the water but do not support additional development on the shoreline.
2. **Why are other options for the boardwalk not being discussed such as:**
  - A non-commercial boardwalk
  - Purchase of shoreline properties
  - A boardwalk over the water
  - Not building the boardwalk
3. **View Corridors** - How does the proposed bylaw protect view corridors? One of the most picturesque qualities of Ganges is the views of the Harbour.
4. **Water** - Why is the Local Trust Committee up-zoning within the NSSWD service area, when NSSWD doesn't have the water to serve it? The NSSWD will only hook up one unit to properties that were not yet connected but have been paying parcel taxes to the District.
5. **Affordable Housing** - How can the Local Trust Committee consider creating demand for water for commercial use when NSSWD cannot provide water for affordable housing - the island's biggest need? A large affordable housing project is held up because NSSWD doesn't have the water to serve it.
6. **Cost of a boardwalk** - Why is the public being asked to discuss a boardwalk when we haven't been told the cost of building and maintaining it or who will pay these costs? Almost anyone would like a nice boardwalk. But like anything we want in life, cost is a critical consideration.
7. **Walkway by road** - The Transportation Commission will be building a walkway - already approved - by the road along the shore. In view of the multitude of problems with building a boardwalk, why isn't a roadside path sufficient?
8. **Shoreline Ecology** - How will more development on the shoreline affect shoreline ecology in the harbour?
9. **Tourism** - Are there studies to show that a boardwalk will promote tourism? A letter from Li Read on the trust website says we need the boardwalk to attract tourists. A letter from a private citizen is NOT evidence. Shouldn't the focus, and any public spending, be on attracting more tourists during the off-season?
10. **Fair benefit to the community** - Has a study been done to determine the dollar value of the increased property value that owners would receive from the proposed up-zoning bylaw? Normally when up-zoning is given to landowners, a community amenity is received in exchange and a study is done to determine if the value of the benefit to the landowner does not vastly outweigh the amenity received by the community.
11. **Consistency with OCP** - How is the bylaw consistent with the OCP. Have the studies been done? Section B.5.1.2.5 says it is the policy "to retain the compact and pedestrian oriented nature and economic viability of island villages, the Local Trust Committee should only consider rezoning applications that would create more commercially-zoned land in Village Designations if the following guidelines are met: ie "existing commercial zones are largely developed to their practical development potential and there is evidence of a community need for additional commercial land and "the Local Trust Committee has undertaken a review and inventory of existing commercially zoned land and development, assessed existing and future projected demand, and on the basis of this assessment, may consider re-designating and rezoning land for additional commercial uses.

any public spending, be on attracting more tourists during the colder months?

**Fair benefit to the community** - Has a study been done to determine the dollar value of the increased property value that owners would receive from the proposed up-zoning bylaw? Normally when up-zoning is given to landowners, a community amenity is received in exchange. Normally in order to know that the value of the benefit does not vastly outweigh the amenity received by the community, a study is done to put a dollar value on the benefits given and received.

**Consistency with OCP** - How is the bylaw consistent with the following guideline in the OCP -  
Section B.5.1.2.5 says it is the policy "to retain the compact and pedestrian oriented nature and economic viability of island villages, the Local Trust Committee should only consider rezoning applications that would create more commercially-zoned land in Village Designations if the following guidelines are met:  
Subsection "B" states the criteria as "existing commercial zones are largely developed to their practical development potential and there is evidence of a community need for additional commercial land. What is that "evidence", and please be explicit: has there been a study?  
Subsection "E" states that "the Local Trust Committee has undertaken a review and inventory of existing commercially zoned land and development, assessed existing and future projected demand, and on the basis of this assessment, may consider re-designating and rezoning land for additional commercial uses. Similar question: Was a formal assessment done? What is the evidence? Please be specific.

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- Use of the leverage of the lease expiring to get the access
- The LTC should consider the dangers of too much development to Ganges and Salt Spring as a whole.
- The LTC should consider alternative ways of developing a boardwalk without upzoning
- Kill C7 at second reading and wait for the developer to make a real application
- It is not worth spending a lot of money on or making concessions to developers
- OCP Section B5.1.2.5 – before considering a new bylaw there are required and review assessments

***Who are we building this Harbourwalk for?***

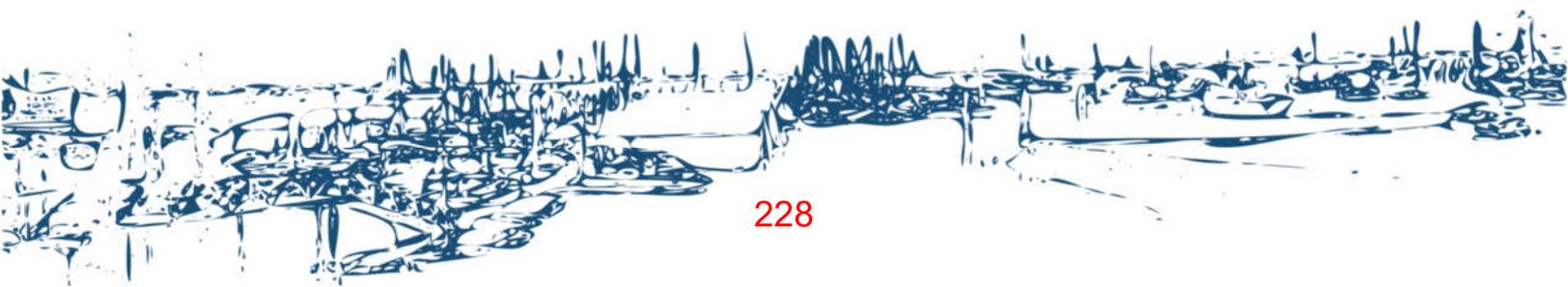
- Inviting for commercial and residential
- Do we have enough visitor market
- Look at whole community but not certain peoples profile
- Too many tourists on the island
- Think about future and next generation
- Too much focus on weekend visitors – not locals
- Family friendly

***Other General Comments***

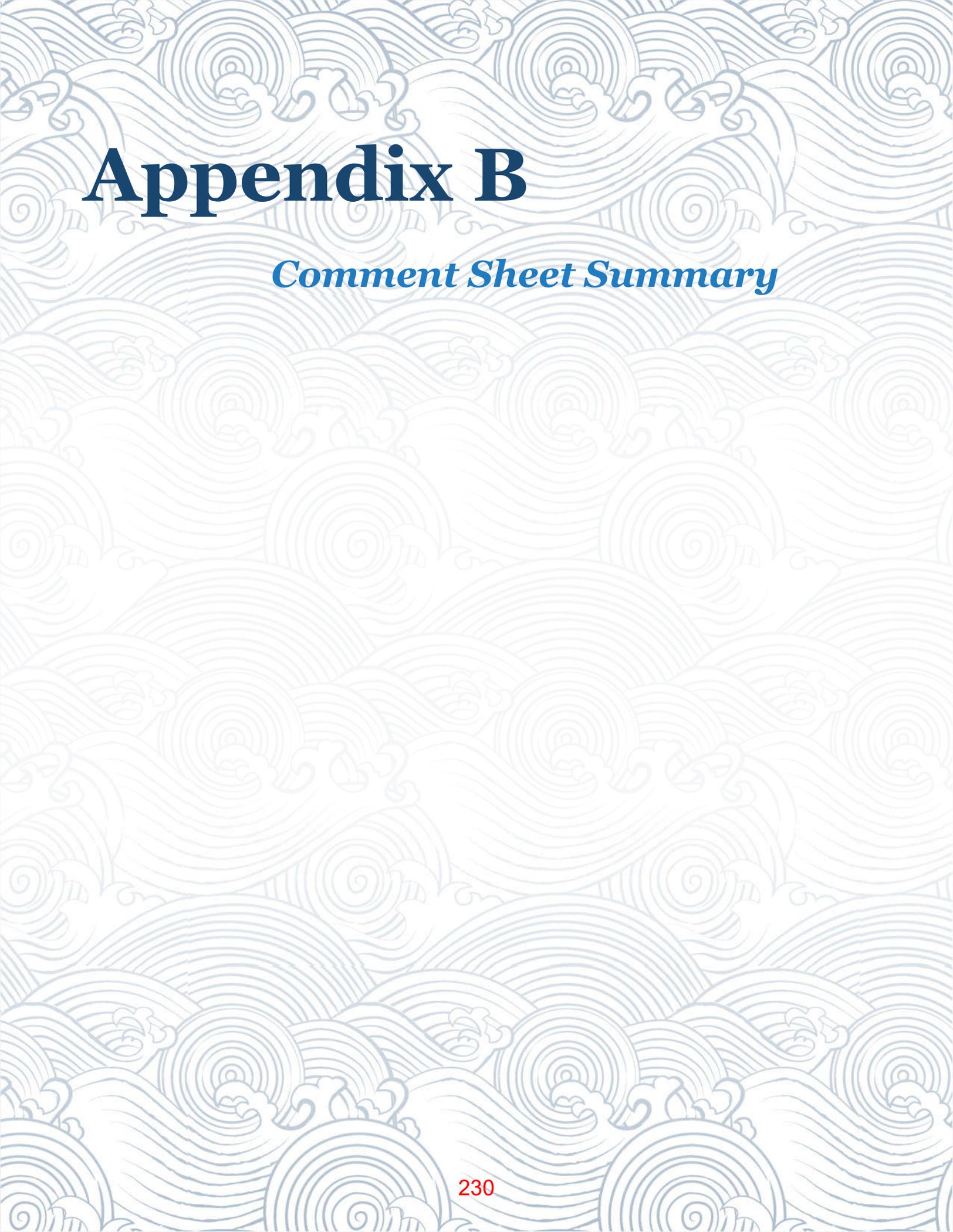
- I believe commercial rather than aesthetic interests are motivating this initiative. Our decision on how we want our community to look should be based on other concerns than those of a few commercial property owners in Ganges
- I don't think it's worth the price of upzoning and the additional density. A boardwalk would be a nice amenity but it's not crucial. If we overdevelop our island, we will lose the island ambience we now have and love. This suggestion is not consistent with Salt Spring
- Completion of the boardwalk is not sufficient justification for increasing development along the harbour
- Keep in mind that any commercial development along the undeveloped part of the harbor should be kept to an absolute minimum public access to the harbor views
- Clean marina and friendly access
- Businesses are ugly – there should be no buildings
- Harbour appearance is an embarrassment
- Marina building ideas – leave as they are – legal non conforming
- I am strongly opposed (to the commercial development) if that greater development potential holds any prospect of affecting the existing views of the harbor. Trading the existing view, enjoyed by everyone, for a walkway enjoyed by relatively few, would not be something future generations would be able to comprehend, let alone support
- Example of a very non intrusive boardwalk is in Lund BC. It appears to be built on the high tide publically owned shoreline. AND, how they helped pay for it...people donated \$ for the boards thus their names are recorded on them – all the way along – we could do this without giving up our connection to the harbor

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Appendix A – Comments from the Workshop

- Concentrate development of the harbour over by gasoline alley
- It feels like it is being driven by a small group despite the lack of support from the community. Trust time should move on to other greater issues in the community, like STVR's, illegal tree cutting, more protection for eagle/osprey/heron nest site. CRD should focus on water/sewer issues, so much work needs doing.
- Clear waterfront, wide walkways, chairs and tables







# Appendix B

## *Comment Sheet Summary*

Community Consultation – Ganges Harbourwalk Upland  
Appendix B – Comment Sheet Summary

This section provides a summary of all of the comment sheets submitted. We have transcribed the comments to the best of our ability given that they were mostly handwritten. We have not corrected grammar or facts. We provide these as submitted by the community members.

**1. What do you think of allowing greater development potential on the properties adjacent to the proposed Ganges Harbourwalk if doing so could lead to the Harbourwalk's completion?**

- I am strongly opposed if that “greater development potential” holds any prospects of affecting the existing views of the harbour – which of course it probably would. Trading the existing view enjoyed by everyone, for a walkway enjoyed by relatively few, would not be something future generations would be able to comprehend, let alone support.
- Allowing any greater development would sacrifice the atmosphere of laid back Salt Spring community. We could/should stop a harbourwalk development
- Absolutely not!
- I do not support greater development potential on the properties adjacent to the Ganges Harbourwalk to be able to complete the boardwalk. I am satisfied with the pathway that is already planned for “leaving foreshore, natural as possible
- Completion of the boardwalk is not sufficient justification for increasing development along the harbour
- I don't think is worth the price of the upzoning and the additional density. A boardwalk would be a nice amenity but its not crucial. If we overdevelop our island, we will lose the island ambience we now have and love. This suggestion is not consistent with Salt Spring.
- I don't think it will be well supported if it is large scale.
- I am opposed! Much as I'd like a boardwalk, it is not at any price. This is blackmail without a blackmailer.
- I think you should start by calling it what it is, namely upzoning certain properties in return for a right of way. This is a terrible precedent and will inevitably lead to all kinds of “me too” requests from other developers. The two issues – upzoning and the harbourwalk – are unrelated. I think this zoning should be left as is and negotiations be based on the current zoning plus moving forward with expropriation or other options for completing the boardwalk, or indeed, not bothering with it at all.
- While I totally support the boardwalk, I totally oppose the intrusion of development on a massive scale along the lines of the marina proposal as being far too high a price to pay. The property owners adjacent to the boardwalk dream should have no power whatsoever over the project. Allowing massive commercial development along the harbour front in the heart of Ganges Village would be a betrayal of the Trust preserve and protect mandate, it would impede the view corridor, it would be out of scale with our rural island community.
- I do not agree with this type of development for the Ganges Harbour – for many reasons. Visually it would destroy our little harbour and town. Environmentally – in everyway not a good idea. Only works for those who will profit financially.
- I think it's a bad idea.

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- The harbourwalk is a long held community wish, not a carrot on a stick for development of a narrow business interest out of step with the OCP. The Official Community Plan notes: “the LTC will support the development of the Ganges Harbour Management Plan” and “the LTC should consult with the users of Ganges Harbour, upland owners, and other levels of government to develop a more detailed plan for zoning of the water surface of Ganges Harbour which is consistent with an approved Harbour Management Plan”. I can’t seem to find Salt Spring Harbour Management Plan. If there is none, then why this piecemeal development to get the harbourwalk, with no broader vision and concerns in mind? Please use this opportunity to delay this decision, create a Harbour Management Plan, then consider revising the zoning for Ganges Marina.
- I do not think that the benefit to the community of having the boardwalk completed is in any way proportional to the benefit of increased waterfront development potential would be to the land owners involved in this scheme. The value of that waterfront property would increase by millions of dollars before any work was done. Ganges Harbour would become another anonymous, overbuilt eyesore, like most seaside towns that value money over all else.
- I agree that property owners should be granted some development potential on landfill above the low water mark (0.0 Datum). This should only be granted following construction of a boardwalk to be opened to public use at all times and connecting to existing structures for the same purpose (to east and west of their properties). Infill harbour limited to east of the Ganges Marina ramp only. The public highway should be impacted to a minimum by the development giving priority to public uses including sidewalk, bike paths, public transit, and on street parking (in that order of importance). Character and scale of buildings should conform to the rest of the harbour and village.
- Just the Harbourwalk/Boardwalk would be my choice – of, if the property owners do not cooperate, just a sidewalk joining Rotary Park
- I suspect that natural environment of the harbour has already been compromised by the existing level of development. A thorough evaluation of existing water quality, sediment quality, BIOTA and habitat and circulation needs to be done to inform any new proposals. The illegally filled lots should remain zoned water and any development proposals should be based on existing zoning with amenity provisions. A decent roadside walk would be preferable to more density.
- I am opposed to allowing commercial etc development adjacent to the harbourwalk. No rezoning for so many reasons. This is referendum arguments all over again. A nice sidewalk would be just fine.
- Sounds like blackmail – being held hostage to a few so that the many can not experience an unencumbered boardwalk. Answer in short – NO
- I would rather see the Harbourwalk never completed than have this development.
- Very much opposed. This looks like yet another attempt to “Whistle-blow” Salt Spring for the benefit of the greedy few.
- I do not think the boardwalk completion compensates for destroying the views at the head of the Ganges Harbour. Any development should conform to the OCP which means lower buildings and

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vegetation preserved. I say forget the boardwalk until there is a plan in place acceptable to all SSI residents

- Not a good idea to trade off the charm and ambience of the waterfront for 3 storey buildings. Any buildings along the water should be designed to minimally interfere with the waterscape view
- Everyone would like the boardwalk to be completed but the greater development that is being proposed is not worth the trade off – the boardwalk should be for the town, not the marina – we should be very careful about what can be permitted as it will be there forever – the boardwalk plan can wait for now – instead of drafting amendments to the OCP other alternatives should be looked into first – such as obtaining the statutory right of way from the Crown to help this project proceed
- Opposed
- I strongly disagree with allowing any greater development potential to the properties adjacent to the proposed Harbourwalk.
- It seems a bit like selling your soul. Since 2005 when the Province and the CRD said no progress without consent of the marina etc., things have changed, hopefully, revise that please.
- I am not in favour of greater development potential adjacent to the proposed boardwalk – low development on currently infilled area OK, remainder rehabilitated natural area. Small physical low impact along Lower Ganges is OK
- I am very opposed to allowing greater development potential to complete the Harbourwalk. One of the greatest amenities of Salt Spring is its natural shoreline, features and wildlife so why would we sacrifice one amenity for another. It is a little like the Gift of the Magi. The questions the Trustees should be putting to the islanders is: Given the harbourwalk will cost x amount to finish and then y to maintain (both essential information which needs to be provided) do you want to continue or not? If we do want to build it, how do we want to finance it? Taxes, grants, fundraising etc? Show us the options and encourage islanders to come up with their own suggestions. The draft bylaw proposal brought forward to accommodate an early proposal from 2005 is frankly divisive to islanders that voted resoundingly against development. My specific objections to increased development lie in several areas:
  - Directly where the development is proposed was the site of a recent herring spawn, one of the most important forage fish in the Salish Sea. Herring are essential food for salmon which is the main diet for our endangered Southern Resident Killer Whales which we have a legal obligation to protect. These intertidal ecosystems rely on soft, natural shorelines that release a steady supply of nutrients as well as control run off and prevent siltation. Hard shorelines provide none of these ecosystem services.
  - These areas have cultural significance and First Nations have already registered their objection. Trustees might need reminding that First Nations are not stakeholders they are RIGHTS holders and besides Trustees having an ethical obligation to respect these concerns, we also have legal obligations. Saanich successfully fought the Saanichton Marina project on the basis of their

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- Douglas Treaty rights. The court evidence should also be an indication to trustees of the significance of these rich marine estuaries for wildlife and cultural practices
- The harbour is already at carrying capacity in every criteria. The trust has got to start considering the cumulative impacts of development on water quality, biodiversity and recreational use. We are shortly to be killing the golden goose.
  - The Experience Gulf Islands Initiative of the CRD and other business groups are already exploring a trail network that could be part of the regional park system. I would like to see the boardwalk evaluated in this context with some analysis of whether we need to adjacent trails or could combine them into one and contain the maintenance costs
  - Why with climate change upon us are we messing around with a fragile shoreline with increasing storm events and rising sea levels? Makes no sense and just invites greater liabilities for the community in the future.
  - I think the vision of Ganges with a built shoreline is the antithesis to the vision of the islands as we market them to visitors. We are not Europe, we are supernatural BC – I strongly believe that we should be expanding the conservation bylaws not decreasing and that we should be incorporating language concerning Douglas treaty rights directly into the Act and Bylaws

**2. *What would you like to see the Local Trust Committee to keep in mind as it considers whether or not to enhance the development potential of properties adjacent to the Ganges Harbourwalk as a means toward Harbourwalk completion?***

- I suppose property owners have some right to development of their properties to their wishes, within the building regs etc.
- No carrot...we don't need the boardwalk completed there – pursue other options for the boardwalk in the water or forget it. Put a sidewalk on Ganges
- They will never be forgiven
- That most of the community doesn't want it and put our tax dollars to better use
- Keep in mind that any commercial development along the undeveloped part of the harbour should be kept to an absolute minimum but maintain public access to the harbour views.
- The LTC should consider the dangers of much development to Ganges and Salt Spring as a whole. The LTC should consider alternative ways of developing a boardwalk without upzoning. A boardwalk can be built in other ways. The CRD can expropriate property and then pay the landowner a fair price for the land expropriated.
- I think a decoupled process will get more local support if it is an option.
- Fairness – remember to put preserve and protect first. Low rise moderate development only. View corridors retained.
- The LTC should keep in mind that their mandate is to preserve and protect and not to facilitate commercial and residential development. Especially on a hot button issue like Ganges waterfront

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development and especially by attempting to do it by a new zoning which could be a “wedge” for every other property.

- The boardwalk dream should be kept completely separate and apart from any proposals related to development of the properties fronting it. The LTC should pursue discussions with the province in search of an alternative to make the boardwalk development conditional on the approval of the adjacent property owners. After 30 years of dreaming about this basic quality of life project at what point do we give serious consideration to expropriation
- Keep in mind that the attraction to locals and tourists alike resides in the fact that the area remains very natural and undeveloped
- We don't need to enhance the development of the harbour – leave it alone
- What is a marina? A floating RV park, using “free” ocean parking. Moreover, boat motors – they all have them, even sailboats – requiring many times the liters/km of fossil fuels as land motor vehicles. They increase our carbon footprint excessively. Increased wharfage and shoreline buildings to serve relatively wealthy clients runs counter to development in accord with the “predominant features of SSI... natural environment, rather than manmade structures”. Boaters are tourists, of a highly seasonal kind. Salt Spring is to be developed for the year round well being of residents. Marina staff will increase in peak season, then drop next to none in the winter. How does this help Salt Spring thrive as a year round community?
- The boardwalk was built many years ago – so long that some parts of it have been rebuilt. Although considerable funds and effort were invested, it was never completed because two landowners – the marina and Mr. Cudmore – wanted special concessions from the community in the form of permissions to develop and increase density on their properties. They would allow the boardwalk to cross their properties on the condition that these concessions were granted. Keep in mind that the only reason that we have not had the benefit of a completed boardwalk for all these years is specifically because waterfront landowners refused to allow the completion of this community asset unless they were granted permissions that would not be available to another applicant. To grant “greater development potential” to these landowners to reward them for holding our boardwalk to ransom for years and years.
- The boardwalk concept was primarily to give public access to appreciate the natural beauty of the cliffs and foreshore, west of the Ganges Marina ramp, around the harbour to Harbours End beach beyond Moby's. View corridors to open waters of the harbour should also be created and preserved (ie: not wall to wall marinas). Automobile rental demands too much space in such constricted sites. Scooter rental might be feasible, without spilling over into the public highway parking. I am pleased to see the discouragement of personal watercraft on any of these sites. The height restriction of buildings should adhere to existing bylaws, and principles of design in the OCP also applied. View corridors from street (and Mahon Hall) level preserved as much as possible. Amenity zoning – addition to land to Rotary Park

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should be considered eligible – in proportion to reasonable valuations. “Boardwalk at any cost” – certainly not

- The people’s choice is more important than a developer or developers choices and should be seen (as today at this meeting) and respected, even acted upon by our politicians. The rights and preferences of the aboriginal people representing this part of their land should be respected and acted upon. Do not give \$ or amenities to those who illegally filled the water – no paper/bureaucratic process, no agreement for them to have any power
- Preserve the natural environment, acknowledge a priority for low cost housing over high end condo/hotel space, acknowledge First Nations objections, CRD could appeal to the province in public interest for a ROW across the waterlot leases without giving away development rights, what is the role of the SS Harbour Authority
- Riparian protection, traffic, parking (not underground), water quality, high end housing is wrong, 1<sup>st</sup> nations positions, we do not need another Grace Islet, and that’s what will happen if you rezone. Other island priorities – affordable /low income housing, laundromat, bike lanes, support for local ferries
- I would like them to remember that they wasted 6 productive years of possibilities by working on incorporation. Trust is not there for the Local Trust Committee and CRD anymore and the “no” vote re incorporation is a good example of why. So keep in mind that the majority of islanders want measured growth that is not tied to the boardwalk. The cost is too high
- I do not believe this should ever be done. We do not need underground parking. We don’t want this to become like Whistler
- The fact that Salt Spring’s charm is a fragile commodity. It won’t withstand gentrification abuse
- Visual access is critical. Who wants to drive along Upper Ganges Road with the view to the harbour cut off by tall buildings on the foreshore? Or arrive by sea with the head of the harbour a mass of buildings? Any development should reflect the form and character of the surrounding area – low buildings, small shops, plants etc. Environmental protection of the harbour ecosystem – so do not allow any infill that is not already allowed (and not even that if possible)
- Do not cave into pressure to overbuild on this site
- The only historic building in Ganges is Mahon Hall and the views from it should not be blocked anymore than it already is now. That the present property owners should not benefit more than the residents of Ganges – the boardwalk can wait until other options have been carefully studied. We should not proceed with the bylaw change proposal at this time until all other options have been exhausted. A referendum should be held when a plan is in place – this is a serious and permanent decision and involves all taxpayers
- A small number of landowners have been holding the community hostage over the harbourwalk completion for decades. It is not appropriate in my view for local government to provide benefits beyond expropriated value.

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- No deals! We don't need the harbourwalk that much. Stick to the OCP. The First Nations have not been consulted. There has been a history of illegal filling. Our local trustees have no credibility right now, after their behaviour regarding the incorporation. They should be replaced before any further decisions are made. The gorse needs to be removed from the Marina land, at the marina's expense
- The harbour and potential walkway/boardwalk are community amenities not source of income for a few businesses
- Keep to current OCP unless emergent unforeseen situation develops and then only adjust OCP if strong community support for changes

**3. Is there anything else you would like to say about the Ganges Harbourwalk project?**

- Improve the sidewalk alongside of Highway
- It's a no go
- It feels like it is being driven by a small group despite the lack of support from the community. Trust time should move onto greater issues in the Community, like STVR's, illegal tree cutting, more protection for eagle/osprey/heron nest sites. CRD should focus on water/sewer issues, so much work needs doing
- I was in Bamfield recently where an excellent boardwalk has been built by the community. Commercial development is limited to the start of the boardwalk and is kept to an absolute minimum. Private homeowners share the boardwalk with visitors. It is several times longer than our proposed walk and is a major asset to the community.
- I believe that commercial rather than aesthetic interests are motivating this initiative. Our decision on how we want our community to look and to function should be based on other concerns that those of a few commercial property owners in Ganges.
- I like the idea to re-approach the BC Ministry to consider granting crown access without requiring upland owner permission. It decouples the harbour walk from development.
- Suggest to you come to a NO decision now, before dragging this on or spending more money and taking more staff time.
- It is not worth spending a lot of money on or making concessions to developers
- I totally support the project as a long time dream of SSI. I would happily see my taxes go up to help pay for it. But upzoning to allow a 4 storey hotel is to be held to ransom by a non community minded developer – much too high a price to pay. Similarly in the case of the Cudmore request for density elsewhere our Trustees should never have given serious consideration to such a concept. The two current trustees should defer on consideration of the project until after the 2018 elections
- Having the boardwalk completed would be an asset to both town and the island. Was on design panel 10 to 12 years ago when this idea was floated – instantly, developers came with the BIG (\$\$\$) ideas. Don't let them win.
- Let it die

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- The harbourwalk needs a view on both sides. It doesn't need more boats in the harbour and a wall of insular, boat serving development on the land side. We need to be enhancing the nature of harbour's end, not trading it off for a wooden walkway. This means raising the profile of First Nations use of the Harbour well beyond the interpretive panel on the present kiosk facing the harbour. We need more direct First Nations involvement in planning and designing the harbourwalk. We need a Harbour Management Plan that ensures this, not only for getting a harbourwalk on Ganges Harbour, but for future developments on other SSI harbours. The harbourwalk will, I'm told, still need to straddle the Cudmore Gap, and the third generation of Cudmores, now in charge, will be looking for generous trade offs to help us realize the harbourwalk dream. In light of these driven, narrow interests, let's design a harbourwalk that goes out in the water and up in the air to avoid such bargainings. The majority of SSI residents, I'm quite certain, would rather spend considerably more money to get a boardwalk that preserves the foreshore, welcomes First Nations home to it and doesn't play into personal and corporate gain
- Build as a sidewalk. A real sidewalk like grown up cities have. A curb to protect pedestrians from vehicles. A vehicle access to each property it crosses. Start at the Thrifty's liquor store and end it at Moby's corner, or better yet, continue it to link up with the sidewalk further up Lower Ganges Road. Decide where parallel parking is appropriate and install appropriate signage. Abandon the Harbourwalk fantasy completely, because we've never had it, and it's not worth compromising the Ganges waterfront to get it. The question as framed does not address the main concern – namely the offer of "greater development potential" has the potential to substantially alter (diminish) the relationship between residents and visitors and the Ganges waterfront. This is called "Bylaw 491". I expect you heard this message loud and clear (if you were listening) at the Lions Hall meeting. The Town as it presently exists is a part of "the commons" in that most of the structures and activity that exist are there by permission of the community. The landowners who want to develop their waterfront properties can do so with permission granted by Bylaw 491, so the main question here is about the bylaw and the bit about the completed boardwalk is a distraction from the question. There is effectively NO Land at the end of Ganges Harbour. Building multiple residences, rental units, retail shops, and other enclosed spaces will require engineered structures that are unlikely to conform to the setbacks that apply to shoreline development. How many rules (that the rest of us are required to follow) would be broken to develop these properties? How many compromises would we as a community be required to make in trade for having a completed boardwalk? Who is served by this process. Back to the beginning, when I couldn't find a parking spot. If Bylaw 491 allows a hundred hotel rooms, several residences, retail, restaurants, pubs etc on this tiny sliver of land, most of the customers, suppliers and employees will not have vehicles? No it is not. How many vehicles will they have? Maybe a hundred and fifty? Maybe more? Ganges is already unable to accommodate the tourist demand for parking spaces in the summer season. Locals are displaced by tourists and therefore are unable to enjoy their town. Ganges is full to overflowing. Whose responsibility is to manage and moderate this issue? What is their plan?

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- I incline toward preference that upland owners own and maintain the boardwalk structures fronting their commercial enterprises. Their clients and customers will all depend and benefit from this pedestrian access. I suggest a preference that our community (CRD) should own and maintain all of the boardwalk and foreshore access west of the Ganges Marina Ramp for the benefit of all the public.
- The look of this overblown development is exactly what we do not want, as seen the recent referendum; past referendum re incorporation and in the OCP. Do not pay those with no rights ie: illegal filling on their and other property. Very skeptical because this is still being considered, post decisive referendum, also considered, post decisive referendum
- An ill-conceived Chamber of Commerce project that was intended to further shoreline development but sold to public as access/beautification. A well designed top of bank path separated from traffic could connect existing boardwalk and bypass the problem owners without extra harbourside development. We are not a municipality and do not need to trade our environment for tax \$
- I think that's it – let's put any new development away from the water
- Very sad that a few sill pushing their "incorporation agenda". This project is really a simple wish and we should be looking at building in a way that solves the problems in a novel and productive way without tying it to commerce at this time.
- Can it!
- Why is it called harbour "walk" when it involves a three storey structure and an underground parking lot?
- The boardwalk completion is being hijacked by the demand for increased density and infill plus height concessions. If these demands are successful contrary to popular opinion, the boardwalk will be seen as a sign that the OCP has no power and developers get what they want – a bad legacy for any boardwalk
- This development has the potential to add a charming and attractive site to the waterfront, if it is thoughtfully done
- Maybe there could be a fund started so that the Harbourwalk would belong to the community not the marina which will have tables etc. on it – restaurants and people will be reluctant to walk along it – as many are at the Tree House with tables on both sides of that right of way
- The LTC should seek a solution through the new provincial government that will permit the expropriation of the lands in question. Further work on this project should be held until clarification of options is received from the province and/or after the local elections and/or until a community based OCP review has been completed to provide guidance on future harbour development
- It would be nice to have a more pedestrian friendly way to get around the harbour, but maybe the harbour walk isn't it
- A great idea but not at any cost – other communities have gone to citizens for funding

**4. What did you think of today's workshop? What improvements could be made for next time?**

- Larger facility, we have lots more people to come

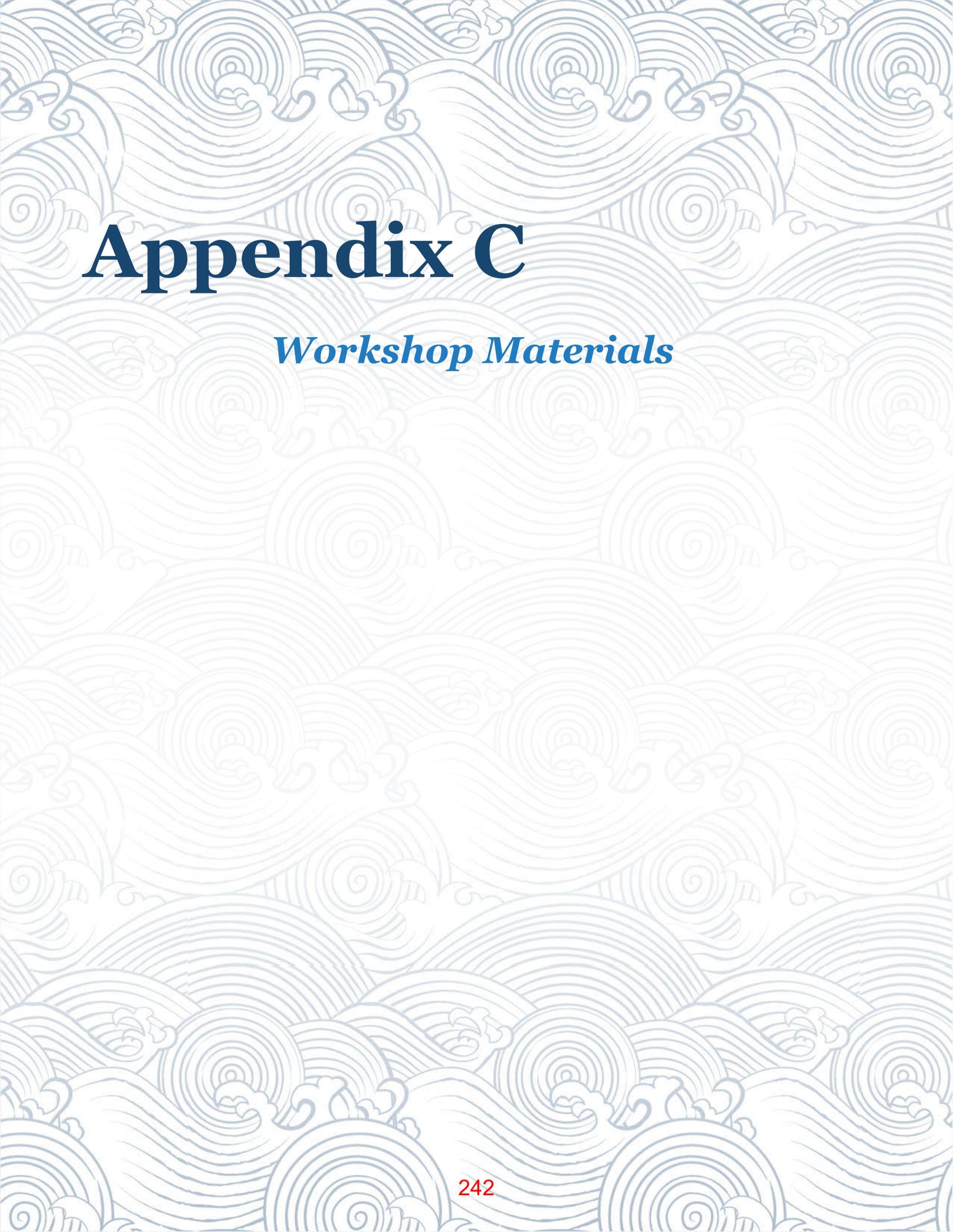
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- This should not have been given to the public for discussion before all of the terms in the rezoning application are known. What was the point in showing drawings from a 2005 proposal with no details as to what is not being proposed. Granting a general rezoning could only create great acrimony in the community. Come up with full details before giving this any further consideration.
- I think it was a very challenging environment and audience and the planner and facilitators did a good job trying to keep it calm and moving forward
- Jason, Pat and Peter were excellent. Participants unruly!
- I thought it was chaotic. I don't think the workshop format works well on Salt Spring. The people here are very opinionated and disruptive and rather to vigorously debate.
- It was poorly organized and poorly run – George Grams rude interruption was out of place and undemocratic – with such an outburst he loses even more credibility on top of proving he is out of touch with the community in connection with the incorporation referendum. Allowing 30 minutes for questions from the floor is ludicrous in a 4 hour marathon workshop. People want to be heard in a community setting and deserve to be heard in a town hall format – they don't wish to be herded solely in the direction of getting private notes on paper.
- We came a bit late, and there was not even standing room left! Couldn't get in – maybe larger venue? High school gym.
- More room, I could not get in
- I expected the usual panels showing the proposal, an introduction, some Q&A and the Trustees and Trust staff mingling throughout the four hours to talk to those who came. This would accommodate those not able to attend the whole thing, especially those arriving a bit later. It was a surprise therefore to arrive about 2:30 and find an agenda designed for only the first 100 people to arrive with discussion groups in process. In future, please make the agenda clear in announcements of such events. If it's as formatted as this workshop, please use a bigger venue, else set it up so that 100's can come and go through the scheduled time.
- Useful – thank you for the opportunity
- Jason was wonderful – calm factual, knew all issues well and related with an excellent rapport and respect. Thank you Jason. I believe that the workshop was faulty in its assumption – re: the as is project even being considered at this point. Had we been incorporated after the referendum, I understand we would have had no input, just a little surface engagement – the timing with 3 pro-incorporation elected officials would have been “perfect”. Next time, more questions and answers as part of the agenda please – then those people who were effectively undermining the process would not have needed to – although we were not part of that group, we understand their sentiments and actions.
- A fundamental flaw was the assumption that the community bought into increased commercial development to get a boardwalk. That was true for our pro incorporation/development trustees but not the larger community. You can't sit around tables and play with lot coverage/building height until the larger question is answered. Who decided to engage a Vancouver based consulting company specializing

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in large infrastructure projects to facilitate a relatively minor public information meeting? What are their terms of reference and do they have an ongoing role with the Trust or CRD?

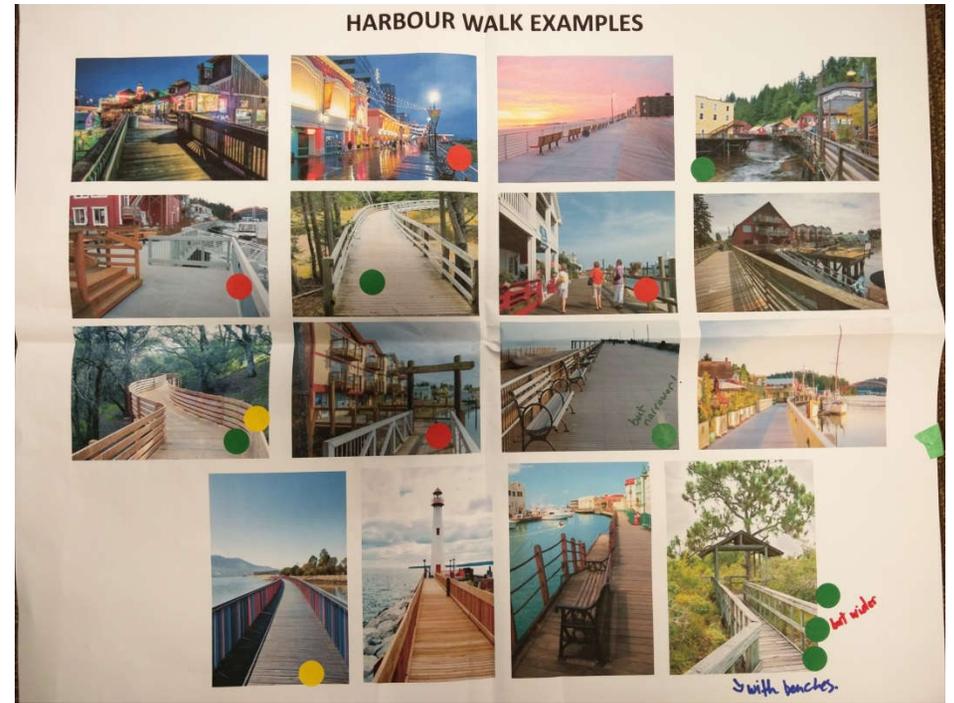
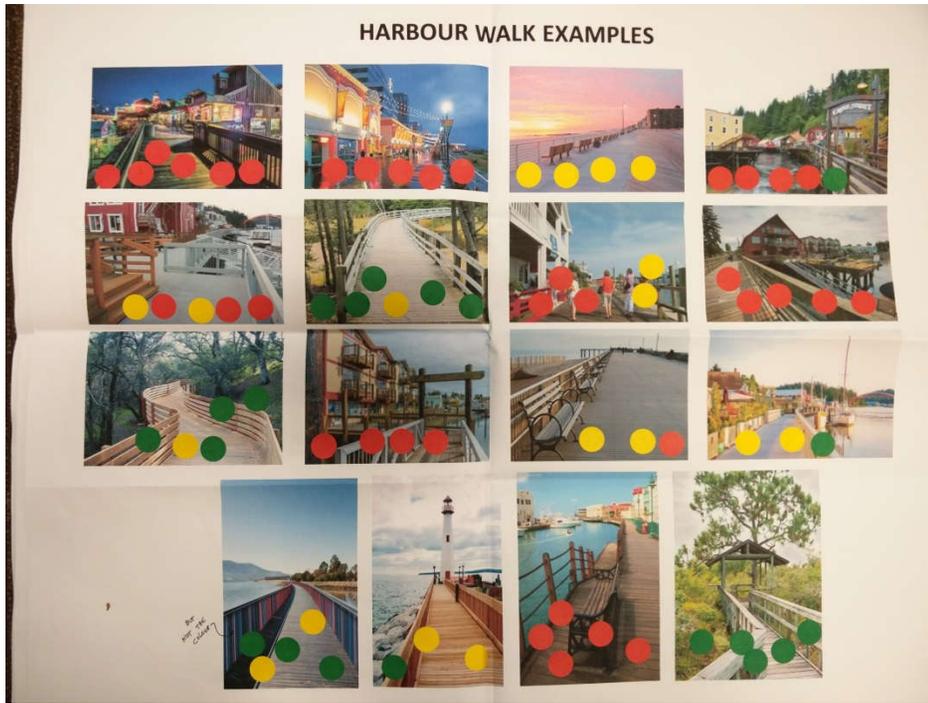
- Raises more questions than answers. Again CRD has misjudged how residents feel. If there is another time then the CRD should have all their “deals” in a row with believable and researched answers.
- Had standing room only. Thought it was an open house so came at 1:30 – no room, let us in anyway but could not see speakers or hear. We were in the entrance hall. Larger venue
- Obviously a much larger venue was required. It was also impossible for late comers to comprehend the process or get up to speed (leaflets maybe)
- We left early because it was too noisy to hear the discussions at our table (the people at the table subsequently moved into the storage room). The concerning issue of harbourfront development requires a larger space for open houses and shows respect for those who attend. Thank you for providing this opportunity to give our feedback on this important issue
- Venue was too small, very noisy and uncomfortable. Some people left early to escape the bedlam. This was not a great choice considering the topic at hand and the strong likelihood that a large turnout was in the cards
- Space rented was not large enough to accommodate interested parties – meeting was not well described beforehand. We were unable to get in for over half an hour as we thought the 1 – 5 time frame was a drop in time to see the proposal and comment on them. All the tables for the workshop type of meeting took up too much room. Another meeting should be held in another (larger) venue when more is definite. As you know from this meeting, many citizens are very concerned about the proposals so far. Everything must proceed carefully and think we should wait. The extended boardwalk is not work what is being required.
- The staff presentation was clear and informative. The elected officials should have anticipated the level of public interest and sentiment and adjusted the format, either on the spot or in advance. This would have diffused negative relations. The facilitator could have repeated the meeting objectives and the status of the draft bylaw more frequently. It is difficult for people to understand the planning process. If the negative reaction to the marina proposal had been acknowledged by the facilitator and the LTC upfront, and the Q&A period had been extended, the session would have been more productive.
- I arrived late and wasn’t able to get into the workshop. Perhaps hold the next meeting in a larger venue. Nice of one could just come for an hour, rather than 1 to 5. Or could you give us some preliminary information so we could come informed.
- Not what I expected. I thought it was an open house where we could look at plans and give input – not a Trust meeting. It ended well though and I hope Trust has heard the community which was pretty close to consensus
- Should have happened prior to developing bylaw. Trust met with business and property owners – should have also met with wider community as well. That part of the process was poor – workshop itself was good – thank you



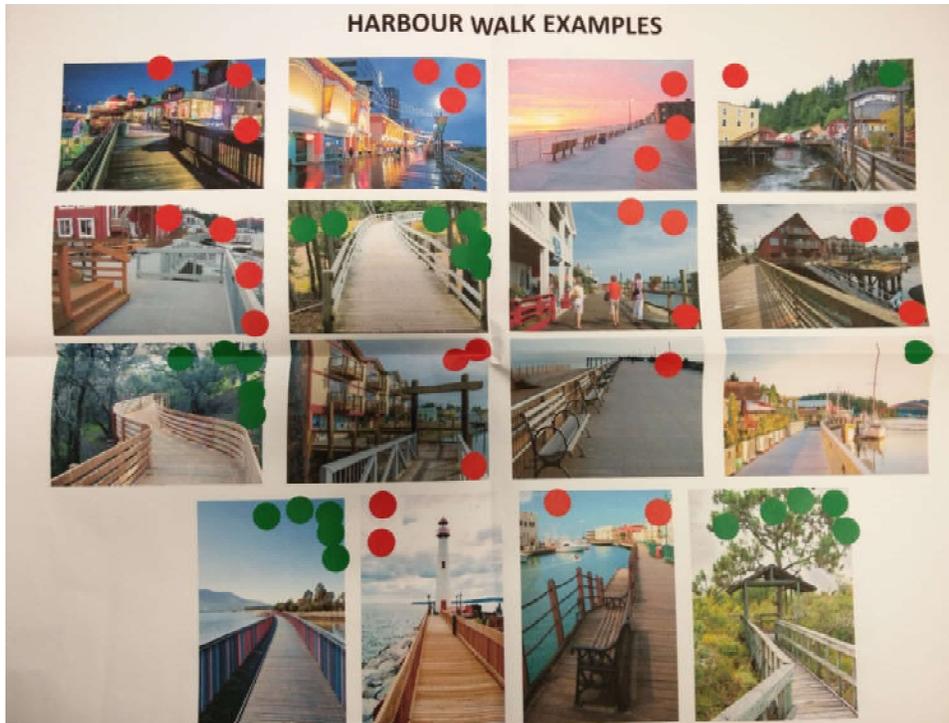
# Appendix C

## *Workshop Materials*

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Wants	Don't want
- boardwalk	- development
- small tasteful building	- not big/out of scale size
- one storey ok to scale (limit)	- up zoning
- high tide boardwalk	- on water boardwalk
- better to use what's still existing (pilings)	- no 3 story buildings
- everyone pays for a board on the boardwalk	
- trees along the bank	* - people like this
- one storey office	- emphasize natural on fill area / uplands area / hillside
- small Marina office on shoreline - one storey	- reduce erosion - give boardwalk "salt spring" feel

<u>wants</u>	<u>don't want</u>
<ul style="list-style-type: none"> <li>• preserve the views/apes</li> <li>• connect to the ocean through view corridors</li> <li>• like to see sea in the village - not only on the waterfront - but from the road.</li> <li>• one story - can't ha</li> <li>• see through to bridges / Moby's</li> <li>• parkspace - easy access pleasant walkspace.</li> <li>• restored <u>filled</u> area</li> <li>• salt marsh</li> </ul>	<ul style="list-style-type: none"> <li>• don't privatize viewscape</li> <li>• 3 story</li> <li>• building you can't see through</li> <li>• tradeoffs</li> <li>• privatization of the water</li> <li>• nothing on the <u>fill</u></li> <li>• no cars - no pollutants into the water</li> </ul>

<u>wants</u>	<del>don't want</del> <u>wants</u>
<ul style="list-style-type: none"> <li>- boardwalk maintained</li> <li>• boardwalk constructed to sea level rise</li> <li>- don't shade out marine vegetation</li> <li>• design to allow light</li> <li>- take into account sea level rise</li> <li>- health of marine env.</li> <li>- rest. of sea environment</li> <li>- ♥ coffee/cute shops/benches on the ocean</li> <li>- upland - nicer</li> <li>• view corridors - viewsapes - breaks - setbacks to allow connection</li> <li>• better development - across from S.D. - busy main</li> <li>- place where people over - put into fabric - in fill/site</li> </ul>	<ul style="list-style-type: none"> <li>• 2 storeys - shading view corridors</li> <li>• F.N. consultation</li> <li>• satisfy req.'s</li> <li>• Man Hall is tall</li> <li>• development d-town are 2 storeys</li> <li>• 2 storey could be setback</li> </ul>

## wants

- take market into consideration
- need some economy for building
- outer stuff - Islands Trust - character consideration
- see pictures of restored shoreline - what are we talking about.
- see compromise - Salt Springy - in fill site is real opportunity
- no building b/w road & water - walkway
- could have 2 walkways - gardening

## Marina building ideas

- leave as they are <sup>↑</sup> - legal <sup>non</sup> conforming

## ~~wants~~ Comments

- Boardwalk moving forward - vision of property owners / community don't give accept delay
- Boardwalk long term vision - walkways should be planned together with land use
- Transp. plan in isolation from land use.
- Have B.W extend ocean (in view) for areas - walkway along roadside that bring you up and down to roadway.
- Don't have to "sell our souls" for Boardwalk
- Ends veer to CRD walkway - roadside - and back on Boardwalk again

## Buildings comments

- not residential - too busy
- something for business - small scale on the shore - not an infill
- trees on the bank -
- extra commercial - all stores on Island are not full - many empty. restaurants and shops
- realters - cost of comm. space is so high - closure of bike shop - too expensive
- owners can wait out owners because low availability of comm.
- no justification to remove commercial in infill areas 50-70 year vision
- put it in the most effective place - not ALR
- no buildings for sea level - mixed use ok → water

- dif people hav dif. noise tolerance
- small apt. for different people
- mixed use - ok - more eyes on the street
- nothing where fill is -
- harbour filled in - parking lot thru it's that area for commercial - not parking
- pragmatic ped. plan → downtown for people doing stuff → not cars
- ped. friendly - currently dom. by cars
- store cars ~~by~~ not on waterfront
- use waterfront for people, access for boats.
- ~~no~~ not space for people to live in
- ~~no~~ waterfront condos - not ~~so~~ cute - will be out of site - don't like waterfront properties

- no small-scale multifamily units  
so people could have walking options to grocery
- condo waterfront – back in the city
- sustainable – development needs for walkability
- look at development services close to people
- block the view – not good
- ~~can't have both~~ can have both –  
need a vision
- affordable housing ~~no more~~ small multi-family stock
- aging → will have to think about off island
- big need for housing types

Questions

- can the proposal go ahead without more consultation

Developer Responsibility

- obligation good citizen/resident to be part of community
- resp for character, design, considerations that fit salt spring
- no fit – for that development
- service provision – no pressure on Island Trust
- allowing lots of trees/vegetation consistent w natural ambience
- don't see linkage b/w desires and boardwalk –

Developer Responsibility  
 not particular proposal - negotiations for  
 community amenity - ~~develop~~  
 protection of marine environment  
 enhancement of m. environment  
 TC terms of development  
 LTC has responsibility for public interest  
list doesn't make sense. - what will  
 trust do.  
 up top residence - % that is  
 affordable in  
 shouldn't consider home down la  
 with sea level rise  
 major changes to come  
 re: development  
 less is  
 more

What Should Be done?

- When foreshore leases come up require property owners to give access to boardwalk before renewing.
- Look at acquiring "Last Line" property for public use
- ~~Not feasible~~ → how it becomes part of boardwalk

What's Important?

- History - what was there before.
- Safety - connections for pedestrians
- Natural Features/Environment
- Respecting First Nations

What Should not be done?

- Infill of foreshore.
- Renew Lease for waterfront/infill areas.
- Parking Lots
- fossil fueled vehicles on site

Concerns.

- current zoning allows for uses community may not want
- services and utilities capacity? sustainability
- view from water: aren't local owned firms/businesses
- SPAS - should address water + related energy issues - fully design guidelines include them
- historical condition of upland properties - if development in certain areas

Community Consultation – Ganges Harbourwalk Upland  
Appendix C – Workshop Materials

① Peter + Lina  
② Peter + Lina

What do you envision?

Other issues

- if we wait for Boardwalk - other developers may come with deeper pockets zoning/bylaw needs to be adopted before building Boardwalk.
- if we don't get property owners' permission, do we have a fall back plan?
- mean-high tide should belong to community
  - ↳ riparian rights say you can't restrict public access
- we should have a referendum on if the community is willing to pay
- have we discussed shutting down main road to improve pedestrian safety?
  - ↳ bypass?
- Keep it simple \* get it done (or not) - we need a decision!!
- LTC driven process causes problems.
- Island Trust Policy statement
  - ⇒ 4.4.2 - water quality + levels are important when talking about increasing density.
- <sup>Proposed</sup> Sidewalk along Ganges should go ahead even if boardwalk does not.
- disabled access

① Peter + Lina

What do you envision?

- Clear view-scape
- Small scale development
- embracement of First Nations culture/heritage
- free standing boardwalk (seawalk) not dependant on SRWs.
- public access to a clean harbour and water
- Complete plan to retain 10m <sup>(open air space)</sup> setback (infill area) for 4 properties
- Safe public + vehicle access along the <sup>existing</sup> road
- consideration of marine life
- A boardwalk similar to Crofton's → emergency services need access
- Infill space should not be rezoned
- Seawalk/boardwalk with no commercialization (blocks views)
- respect nature + surroundings to embrace history of Salt Spring Island
  - ↳ interpretive signs.
- No buildings except facilities (washrooms etc.)
- No further densification
- Clean Marina and friendly access
- grassroots process (instead of Islands Trust) Bottom-up

### Wants

- CRD to purchase house
- infill to be grass/natural/vegetation  
Development present 201-89
- Boat access boardwalk
- business could have access over boardwalk
- buildings done in local style (FUs, historic)
- no theme park - not a facade, different architectural styles, authentic
- 2-3 two stories denture down lower ganges, none or fill
- small office for marina, chandlery  
 year-round use for locals  
not a coffee shop
- Stairs connecting to current boardwalk
- Aerial walkway

Marina area  
Grass Boardwalk  
How connect? Means

Who benefits from the development

concern about current business viability

Pros	Cons
<ul style="list-style-type: none"> <li>• Could be like Dotery Park - Green w/ boardwalk</li> <li>• Access for general public</li> <li>• maintain some natural grasses</li> <li>• Could house be bought for park</li> <li>• Could CRD take control instead of developers? Who has domain?</li> <li>• Could be as natural as possible</li> <li>• Oyster catcher boardwalk is okay</li> <li>• Farmers institute access</li> <li>• Lots of access</li> </ul>	<ul style="list-style-type: none"> <li>• <u>Marina would have to cross the boardwalk</u></li> <li>• J Trust is giving too much to developers</li> <li>• LTC = Mandate to protect, not for developers</li> </ul>

Community Consultation – Ganges Harbourwalk Upland  
Appendix C – Workshop Materials

- Proposed boardwalk infringe on the existing businesses.
- First Nations land, not just these 4 properties in question.
- Sustainable
- Opportunity for ~~forging~~ Forging connection.
- Collaboration with First Nations, chance to do it right.
- Sidewalks - there are none.
- Don't do archaeological study. Cart before the horse
- Too much focus on weekend visitors. Not Locals
- We need 1 comprehensive Harbour Planning Board.
- ~~Unceded~~ <sup>Unceded</sup> territory - Grace Island example
- Filled over beach values + First Nations values

- Sunrise, Moon views, Ocean breeze ✓  
Read + Follow OCF - trustees
- ~~Do~~ Not reward developers for previous illegal activity
  - Want access for public
  - Want clear map of who owns what.
  - Water needs cleaning
  - Don't need fill-in
  - Cause future fish habitat issues
  - Return to working harbour
  - Stop C-7
  - Look for other examples
  - Port Alberni.

- BAD PROCESS
- Best part greenery
- Businesses are ugly, No buildings
- Ugliness is businesses choice
- Want accessibility - water
- OCP - Storeys Not allowed.
- Boardwalk is not necessary
- Want to talk more.
- Boardwalk good but no development
- Removal of illegal infill.

Community Consultation – Ganges Harbourwalk Upland  
Appendix C – Workshop Materials

Boardwalk would increase safety on the road side.

businesses can contribute to ownership of boardwalk.

Pros.

- finish it !!
- Walk along the water.
- Path, public access.
- beautification.
- simple solution.
- view scapes, public park.
- walk connecting path.
- boardwalk focus.
- pathway

Cons.

- too expensive.
- environmental impact.
- development.
- sea level changes.

First Nation collaboration and scapes incorporated.

\*overpass over marina !!

+ would be great to finish the boardwalk!

Public access to waterfront, walkway, other parks etc.

Maintain views.

Non-commercial or very small-scale commercial.

as many commercial or world-wide (P. Vallarta, San Francisco, Brighton etc)

Have simple walkways along the shore, not commercialized but extremely well-used + seen as a major attraction + asset to the community

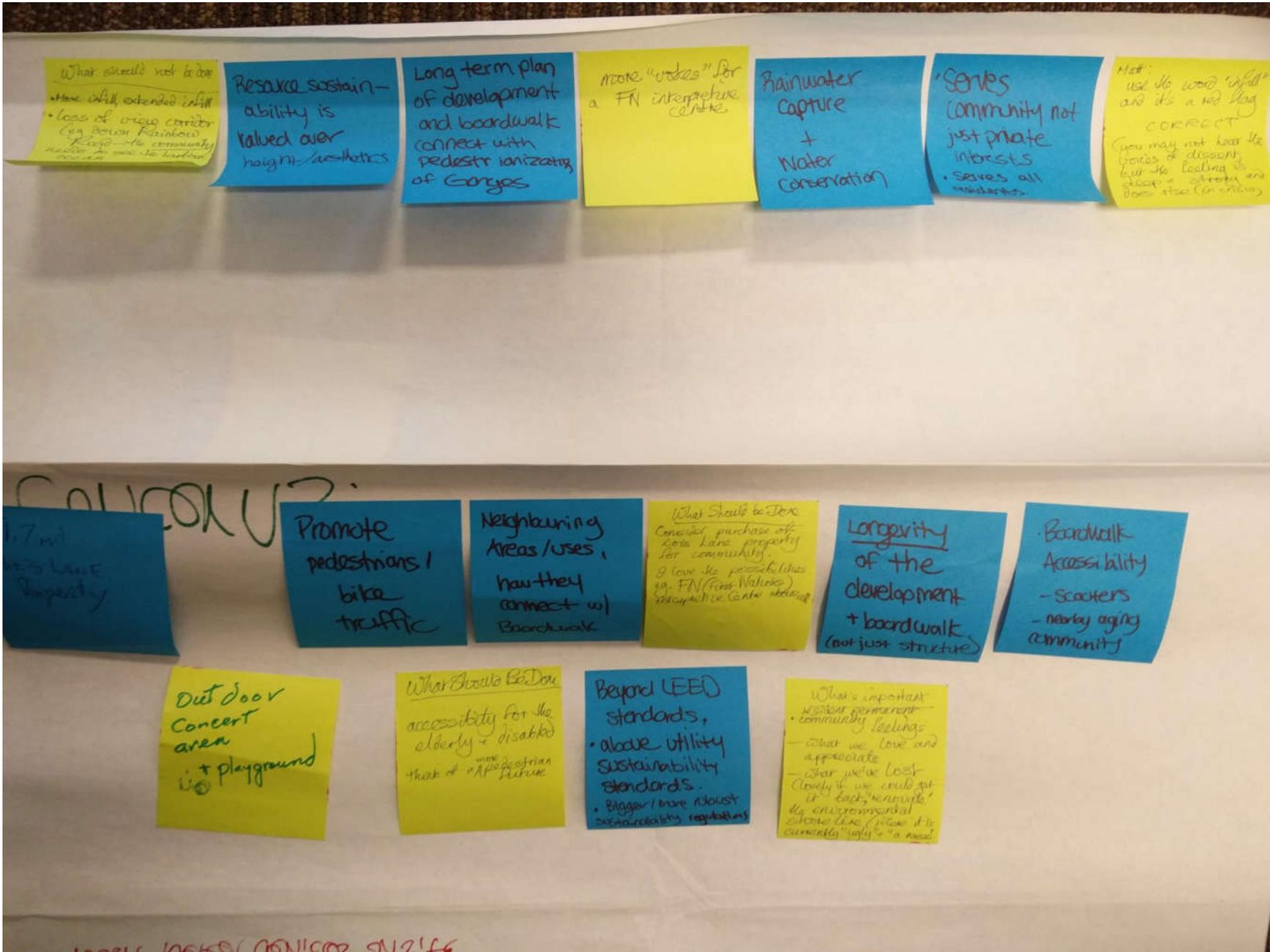
A few examples

- Davis Bay
- Powell River
- Vancouver Seawall
- (Quebec)

No Commercialised Boardwalk

- Build the Boardwalk and provide interpretive signage about First Nation history, sea shells life crabs fish etc.

Community Consultation – Ganges Harbourwalk Upland  
Appendix C – Workshop Materials







1 - 500 Lower Ganges Road Salt Spring Island BC V8K 2N8  
Telephone **250.537.9144**

Toll Free via Enquiry BC in Vancouver 604.660-2421. Elsewhere in BC **1.800.663.7867**

Email [ssiinfo@islandstrust.bc.ca](mailto:ssiinfo@islandstrust.bc.ca)

Web [www.islandstrust.bc.ca](http://www.islandstrust.bc.ca)

May 16, 2018

File: 6500-20 – Ganges Village Area Planning -  
Harbourwalk

via email: [FLNR.Minister@gov.bc.ca](mailto:FLNR.Minister@gov.bc.ca)

The Honourable Doug Donaldson  
Minister of Forests, Lands, Natural Resource Operations and Rural Development  
PO Box 9049 Stn Prov Govt  
Victoria, BC V8W 9E2

Dear Minister,

Re: Ganges Harbourwalk, Salt Spring Island

On behalf of the Salt Spring Island Local Trust Committee (LTC), I am writing to seek your support in helping the Salt Spring Island community achieve its longstanding vision of a harbour boardwalk in Ganges.

Our staff will soon submit an application to the Ministry of Forests, Lands, Natural Resource Operations and Rural Development (FLNRORD) for a statutory right-of-way (SRW) along a portion of the foreshore in Ganges Harbour to allow completion of a critical segment of the Ganges Harbourwalk (see page 3 for approximate SRW route). In the late 1980s a community-led boardwalk was constructed within the area of Islands Trust's forthcoming SRW application, but it fell into disrepair. Much of the original infrastructure remains in place, and the Capital Regional District (CRD) is currently undertaking a structural assessment to determine whether it can be refurbished and used anew. The results of that assessment should be made public shortly.

At the same time, FLNRORD staff are currently considering renewal of a Crown tenure for the Ganges Marina that includes within its boundaries the area proposed for harbourwalk development. For the past 30 years, the old boardwalk infrastructure has been included in the Ganges Marina tenure, with the marina operators assuming liability for it. The LTC is proposing that the general path of the original boardwalk be excluded from the marina's tenure area and instead kept in public hands. While today's overlapping local government and private sector interest in the same water area may appear to present a conflict, the LTC is confident that that the harbourwalk and marina can co-exist, and indeed be of mutual benefit. Thoughtful wording within any agreements reached between FLNRORD, Islands Trust and the Ganges Marina operators can ensure that all agencies, as well as the Ganges Marina operators, can achieve their respective objectives.

The LTC respectfully requests that the Minister encourage FLNRORD staff to forego renewal of the Ganges Marina tenure until they have received and assessed an application from Islands Trust seeking public use of the subject foreshore area, and that said application be considered in light of a longstanding Salt Spring Island Official Community Plan policy to work towards completion of the Ganges public pathway system, of which the harbourwalk is an integral part. It should be noted here that because the *Islands Trust Act* does not permit LTCs to incur liabilities, Islands Trust's SRW application will request that the Crown retain liability for the existing boardwalk infrastructure within the SRW until such time as a detailed plan to advance boardwalk construction and operation is developed.

.../2

Honourable Doug Donaldson

May 16, 2018

Page 2

We hope that ministry staff will give due consideration to this request. Islands Trust staff are available to provide you with any necessary information concerning the harbourwalk project to date.

Completing the Ganges Harbourwalk is a matter of public interest for the people of Salt Spring Island and its many visitors. Its development will help our Local Trust Committee fulfil the Trust object, "to preserve and protect the trust area and its unique amenities and environment for the benefit of residents of the trust area and of British Columbia generally . . .".

The Salt Spring Island Local Trust Committee looks forward to working with FLNRORD, the CRD, and all stakeholders to bring this important piece of community infrastructure to fruition.

Yours sincerely,



Peter Luckham

Chair, Salt Spring Island Local Trust Committee

cc: The Honourable Selina Robinson, MLA, Minister of Municipal Affairs and Housing  
Adam Olsen, MLA, Saanich North and the Islands  
Elizabeth May, MP, Saanich and the Gulf Islands  
Wayne McIntyre, CRD Area Director, Salt Spring Island  
Salt Spring Island Local Trust Committee

.../3

**Approximate Location of Subject Harbourwalk Segment**



**Jason Youmans**

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**From:** Lisa Floritto  
**Sent:** Wednesday, June 13, 2018 3:56 PM  
**To:** Salt Spring Island Local Trust Committee  
**Cc:** Jason Youmans  
**Subject:** FW: 239273 LUCKHAM Ganges Harbourwalk

**From:** de Bree, Jennifer FLNR:EX [<mailto:Jennifer.deBree@gov.bc.ca>] **On Behalf Of** Sutherland, Craig FLNR:EX  
**Sent:** Wednesday, June 13, 2018 3:53 PM  
**To:** Lisa Floritto  
**Cc:** Minister, FLNR FLNR:EX; Engels, Simone FLNR:EX  
**Subject:** 239273 LUCKHAM Ganges Harbourwalk

Reference 239273

Peter Luckham, Chair  
Salt Spring Island Local Trust Committee

Dear Peter Luckham:

Thank you for your letter of May 16, 2018, to Honourable Doug Donaldson, Minister of Forests, Lands, Natural Resource Operations and Rural Development, regarding the Ganges Harbourwalk. I have been asked to respond.

I appreciate the background you provided, as well as the Salt Spring Island Local Trust Committee's commitment to this proposal. I understand that West Coast Authorizations staff have been working with Islands Trust staff and are aware of the importance of the Ganges Harbourwalk proposal to your community.

At the same time, ministry staff must also consider the existing interests, contractual and other rights of the Crown tenure holder, Ganges Marina. As you are aware, ministry staff are currently reviewing the Ganges Marina tenure for replacement. The replacement review will provide opportunity for public and local government input. A referral on the subject replacement application will be sent to the Islands Trust, and I would encourage the Salt Spring Island Local Trust Committee to submit specific comments to ensure all relevant information is available to the decision maker.

West Coast Authorizations staff are aware of Islands Trust's intent to apply for a statutory right of way for the Harbourwalk route, and will support the review of this application once it is received. At the same time, I would like to advise and encourage you to initiate and engage in discussions with Ganges Marina, the upland property and rights holder, with the objective to reach a mutually acceptable arrangement.

Again, thank you for taking the time to write and express your interests. Please do not hesitate to contact Simone Engels, Crown Land Section Head by email at [Simone.Engels@gov.bc.ca](mailto:Simone.Engels@gov.bc.ca) or by phone at 250 751-7271 if you or your staff require any further information.

Sincerely,

Craig Sutherland  
Assistant Deputy Minister  
Coast Area

pc: Honourable Doug Donaldson, Minister of Forests, Lands, Natural Resource  
Operations and Rural Development  
Simone Engels, Crown Land Section Head, West Coast Natural Resource Region

**Ganges Harbour Walk Budget Update (November 2018)**

<b>Cost Explanation</b>	<b>Budget</b>	<b>Actual</b>	<b>Funding Source</b>
Secure property rights to access	10,000	1,170	
Engineering and Environmental Assessments	60,000		
Project Manager*		22,846	
Survey		3,695	
Structural		10,686	
Geotechnical		3,583	<b>CX.075.2902</b>
Archeological		2,305	
Environmental		6,354	
Pecks Cove design and specifications	20,000		
Harbour walk design and specifications	60,000		
Pecks Cove construction	TBD		
Harbour walk construction	TBD		
	<b>150,000</b>	<b>50,638</b>	